



AGENDA

Lincoln City Planning Commission
Tuesday, October 19, 2021, 6:00 PM
Zoom, Streamed LIVE on Zoom
801 SW Highway 101 - 3rd Floor, Streamed LIVE on Zoom, Lincoln City, OR 97367

- 1. CALL TO ORDER, PLEDGE OF ALLEGIANCE, & ROLL CALL**
- 2. AGENDA CHANGES OR REVISIONS**
- 3. MINUTES**
 - 3.1. Planning Commission - Regular Meeting - Sep 21, 2021 6:00 PM
- 4. FINAL ORDERS, RESOLUTION, & WRITTEN COMMUNICATIONS**
 - 4.1. FR 2021-04 for ZOA 2021-04
 - 4.2. FR 2021-03 for ZOA 2021-03
 - 4.3. FR 2021-02 for ZOA 2021-02
 - 4.4. ZOA 2021-01 Final Recommendation
- 5. PUBLIC HEARINGS/DELIBERATIONS**
 1. Continued to 11/2/2021: ZOA 2021-05 Lighting
 - 5.2. VAR 2021-02 Excellence Inc 2219 NW Hwy 101
 - 5.3. CUP 2021-01 Dock 3000 NE 26th Street
- 6. OLD BUSINESS**
- 7. NEW BUSINESS**
- 8. PLANNING COMMISSION TRAINING**

9. PLANNER COMMENTS

10. FUTURE AGENDA ITEMS & NEXT MEETINGS

11. ADJOURN

All information for this meeting is available on the City of Lincoln City website at www.lincolncity.org, and this meeting will be televised live on Charter Channel 4 Lincoln City and rebroadcast at various times. Planning Commission meetings are streamed live on the Internet through a link on the City of Lincoln City website, and can also be viewed following the meeting. The meeting location is accessible to persons with disabilities. A request for an interpreter for the hearing impaired, for a hearing impaired device, or for other accommodations for persons with disabilities, should be made at least 48 hours in advance of the meeting to Cathy Steere, City Recorder, at 541-996-1203.

**LINCOLN CITY PLANNING COMMISSION
MINUTES
September 21, 2021**

1. CALL TO ORDER, PLEDGE OF ALLEGIANCE, & ROLL CALL

Attendee Name	Title	Status	Arrived
Marci Baker	Chair	Present	
Kim Blackerby	Commissioner	Present	
Lenny Nelson	Commissioner	Late	6:08 AM
Miles Schlesinger	Commissioner	Present	
MacNeale Smith	Commissioner	Present	
Robert Vincent	Commissioner	Present	
Mellissa Sumner	Commissioner	Present	

Chair Baker welcomed the two new commissioners - Melissa Sumner and Robert Vincent. Commissioners Sumner and Vincent each introduced themselves. The rest of the commissioners introduced themselves to the new commissioners.

2. AGENDA CHANGES OR REVISIONS

3. MINUTES

3.1. Planning Commission - Regular Meeting - Aug 17, 2021 6:00 PM

MOTION:	Adopt the minutes from the August 17, 2021 Planning Commission meeting
MOVER:	Kim Blackerby, Commissioner
SECONDER:	MacNeale Smith, Commissioner
AYES:	Baker, Blackerby, Nelson, Schlesinger, Smith, Vincent, Sumner
RESULT:	Passed

4. FINAL ORDERS, RESOLUTION, & WRITTEN COMMUNICATIONS

None

5. PUBLIC HEARINGS/DELIBERATIONS

None

6. OLD BUSINESS

None

7. NEW BUSINESS

7.1. Planning Director Introduction and Goals

Planning and Community Development Director Anne Marie Skinner addressed the commissioners. She said that she thought it would be a good opportunity to get together and review her thinking related to goals and plans for the Planning Department and the Planning Commission.

The Comprehensive Plan needs to be updated. The last time it was done was in 1984. The very first thing per state law - Goal #1 - is citizen involvement. The first thing to do before you write a draft is to involve the public. Ms Skinner said that we will hopefully be back to in-person meetings in the next couple of months, and we would like to start having public workshops with the Planning Commission. It would be nice to do those in the Council Chambers or someplace else where there is room to spread out. However, if we are still in this situation by the end of the year I will be rethinking that. She said that the delay also gives her the next couple of months to get all of the processes streamlined in the Planning Department. All of the applications need to be redone to reflect the procedures ordinance that was adopted last year and went into effect at the beginning of 2021. This is necessary so that our current development and consultants have what they need. In our small jurisdiction our planners do both current and long-range planning, unlike large jurisdictions that have entire groups that do each. Because our department is small, we do double duty. Ms Skinner said that she does not want to hinder current development and wants those processes to be as smooth and time efficient as possible so that she can concentrate completely on future planning. She said that she would like the commissioners to think about how you they would like to divide up the goals - there are 19 Statewide Planning Goals, but we are not mandated to address them all, and not every one needs to be revised.

Commissioner Nelson pointed out that big chunks of the Comprehensive Plan were updated in the 1990s. Ms Skinner agreed, saying that it is not like nothing has been done since 1984 - we have new documents such as the 2017 Housing and Economic Needs Analysis, the Transportation System Plan, and the Stormwater Plan. Those components need to be looked at in relation to the chapter in the Comprehensive Plan that deals with it in the context of the goals and objectives. Those objectives have not been touched. Even though we have the 2017 housing analysis, if you go to the housing goal of the Comprehensive Plan, the objectives listed are the same ones that were put there in 1984. The goals and objectives need to be updated to reflect the analysis. She said that she is also looking into having the housing needs analysis updated. With all the emphasis on housing and the shortages in workforce housing, it is important to have that analysis updated.

Ms Skinner said that she will e-mail the 19 Statewide Planning Goals (there are 3 or 4 that do not apply to Lincoln City), and asked that for the next meeting the commissioners think about which ones we should focus on - should we focus on all of them or on elements of each one - so we can put a framework together of how we are going to approach the public.

Commissioner Schlesinger brought up the 2018 visioning effort. Ms Skinner replied that the visioning document has been adopted and we can use it to incorporate into the Comprehensive Plan revision, but it was too short-reaching and did not provide enough long-term vision, and mostly focused on commercial and how we want it to look. When we redid the design standards, we pulled a lot from that visioning. She said that she would like to expand on the 2018 visioning and see what we want Lincoln City to look like in 20 years. It may be that 1-2 work sessions will be enough and we can put together bullet points and that will be enough. It may not take 6 months to get public input, but we need to start with that.

Commissioner Blackerby brought up feedback on a draft Comprehensive Plan that was given to the previous Planning and Community Development Director. He said that a lot of feedback was given both at meetings and submitted by e-mail, so he hopes that Ms Skinner has access to it somewhere. Ms Skinner replied that if commissioners still have the feedback they provided to the previous director to feel free to e-mail it to her and she will sift through it and at the next meeting will have a plan based on the feedback. Commissioner Blackerby clarified that the draft Comprehensive Plan they were giving feedback on was confusing at best and was footnoted in a strange way. He said that he would like to restart the feedback as Ms Skinner had suggested previously, but wanted to make sure that she also had access to what had already been done.

Commissioner Nelson said that, when it comes to the housing thing, it seems that the Planning Commission and the City Council have removed most of the restrictions on anything that you could build. So we are down to economics, which is not the focus of the planning commission. Ms Skinner replied that the City has an Economic Development Department and we will rely on them heavily for those sections. Chair Baker added that she does not think that the Planning Commission will be dealing with the implementation of the Comprehensive Plan as much as facilitating that conversation with the public on the overall vision of how we want housing to look like rather than how we are going to build it. Ms Skinner agreed, saying that the vision comes from the public (eg we want to see tiny house developments that are within x ft of these services). When we draft the goals and objectives for the housing chapter, those goals and objectives are based around the vision of the community. The Comprehensive Plan is goals and objectives, rather than rules that say "this goes here and this is that height." It is the overall vision and has those goals and objectives broken down on how to get to that vision. And the zoning ordinance is the rules.

Chair Baker asked Ms Skinner to also send out a link to the existing Comprehensive Plan when she sends out the Statewide Planning Goals.

Commissioner Schlesinger asked if there is a section in the Comprehensive Plan on transportation. Ms Skinner said that there is a Transportation Master Plan that was recently updated. There were also portions on transportation included in the Nelscott Gap Plan, which was also adopted. So most of the effort for transportation in the Comprehensive Plan will be around incorporating the Transportation Master Plan and the Nelscott Gap Plan. Commissioner Schlesinger replied that he really thinks that the transportation component is important, and that he would like to include the effort to get a secondary route through Lincoln City. If there was ever a catastrophic event here, the bridge over the D-River could be destroyed and that would cut off the community's ability to get around. Highway 101 alone is not adequate anymore.

Ms Skinner said that Zoom meetings will continue for the foreseeable future.

There are no cases scheduled for the next [October 5, 2021] meeting. Two cases - a variance and a conditional use permit (CUP) - were just deemed complete on Friday, so they will not be ready in time for the next meeting, and so the first meeting in October will be canceled. The second meeting in October [October 19, 2021], there will be a variance, and a conditional use permit. There will also be a hearing for the lighting ordinance. For the variance and the CUP, you as the Planning Commission make the decision to approve or deny. For the lighting ordinance, the Planning Commission makes a recommendation to City Council. For the variance and the CUP (this is just a reminder for current commissioners and the benefit of the new commissioners when cases come before the Planning Commission), your decision has to be based on the criteria in the code. A variance has a certain number of criteria, and each of those has to be met to approve it,

and if each of those is not met, then we break our code, and we cannot do that. It can be difficult because many times a case will come before you and you think it is great and like whatever it is and would like to approve it, but it doesn't meet the criteria and you have to deny it even though your personal opinion is that you like it. The CUP - there is a little bit more subjectivity to it. There are criteria, but they are a little bit more subjective. This one is for a dock, and the criteria for a dock are pretty black and white. Either you meet the distance or you don't. There will be a staff report in your packet to review. You are welcome to ask me [Ms Skinner] any questions prior to the meeting. Do it individually - you don't want to talk to anyone about the case before the hearing. The staff report will be briefly presented at the beginning of the hearing. There is an opportunity for the applicant to present, and then anyone who is in favor, neutral, or against will give testimony, and then the applicant has an opportunity to offer a rebuttal. Often there is no testimony so there is nothing to rebut. There is an entire script that staff prepares and gives to Chair Baker - she will keep us on task. Once you've closed public testimony and the record, you cannot ask any more questions. Any of your questions you will have to ask before you close the record.

Commissioner Vincent asked about talking to other commissioners as long as they don't form a quorum. Ms Skinner responded that it could cause problems, so it is better not to, and that if any of the commissioners have questions when they get the packet, that they can always contact her.

Commissioner Vincent asked whether the packet can be printed. Commissioner Blackerby said that during the period that Ms Skinner was gone, the commissioners were discouraged from making hard copies, and were told that they would have to produce any notes that they made on the hard copies. Ms Skinner said that she will check with the City Attorney and will send an e-mail to the commissioners.

Commissioner Blackerby asked about previous requests from commissioners to see traffic studies that staff reports were based on, but the studies were never provided. He said that the commissioners were Public Works would come, and nobody ever showed up. He suggested that Public Works might come and explain how they do traffic studies. Ms Skinner replied that typically the City Engineer does not attend the Planning Commission. Typically the only staff at the Planning Commission meeting are from the Planning Department. Commissioner Blackerby clarified that it is a one-time presentation that he is asking for.

Commissioner Blackerby asked if there was an update on the City Manager position. Ms Skinner said that the posting closed, and applications have been received, and that she believes there is a work session in October to go through the applications.

- 8. PLANNING COMMISSION TRAINING**
- 9. REPORTS & COMMENTS**
- 10. FUTURE AGENDA ITEMS & NEXT MEETINGS**
- 11. PLANNING STAFF REPORT**
- 12. ADJOURN**

The meeting was adjourned at 6:55 PM

Respectfully submitted,

James White
Assistant Planner

Marci Baker
Chair

Planning Commission Communication

FR 2021-04 for ZOA 2021-04

Meeting Date: October 19, 2021 Primary Staff Contact: AnneMarie Skinner
Department: Planning Commission E-Mail: ASkinner@lincolncity.org
Secondary Dept: Secondary Contacts:
Approval: Estimated Time:

See attached document FR 2021-04 for ZOA 2021-04 (provisions for administrative adjustment) for the chairperson's signature. Public hearing was held and recommendation given on August 17, 2021.

Attachments:

FR 2021-04 - ZOA 2021-04 Administrative Adjustment (DOC)

**LINCOLN CITY
PLANNING COMMISSION**

IN THE MATTER OF

Amendments to Title 17, Zoning, concerning) Final Recommendation
Administrative Adjustment) No. 2021-04
)

NATURE OF THE APPLICATION

ZOA 2021-04 amends Title 17 (Zoning), Chapter 17.77 (Applications), adding a new Section 17.77.005 (Administrative Adjustment), amending Section 17.77.010 (Adjustment), amending Chapter 17.76 (Administrative Provisions) to make corresponding changes to Table 17.62.020-1.

FINAL RECOMMENDATION

Based on the evidence presented at the public hearing on August 17, 2021, including the staff report, the Planning Commission recommends unanimously that the City Council approve the draft ordinance.

APPROVED THIS 19th DAY OF OCTOBER, 2021.

Marci Baker
Planning Commission Chair

ATTEST:

Anne Marie Skinner, Director
Planning & Community Development

Planning Commission Communication

FR 2021-03 for ZOA 2021-03

Meeting Date: October 19, 2021 Primary Staff Contact: AnneMarie Skinner
Department: Planning Commission E-Mail: ASkinner@lincolncity.org
Secondary Dept: Secondary Contacts:
Approval: Estimated Time:

See attached document FR 2021-03 for ZOA 2021-03 (provisions for single-family dwellings in the RM zone) for signature by the chairperson. Public hearing was held and recommendation given on July 6, 2021.

Attachments:

FR 2021-03 - ZOA 2021-03 RM fix (DOC)

**LINCOLN CITY
PLANNING COMMISSION**

IN THE MATTER OF

Amendments to Title 17, Zoning, concerning)	Final Recommendation
Revision to RM and GC zones)	No. 2021-03
)	

NATURE OF THE APPLICATION

ZOA 2021-03 amends Title 17 (Zoning), amending Chapter 17.20 (Multi-Unit Residential (R-M) Zone) to restore single family units as permitted uses under limited circumstances.

FINAL RECOMMENDATION

Based on the evidence presented at the public hearing on July 6, 2021, including the staff report, the Planning Commission recommends unanimously that the City Council approve the draft ordinance.

APPROVED THIS 19TH DAY OF OCTOBER, 2021.

Marci Baker
Planning Commission Chair

ATTEST:

Anne Marie Skinner, Director
Planning & Community Development

Planning Commission Communication

FR 2021-02 for ZOA 2021-02

Meeting Date: October 19, 2021 Primary Staff Contact: AnneMarie Skinner
Department: Planning Commission E-Mail: ASkinner@lincolncity.org
Secondary Dept: Secondary Contacts:
Approval: Estimated Time:

See attached FR 2021-02 for ZOA 2021-02 Floodplain Procedures for signature by the chairperson. The public hearing for Planning Commission was held on June 15, 2021 and the recommendation was given subsequent to the closure of the public hearing.

Attachments:

FR 2021-02 - ZOA 2021-02 Floodplain Procedures Correction (DOC)

**LINCOLN CITY
PLANNING COMMISSION**

IN THE MATTER OF

Amendments to Title 17, Zoning, concerning) Final Recommendation
Floodplain Procedures) No. 2021-02
)

NATURE OF THE APPLICATION

ZOA 2021-02 amends Title 17 (Zoning), Chapter 15.16 (Flood Damage Prevention), revising LCMC 15.16.450 to specify applicable procedures for appeals and variances.

FINAL RECOMMENDATION

Based on the evidence presented at the public hearing on June 15, 2021, including the staff report, the Planning Commission recommends unanimously that the City Council approve the draft ordinance.

APPROVED THIS 19th Day of OCTOBER, 2021.

Marci Baker
Planning Commission Chair

ATTEST:

Anne Marie Skinner, Director
Planning & Community Development

Planning Commission Communication

ZOA 2021-01 Final Recommendation

Meeting Date: October 19, 2021 Primary Staff Contact: AnneMarie Skinner
Department: Planning Commission E-Mail: ASkinner@lincolncity.org
Secondary Dept: Secondary Contacts:
Approval: Estimated Time:

FR 2021-01 for ZOA 2021-01 (COVID Timetable Extension) is attached for signature by the chairperson. The public hearing was held and the recommendation given on June 15, 2021.

Attachments:

FR 2021-01 - ZOA 2021-01 COVID extension (DOC)

**LINCOLN CITY
PLANNING COMMISSION**

IN THE MATTER OF

Amendments to Title 17, Zoning, concerning) Final Recommendation
Amendments for Covid Timetable Extension) No. 2021-01
)

NATURE OF THE APPLICATION

ZOA 2021-01 amends Title 17 (Zoning), Chapter 17.76 (Administrative Provisions), adding a new section LCMC 17.756.155 (COVID Timetable Extension) to grant all eligible approvals an 18 month timetable extension due to the economic impact of COVID 19.

FINAL RECOMMENDATION

Based on the evidence presented at the public hearing on June 15, 2021, including the staff report, the Planning Commission recommends unanimously that the City Council approve the draft ordinance.

APPROVED THIS 19th DAY OF OCTOBER, 2021.

Marci Baker
Planning Commission Chair

ATTEST:

Anne Marie Skinner, Director
Planning & Community Development

Planning Commission Communication

VAR 2021-02 Excellence Inc 2219 NW Hwy 101

Meeting Date: October 19, 2021 Primary Staff Contact: AnneMarie Skinner
 Department: Planning Commission E-Mail: ASkinner@lincolncity.org
 Secondary Dept: Secondary Contacts:
 Approval: Estimated Time:

Question:

Should the Planning Commission approve, approve with conditions, or deny the requested VAR 2021-02 for parking space encroachment at 2219 NW Hwy 101?

Staff Recommendation:

Staff recommends the Planning Commission approve with conditions VAR 2021-02, a request for a variance for parking space encroachment into the rear building setback at 2219 NW Hwy 101.

Authority:

Lincoln City Municipal Code (LCMC) Chapter 17.76 Procedures
 LCMC 17.77.060 Conditional Uses

Background:

See attached Staff Report

Financial Impact

None

Potential Motions:

1. Motion to approve VAR 2021-02 for a variance to allow parking space encroachment in the rear building setback at 2219 NW Hwy 101, with the conditions noted in the staff report; or

2. Motion to approve VAR 2021-02 for a variance to allow parking space encroachment in the rear building setback at 2219 NW Hwy 101; or
3. Motion to deny VAR 2021-02 for a variance to allow parking space encroachment in the rear building setback at 2219 NW Hwy 101 (then list each criterion specifically not being met from Section 17.77.140)

See attached Staff Report

Attachments:

Staff Report VAR 2021-02 Excellence Inc (PDF)

Signed Applications (PDF)

Landscaping Plan Set (PDF)

Architect Plan Set (PDF)

Narrative variance (PDF)

Civil Plan Set (PDF)

Zoning Map (PDF)

Site Map (PDF)

Staff Report

Planning Commission Hearing on October 19, 2021

Case File VAR 2021-02

Date: October 1, 2021

Case File: VAR 2021-02 Parking Space Encroachment in Rear Building Setback

Applicant: Gregory Wescott
PO Box 1255
Lincoln City, OR 97367
excellenceinc@charter.net

Property Owner: Wescott Gregory D Trustee
PO Box 1255
Lincoln City, OR 97367

Situs Address: 2219 NW Highway 101

Location: The subject property is located at the northwest corner of the Highway 101/22nd Street intersection.

Tax Map and Lot: 07-11-11-AD-02800

Comprehensive Plan Designation: General Commercial (G-C)

Zoning District: General Commercial (G-C)

Site Size: Approximately 10,454 square feet or 0.24 acre

Proposal: The applicant is requesting a variance to the requirement for parking spaces to be clear of building setbacks as described in Lincoln City Municipal Code (LCMC) Section 17.56.080.A.4. Specifically, the request would allow 4 feet of the front end of the required parking spaces to encroach into the required 12-foot rear building setback.

Surrounding Land Uses and Zones: North: Retail; G-C
South: NW 22nd Street and Subway Sandwiches; G-C
East: Highway 101; G-C
West: NW Oar Place and townhouses; R-M

Public Notice: The Planning and Community Development Department mailed notice to the owners of all properties within 250 feet of the site on September 28, 2021. The *News Guard* published the public hearing notice on October 5, 2021.



Authority: Lincoln City Municipal Code (LCMC) 17.76.050.G states that Table 17.76.020-1 identifies the decision authority for each Type III application. Table 17.76.020-1 of LCMC Chapter 17.76 identifies a variance as a Type III application with the decision authority given to the Planning Commission.

Applicable Substantive

Criteria: LCMC Chapter 17.32 General Commercial (GC) Zone
LCMC Chapter 17.56 Off-Street Parking and Loading
LCMC Section 17.77.140 Variance

BACKGROUND

The subject property (site) contains asphalt paving and an existing 740-square-foot building that is vacant. In August 2021, a development review application was submitted (File SPR 2021-03) for converting the existing vacant structure from its previous use as a gas station to an office/retail use. However, for File SPR 2021-03 to be approved, approval of a variance is needed to accommodate the required number of parking spaces.

Per LCMC 17.56.030, seven parking spaces are required. Per LCMC 17.56.080, the required spaces must be located on the same lot or parcel as the use; above, beneath, to the rear, or to the side of the building; outside of the required building setback areas; at least 10 feet from any public right-of-way; such that no backing or maneuvering takes place in right-of-way; and, in no case shall any parking spaces be located between the front of the building and the front property line. Per LCMC 17.32.050.B, when the rear yard is abutting or across the street from a parcel in a residential zone, the rear yard shall be a minimum of 10 feet, but be increased by one-half foot for each foot by which the building height exceeds 15 feet. In this case, the existing building is 19 feet tall, which equates to a 12-foot rear setback.

The required spaces are located on the same lot as the use. The required parking area landscaping is provided along with wheel stops and curbing. The spaces are located to the rear of the building. The spaces are more than 10 feet from any public right-of-way. The spaces are located such that all backing and maneuvering takes place on the site and not within any right-of-way. Not a single parking space is between the front of the building and the front property line.

However, the location of the proposed parking spaces encroaches into the required 12-foot rear building setback by approximately 4 feet. The reasons for the encroachment are as follows:

1. The parking spaces are prohibited from being in front of the building.
2. The building is an existing building constructed in 1940. The property owner is preserving and repurposing the existing building rather than demolishing it. The building is located roughly in the center of the site. New construction would have allowed the building to be placed close to the front property line, but this isn't new construction. Preserving and repurposing the building, rather than demolishing it, was chosen as the most economical, environmentally-friendly, and sustainable option. Additionally, even though the building is not specifically designated as historic by the state, buildings constructed more than 50 years ago are considered historic. Accordingly, preservation of the building in its current location preserves an historic building.
3. There isn't enough space on either side of the building to place all the required parking spaces, provide the required drive aisle width and site circulation, and provide the required percentage of landscaping, parking landscaping, and the required 4-foot-deep landscape buffer at the perimeter of parking spaces and drive aisles.
4. Standard spaces are required to be 20 feet deep. While compact spaces are only required to be 16 feet deep, no more than 50% of the required spaces can be compact. By providing the required depth for

the standard parking spaces, the required width of the drive aisle, and the required 4-foot-deep landscape buffer, a portion of the front of the parking spaces ends up in the 12-foot rear building setback.

COMMENTS

No written comments were received during the 14-day comment period.

ANALYSIS

17.32.050 Setback requirements.

B. Side, Street Side and Rear Yard. None, except when the side, street side or rear yard is abutting or across the street from a parcel in a residential zone, and then the side, street side or rear yard shall be a minimum of 10 feet. The required side, street side or rear yard shall be increased by one-half foot for each foot by which the building height exceeds 15 feet.

Finding: The building is a height of 19 feet which requires an additional 2 feet to the required 10-foot setback, creating the required 12-foot setback.

17.56.080 Development standards for off-street parking and loading areas for all uses other than detached single-unit dwellings, attached single-unit dwellings, and duplexes.

A. Location.

4. Parking and loading spaces must be outside of required building setback areas.

Finding: The existing required parking spaces are located 8 feet – 10 inches from the property line. The required building setback is 12 feet, as identified below in 17.32.050.

17.77.140 Variance

C. Approval Criteria. To approve a variance, the planning commission shall make findings of fact, based on evidence provided, that all of the following circumstances exist:

1. Exceptional or extraordinary circumstances apply to the property which do not apply generally to other properties in the same zone or vicinity and result from lot size or shape legally existing prior to the date of the ordinance codified in this title, topography, or other circumstances over which the applicant has no control;

Finding: The site contains an existing building that was constructed in 1940, which is prior to the date of the ordinance codified in this chapter. The placement of the building, roughly in the center of the site and not adjacent to the front property line, would not be allowed under the current code. The unusual placement of the building, in conjunction with the criteria in Chapter 17.56 regarding the size and location of parking spaces, drive aisles, and parking screening and landscaping, prevent compliance with the criterion regarding parking spaces not being in the building setback area.

The property owner does not have any control over the past placement of the building because the building is existing. The property owner does not have any control over the requirements in Chapter 17.56. It is recognized that the requirements in Chapter 17.56 apply generally to other properties in the same zone, but other properties in the same zone are not preserving and repurposing an existing building, built in 1940 prior to the codification of the existing code and that is placed on the site contrary to existing code because that code didn't exist when the building was placed. The property owner has chosen to preserve and repurpose the existing building, rather than demolishing it and building a new building in the proper location, since this is the more economical, environmentally-friendly, sustainable, and history-preserving option..

The location of the building, the requirements for the number of off-street parking spaces, and the requirements for the drive aisle width and lot shape and site layout create a circumstance that applies to the property and the need for a variance to allow a portion of the parking spaces to encroach into the rear building setback.

Staff concludes that this circumstance for granting a variance exists.

2. *The variance is necessary for the preservation of a property right of the applicant which is substantially the same as owners of other property in the same zone or vicinity possess;*

The site's GC zone allows retail and office uses, which are allowed uses on anyone's property in the GC zone. These uses require one parking space per 400 square feet of floor area. Accordingly, for these uses to be allowed the required parking must be provided. Without the required parking, the site could not be utilized for retail and office uses. The only way to provide the required parking and still meet all the other criteria is to have the approximate 4-foot encroachment into the rear building setback area.

Staff concludes that this circumstance for granting a variance exists.

3. *The variance should not be materially detrimental to the purposes of this title, or to property in the zone or vicinity in which the property is located, or otherwise conflict with the objectives of any city planning policy;*

The site contains an existing retaining wall along the west boundary. The project proposes construction of another retaining at the front end of the parking spaces with a 4-foot-deep landscape buffer on the other side of the retaining wall. The west boundary line is adjacent to the Oar Street public right-of-way. On the other side of the public right-of-way is the residential zone. The retaining walls and landscaping will provide an effective screen and buffer along the public right-of-way. With the retaining walls, landscaping, sidewalk, and street between the parking spaces and the residential zone, the 4-foot encroachment into the west building setback should not be detrimental to the properties in the residential zone across Oar Street. The objective of the criterion requiring no parking spaces in a building setback area is to provide distance and buffering between properties and parking areas. In this case, there is already a public right-of-way (street and sidewalk) between the site and the residential zone on the west side of Oar Street. The addition of the retaining walls and landscaping provides even greater buffering, so the objective is still being met with the minor encroachment.

Staff concludes that this circumstance for granting a variance exists.

4. *The variance requested is the minimum variance which would alleviate the hardship.*

The hardship is providing the required number of parking spaces outside of the building setback along the west boundary while still meeting all the parking, drive aisle, and parking landscaping and screening requirements. All the parking, drive aisle, and parking landscaping and screening requirements are being met, except the requirement for having the parking spaces outside of the rear building setback area. The building setback along the west boundary, since it is adjacent to a residential zone, is 12 feet. The parking spaces encroach into the 12-foot setback by approximately 4 feet. The variance being requested is to allow the front 4 feet of the parking spaces to be in the 12-foot setback. This is the minimum variance that will allow for placement of the required parking spaces. No more than the 4-foot-encroachment is necessary, and no more than 4 feet is being requested.

Staff concludes that this circumstance for granting a variance exists.

RECOMMENDATION

Staff recommends that the evidence presented in the submitted application materials as concluded in the findings in this report warrants approval of the variance request to allow a 4-foot parking space encroachment into the 12-foot rear setback.

OFFICE USE ONLY
 Date Filed: _____
 Amount/Fee: _____
 Receipt No: _____
 Received By: _____
 30 Days: _____
 Deemed Incomplete: _____
 Deemed Complete: _____
 120 Day Deadline: _____



OFFICE USE ONLY
 Stamp Date Received
RECEIVED
 Sep 03, 2021
PLANNING
 File No: _____

APPLICATION TYPE

<input type="checkbox"/> ANNEXATION	<input type="checkbox"/> LOT LINE ADJUSTMENT	<input type="checkbox"/> SITE PLAN REVIEW
<input type="checkbox"/> APPEAL OF PLANNING COMMISSION DECISION	<input type="checkbox"/> MINOR PARTITION	<input type="checkbox"/> SUBDIVISION
<input type="checkbox"/> APPEAL OF PLANNING DEPARTMENT DECISION	<input type="checkbox"/> NATURAL RESOURCE DEVELOPMENT REVIEW	<input type="checkbox"/> URBAN GROWTH BOUNDARY AMENDMENT
<input type="checkbox"/> COMPREHENSIVE PLAN & ZONING MAP AMENDMENT	<input type="checkbox"/> NATURAL RESOURCE DEVELOPMENT VARIANCE	<input type="checkbox"/> VACATION
<input type="checkbox"/> COMPREHENSIVE PLAN &/OR ZONING ORDINANCE TEXT AMENDMENT	<input type="checkbox"/> PLANNED UNIT DEVELOPMENT PRELIMINARY MASTER PLAN	<input checked="" type="checkbox"/> VARIANCE
<input type="checkbox"/> CONDITIONAL USE PERMIT	<input type="checkbox"/> PLANNED UNIT DEVELOPMENT FINAL MASTER PLAN	<input type="checkbox"/> OTHER _____

NAME OF APPLICANT: Gregory Wescott
MAILING ADDRESS: PO BOX 1255
CITY: LINCOLN CITY **STATE:** OR **ZIP:** 97367
DAYTIME PHONE: _____ **Signature:**

OWNER OF RECORD (if other than applicant)
NAME: Wescott Gregory D Trustee
MAILING ADDRESS: PO BOX 1255
CITY: LINCOLN CITY **STATE:** OR **ZIP:** 97367
DAYTIME PHONE: _____ **Signature:**

APPLICANT AND/OR OWNER'S INTEREST IN PROPERTY SUBJECT TO REQUEST:
 AGENT, FEE OWNER, CONTRACT PURCHASER, OPTION HOLDER, LESSEE, OTHER
PROJECT LOCATION: northwest corner of Hwy 101/22nd Street intersection
PROJECT ADDRESS: 2219 NW Hwy 101
ASSESSOR'S MAP NO.: 07-11-10-AD **TAX LOT(S):** 02800
AREA: 0.24 **(acres)** **SQ. FT.:** 10,454.4 **ZONING:** GC

**VARIANCE
EVIDENCE TO SUPPORT OF FINDINGS OF FACT**

NATURE OF VARIANCE REQUEST:

See attached narrative.

FINDINGS OF FACT:

The Planning Commission may authorize variances from the requirement of Ordinance No. 84-02 (as amended) where it can be shown that, owing to special and unusual circumstances related to a specific piece of property, strict application of the ordinance would cause an undue or unnecessary hardship. No variances shall be granted to allow the use of property for a purpose not authorized within the zone in which the proposed use would be located. In granting a variance, the Planning Commission may attach conditions that it finds necessary to protect the best interests of the surrounding property or neighborhood and otherwise achieve the purposes of the ordinance.

A VARIANCE MAY BE GRANTED ONLY IN THE EVENT THAT ALL THE FOLLOWING CIRCUMSTANCES EXIST:

1. Exceptional or extraordinary circumstances apply to the property which do not apply generally to other properties in the same zone or vicinity and result from lot size or shape legally existing prior to the date of the ordinance, topography, or other circumstances over which the applicant has not control.

See attached narrative.

2. The variance is necessary for the preservation of a property right of the applicant which is substantially the same as owners of other property in the same zone or vicinity possess.

See attached narrative.

- 3. The variance should not be materially detrimental to the purposes of the ordinance, or to property in the zone or vicinity in which the property is located, or otherwise conflict with the objectives of any city planning policy.

See attached narrative.

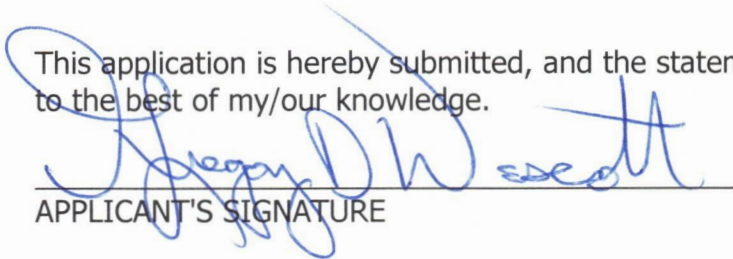
- 4. The variance requested is the minimum variance that would alleviate the hardship.

See attached narrative.

THE FOLLOWING ATTACHMENTS SHALL ACCOMPANY THE FILING OF A VARIANCE REQUEST APPLICATION:

- SITE PLAN AND/OR ELEVATIONS. (Or other depiction or documents indicating the nature of variance request).
- LEGAL DESCRIPTION OF PROPERTY SUBJECT TO VARIANCE REQUEST.
- EVIDENCE IN SUPPORT OF REQUISITE FINDINGS OF FACT.

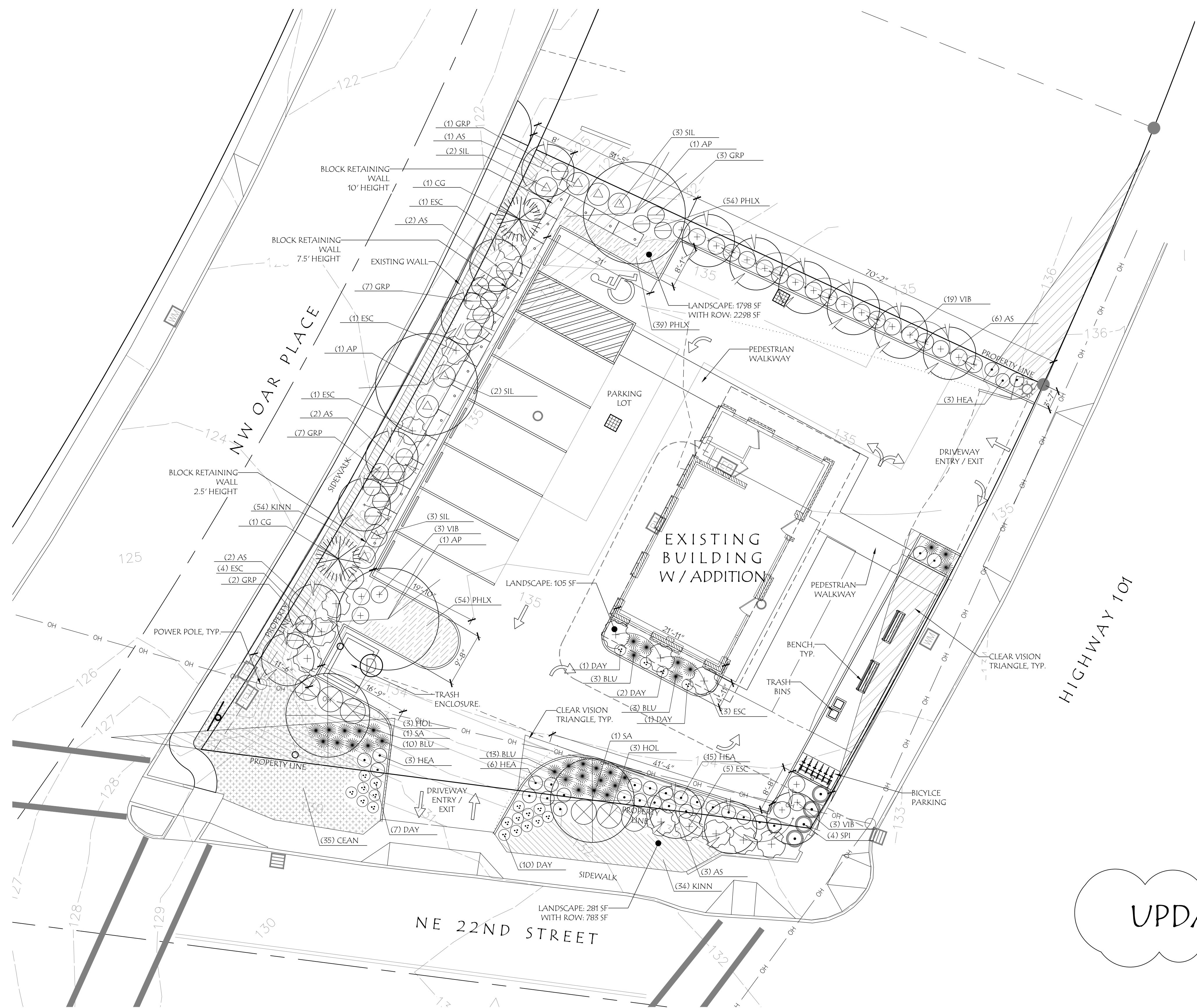
This application is hereby submitted, and the statements and information are true and complete to the best of my/our knowledge.



APPLICANT'S SIGNATURE

September 2, 2021
 DATE

NOTE TO APPLICANT: Since the burden of presenting evidence to support the request rests with the applicant at the public hearing, your attendance and participation at this public meeting, or that of your representative, is respectfully requested.



General Notes:

1. VERIFY ALL UTILITIES AND EASEMENTS. SEE CIVIL DRAWINGS FOR MORE INFORMATION. NOTIFY LANDSCAPE ARCHITECT OF CONFLICTS.
2. SEE ARCHITECTURAL DRAWINGS FOR SITE PLAN AND BUILDING DETAILS.
3. SEE CIVIL DRAWINGS FOR RETAINING WALL AND GRADING.
4. PLANTS SELECTED FROM LINCOLN CITY GUIDE TO LANDSCAPE SELECTIONS.
5. SHRUBS AND GROUND COVERS IN VISION CLEARANCE TRIANGLES ARE 2' HEIGHT OR LESS.
6. PLANT LEGEND, DETAILS AND NOTES SEE SHEET L1.2.
7. IRRIGATION PLAN SEE SHEET L2.1.

Laurus Designs, LLC

1012 Pine Street
Silverton, Oregon
503.784.6494

Excellence, Inc.

2219 NW Highway 101
Lincoln City, Oregon

REGISTERED
643
Laura A. Antonson
LAURA A. ANTONSON
OREGON
11/16/2007
LANDSCAPE ARCHITECT

PLANTING PLAN

SCALE: 1"=10'-0"
0' 5' 10' 20'
SCALE

May 26th, 2020

REVISIONS

#	DATE	NOTES	INITIALS
1	5.24.21	CITY COMMENTS	LAA

UPDATE PLANTING PLAN

1

L1.1

SHEET 1 OF 3

PROJECT # 1291C

Plant Legend

TREES	QTY	BOTANICAL / COMMON NAME	SIZE	MATURE SIZE	
	AP	3	Acer platanoides "Crimson King" / Crimson King Maple	11/2" Cal., B&B	Large
	AS	16	Arbutus unedo / Strawberry Tree	11/2" Cal., B&B	Small
	CG	2	Chamaecyparis nootkatensis "Glauca Pendula" / Weeping Nootka False Cypress	5" Ft. Min., B&B	Large
	SA	2	Sorbus aucuparia / European Mountain Ash	11/2" Cal., B&B	Large
SHRUBS	QTY	BOTANICAL / COMMON NAME	SIZE		
	SIL	10	Elaeagnus x ebbingei "Gilt Edge" / Ebbing Silverberry	3 Gal.	
	ESC	15	Escallonia x exoniensis "Fradesi" / Pink Escallonia	3 Gal.	
	HOL	6	Ilex crenata "Drops of Gold" / Drops of Gold Japanese Holly	3 Gal.	
	GRP	21	Mahonia aquifolium / Oregon Grape	3 Gal.	
	SPI	4	Spiraea japonica "Goldflame" / Spirea	3 Gal.	
	VIB	25	Viburnum davidii / David Viburnum	3 Gal.	
GRASSES/PERENNIALS/ GROUND COVERS	QTY	BOTANICAL / COMMON NAME	SIZE		
	HEA	29	Erica carnea / Winter Heath	1 Gal.	
	BLU	32	Helictotrichon sempervirens / Blue Oat Grass	1 Gal.	
	DAY	21	Hemerocallis Spp. / Daylily	1 Gal.	
GROUND COVERS	QTY	BOTANICAL / COMMON NAME	SIZE	SPACING	
	KINN	88	Arctostaphylos uva-ursi "Massachusetts" / Massachusetts Manzanita	4" Pot	30" o.c.
	CEAN	35	Ceanothus gloriosus "Point Reyes" / Point Reyes Ceanothus	1 Gal.	48" o.c.
	PHLX	95	Phlox subulata / Moss Phlox	4" Pot	18" o.c.

General Planting Notes:

1. THE LANDSCAPE CONTRACTOR IS TO THOROUGHLY REVIEW THE SITE. IF THERE ARE ANY DISCREPANCIES BETWEEN THE PLAN AND EXISTING CONDITIONS THE LANDSCAPE ARCHITECT IS TO BE IMMEDIATELY NOTIFIED.
2. IF THE LANDSCAPE CONTRACTOR STARTS WORK BEFORE SITE CONDITIONS ARE READY, THEY WILL BE RESPONSIBLE FOR ANY ADDITIONAL COSTS RELATING TO THE CONDITION.
3. PLANT MATERIALS SHALL BE FREE OF DISEASE, INJURY, AND INSECT INFESTATION. UNHEALTHY OR DAMAGED PLANTS SHALL BE REPLACED BY LANDSCAPE CONTRACTOR.
4. TOPSOIL: ALL SHRUB BEDS SHALL HAVE A MINIMUM DEPTH OF 6" WORKABLE TOPSOIL. TOPSOIL SHALL BE OVER ROCK-FREE SUBGRADE. TOPSOIL TO BE RIPPED AND TILLED INTO SUBGRADE. PARKING LOT PLANTERS WHERE CONCRETE/ASPHALT IS REMOVED, IMPORT 70/30 TOPSOIL/ORGANIC COMPOST TO PROVIDE 24" DEPTH WORKABLE SOIL IN ALL PLANTER BEDS.
5. MOUND PLANTING BED AREAS 3% FOR POSITIVE DRAINAGE AND AESTHETICS.
6. SOIL AMENDMENTS: ADD 2" CLEAN COMPOST TO TOPSOIL, TILL IN, FOR ALL PLANTER BEDS.
7. BARK MULCH: SPREAD 2" MIN. DEPTH CHEMICAL FREE AGED DOUG FIR MULCH IN ALL PLANTER BED AREAS AND TREE CUT OUTS. KEEP MULCH AWAY FROM PLANT BASE.
8. PLANT QUANTITIES SHOWN ARE INTENDED TO ASSIST THE CONTRACTOR IN EVALUATING THEIR OWN TAKE-OFFS. IF THERE IS A DISCREPANCY BETWEEN PLANT QUANTITIES AND SYMBOLS SHOWN, USE THE LARGER OF THE TWO AMOUNTS. CONTRACTOR IS RESPONSIBLE FOR ALL FINAL QUANTITIES.

General Notes:

1. VERIFY ALL UTILITIES AND EASEMENTS. SEE CIVIL DRAWINGS FOR MORE INFORMATION.
2. SEE ARCHITECTURAL DRAWINGS FOR SITE PLAN AND BUILDING DETAILS.
3. PLANTS SELECTED FROM LINCOLN CITY GUIDE TO LANDSCAPE SELECTIONS.
4. PLANTING PLAN SEE SHEET L1.1.
5. IRRIGATION PLAN SEE SHEET L2.1.

Laurus Designs, LLC



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Silverton, Oregon
503.784.6494

Excellence, Inc.

2219 NW Highway 101
Lincoln City, Oregon



PLANT LEGEND, NOTES AND DETAILS

May 26th, 2020

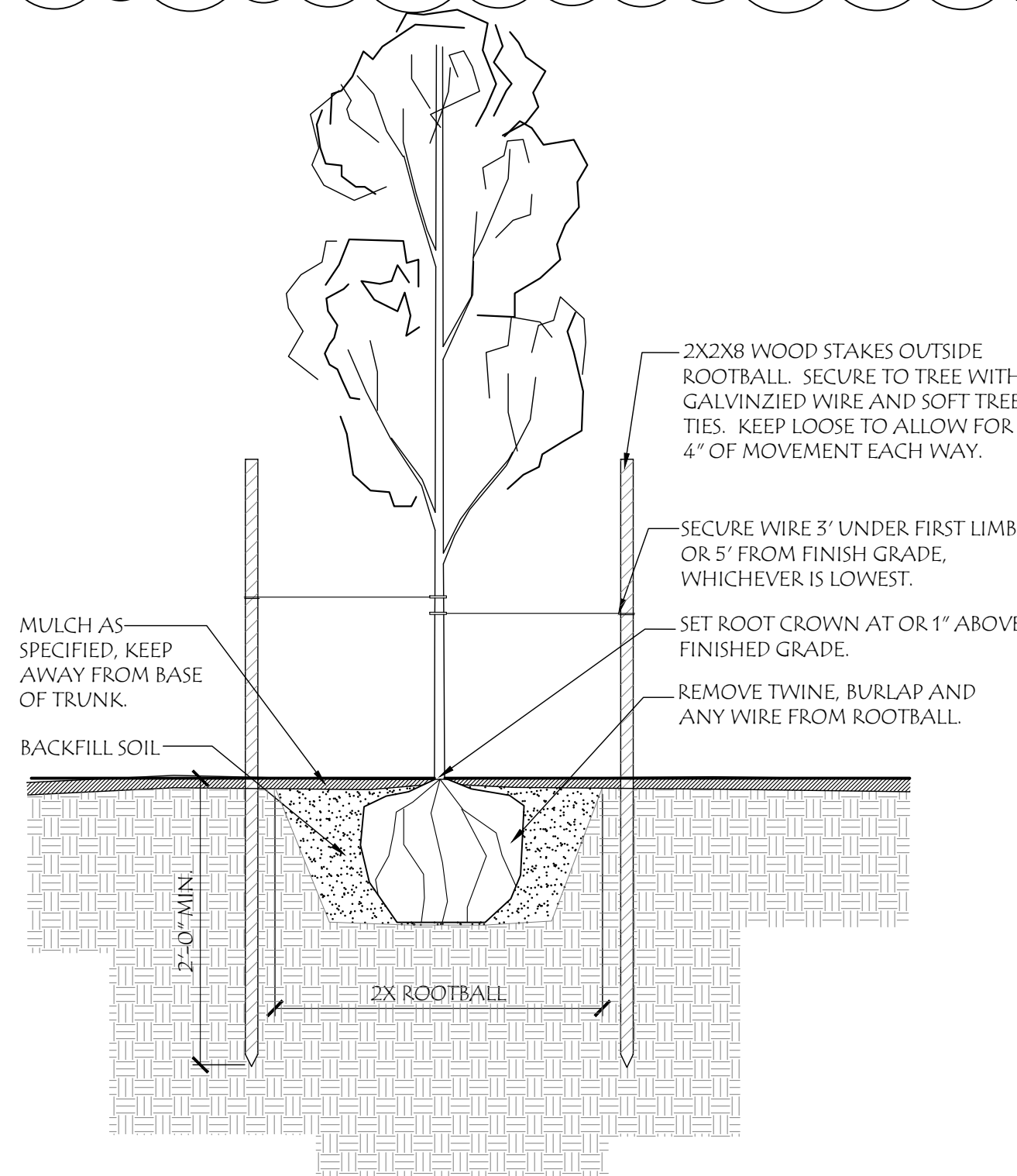
REVISIONS

#	DATE	NOTES	INITIALS
1	5.24.21	CITY COMMENTS	LAA

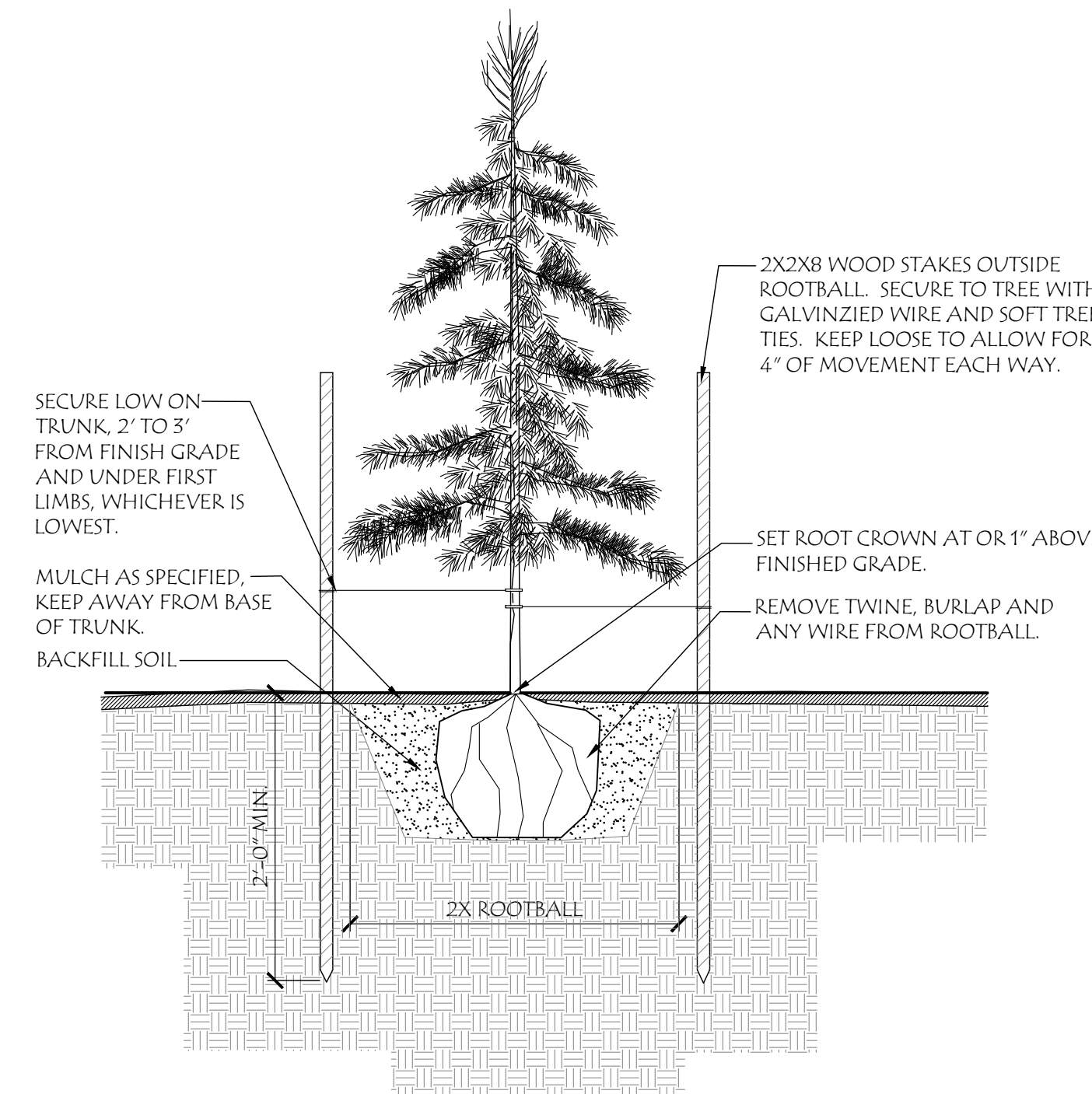
L1.2

SHEET 2 OF 3

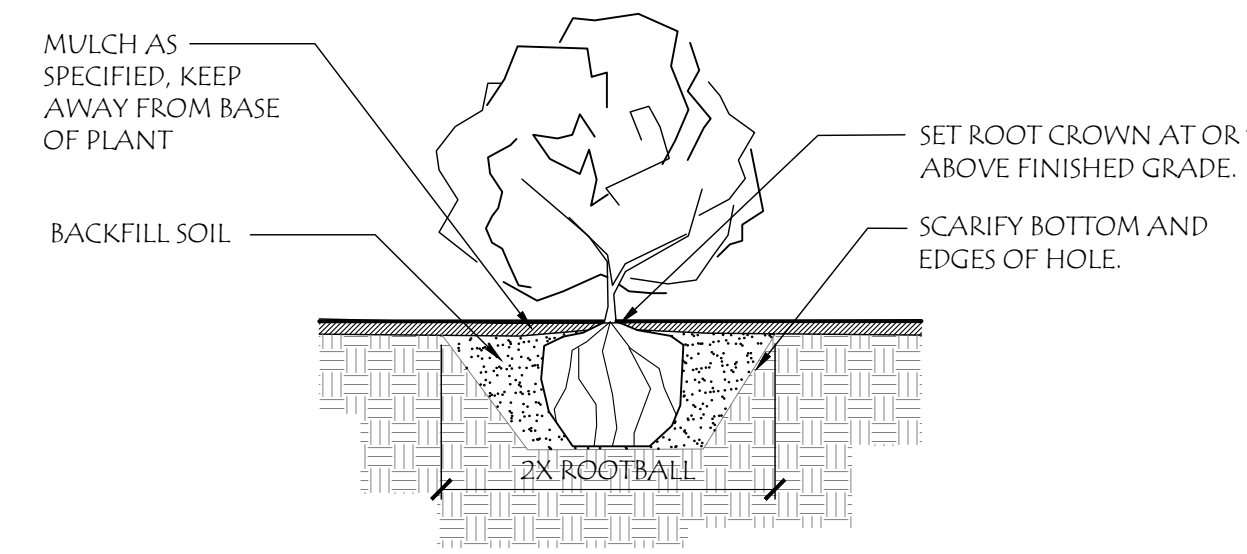
PROJECT #: 1291C



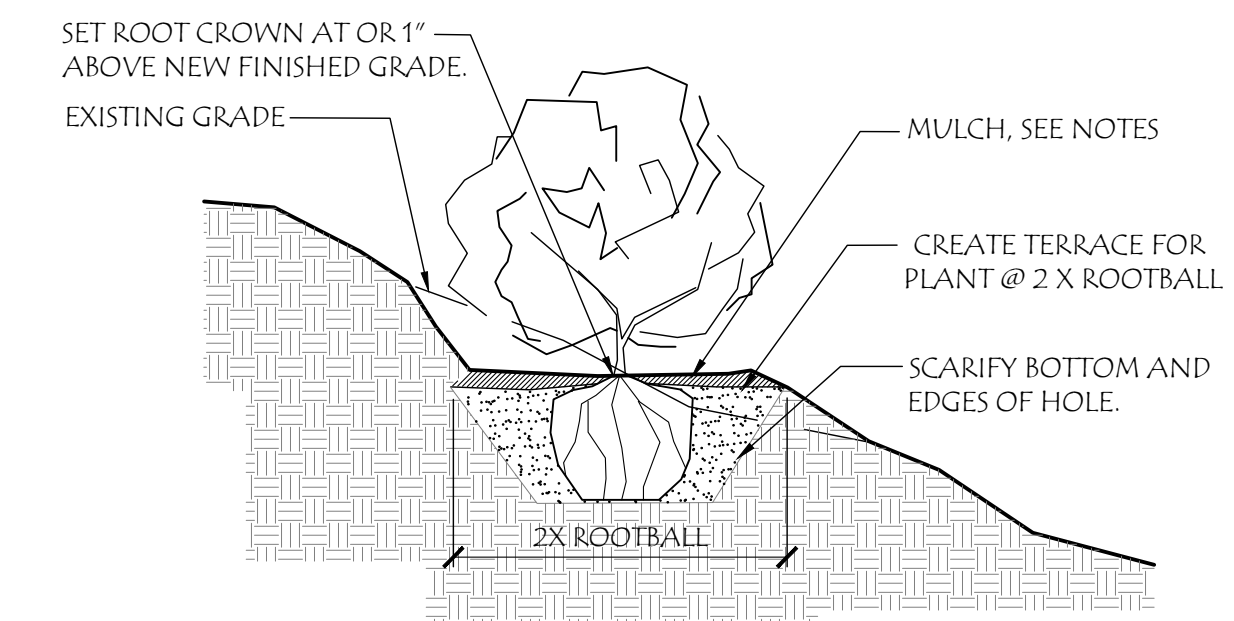
1 TREE PLANTING DETAIL
SCALE: NTS



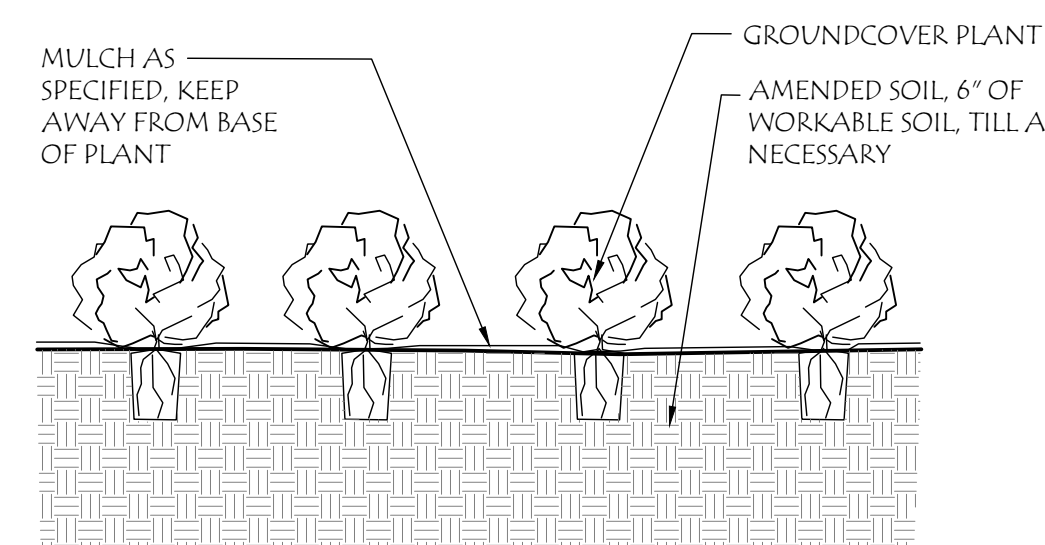
2 CONIFER TREE PLANTING DETAIL
SCALE: NTS



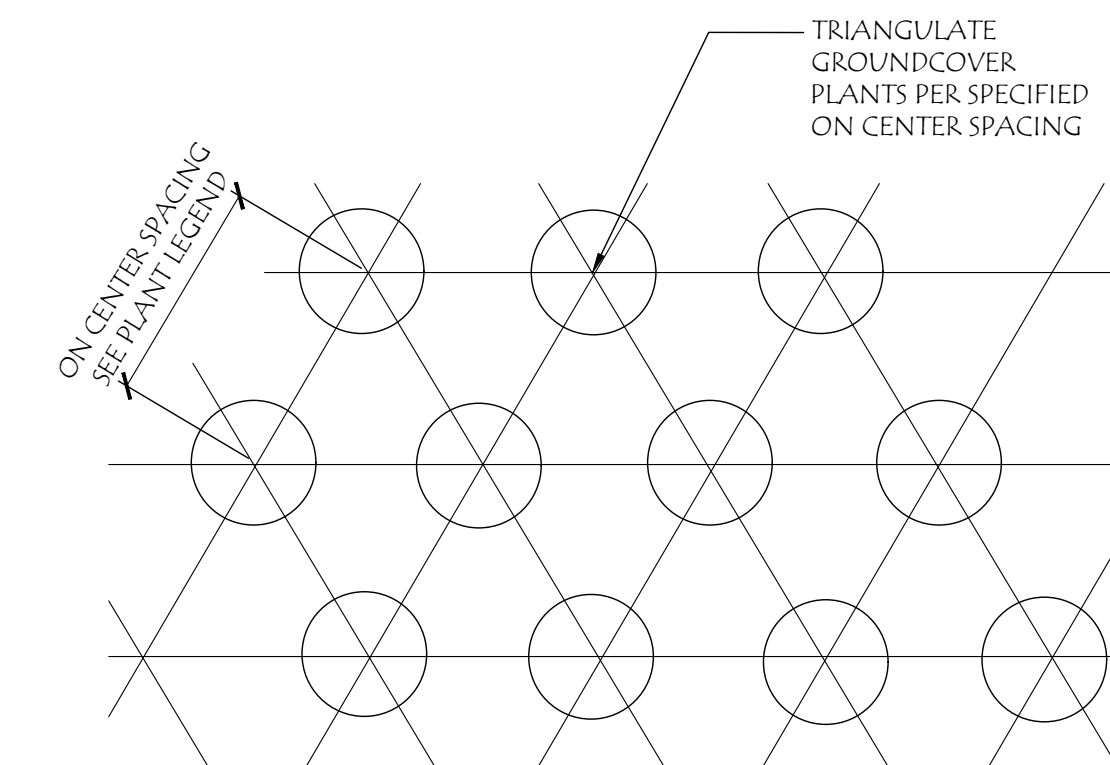
3 SHRUB PLANTING DETAIL
SCALE: NTS



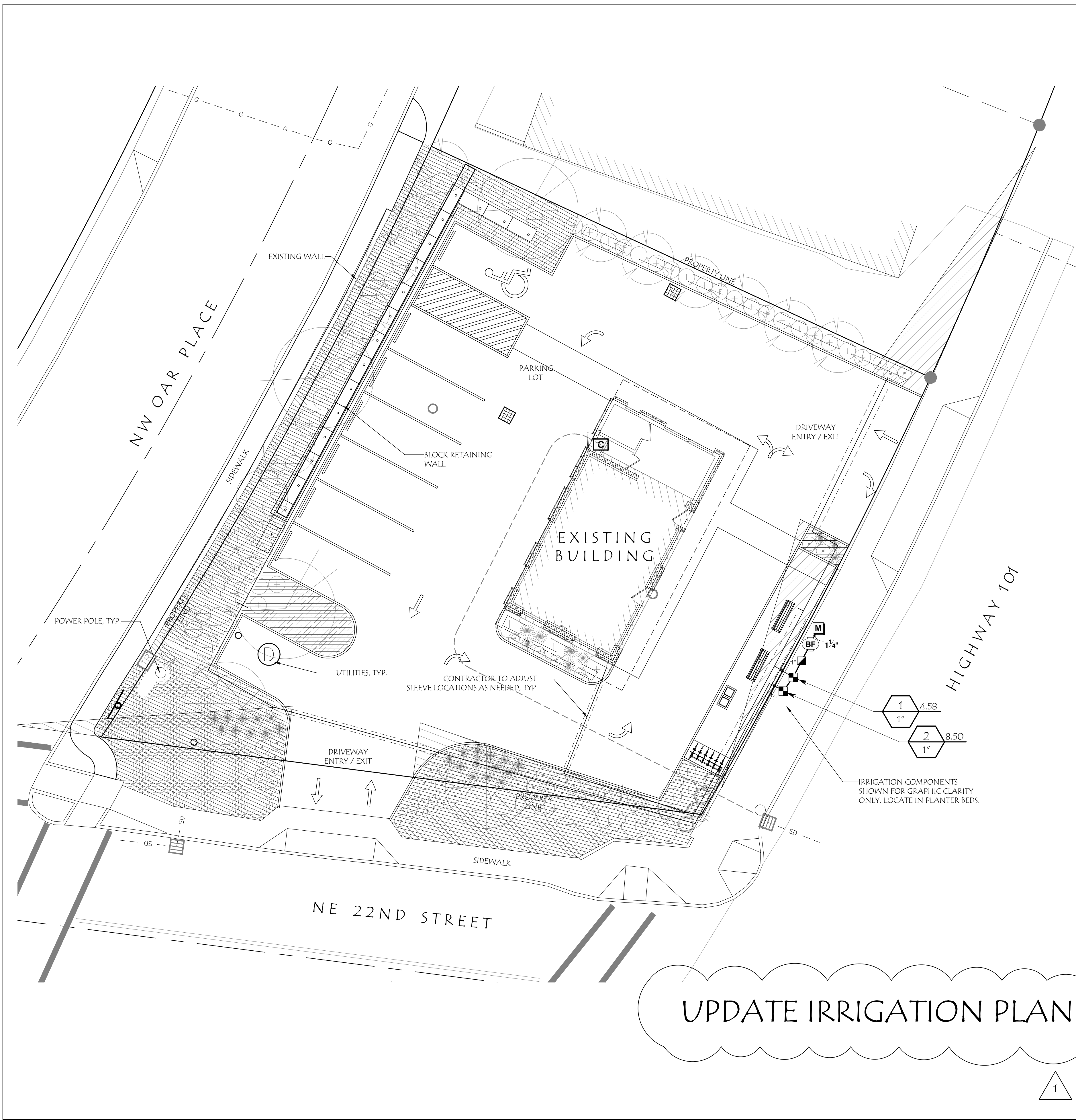
4 SLOPE PLANTING DETAIL
SCALE: NTS



5 GROUNDCOVER PLANTING DETAIL
SCALE: NTS



6 GROUNDCOVER SPACING DETAIL
SCALE: NTS



General Notes:

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3. SEE CIVIL DRAWINGS FOR RETAINING WALL AND GRADING.
4. PLANTING PLAN SEE SHEET L1.1.

Critical Analysis

P.O.C. NUMBER: 01
Water Source Information:

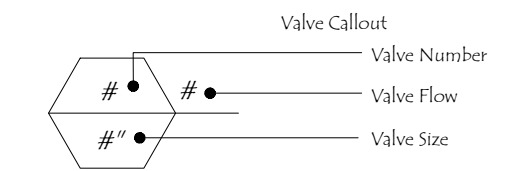
FLOW AVAILABLE	
Water Meter Size:	1"
Flow Available:	37.50 gpm
PRESSURE AVAILABLE	
Static Pressure at POC:	60.00 psi
Elevation Change:	5.00 ft
Service Line Size:	3"
Length of Service Line:	20.00 ft
Pressure Available:	58.00 psi
DESIGN ANALYSIS	
Maximum Station Flow:	8.50 gpm
Flow Available at POC:	37.50 gpm
Residual Flow Available:	29.00 gpm
Pressure Req. at Critical Station:	46.82 psi
Loss for Fittings:	0.01 psi
Loss for Main Line:	0.07 psi
Loss for POC to Valve Elevation:	0.00 psi
Loss for Backflow:	5.50 psi
Loss for Water Meter:	0.55 psi
Critical Station Pressure at POC:	52.95 psi
Pressure Available:	58.00 psi
Residual Pressure Available:	5.05 psi

Irrigation Notes

1. IRRIGATION SYSTEM DESIGN BASED ON 37.5 GPM AT 60 PSI. IF METER SIZE, FLOW (GPM) AND/OR STATIC PRESSURE (PSI) VARY, CONTACT LANDSCAPE ARCHITECT.
2. IRRIGATION DESIGN IS FROM THE POINT OF CONNECTION (POC) ONLY. THE DESIGN IS BASED ON GALLONS PER MINUTE (GPM) AND POUNDS PER SQUARE INCH (PSI).
3. IRRIGATION CONTRACTOR IS TO VERIFY POINT OF CONNECTION IN THE FIELD. INSTALLER IS TO CONFIRM THE MINIMUM DISCHARGE REQUIREMENTS OF THE POINT OF CONNECTION AS INDICATED ON THE LEGEND PRIOR TO INSTALLATION.
4. THE PRESSURE REQUIREMENT AT THE POINT OF CONNECTION IS BASED ON NO MORE THAN 5- FEET OF ELEVATION CHANGE IN THE AREAS OF IRRIGATION.
5. ALL PRODUCTS SHALL BE INSTALLED IN ACCORDANCE WITH THE MANUFACTURER'S RECOMMENDATIONS AND ACCORDING TO LOCAL BUILDING, ELECTRICAL AND PLUMBING CODES.
6. IRRIGATION CONTRACTOR WILL ARRANGE INSPECTIONS REQUIRED BY LOCAL AGENCIES AND ORDINANCES DURING THE COURSE OF CONSTRUCTION AS REQUIRED. ALL WIRING TO BE PER LOCAL CODE. BACKFLOW PREVENTION PER LOCAL CODE.
7. LOCATION OF IRRIGATION COMPONENTS SHOWN ON DRAWINGS IS APPROXIMATE. ACTUAL PLACEMENT MAY VARY SLIGHTLY. PIPE LOCATIONS ARE DIAGRAMMATIC. VALVES AND MAINLINE SHOWN IN PAVED AREAS ARE FOR GRAPHIC CLARITY ONLY.
8. INSTALL IRRIGATION MAINS WITH A MINIMUM 18" OF COVER BASED ON FINISH GRADES. INSTALL IRRIGATION LATERALS WITH A MINIMUM 12" OF COVER BASED ON FINISH GRADES.
9. USE IN-LINE CHECK VALVES TO AVOID LOW LINE DRAINAGE.
10. DRIP LINE: DO NOT EXCEED 200 LINEAR FEET OF CONTINUOUS DRIP LINE. STAKE AS NEEDED TO KEEP IN PLACE. DRIP LINE IS DIAGRAMMATIC AND SHOULD SURROUND AT LEAST TWO SIDES OF EACH PLANT. COVER WITH MULCH. (SEE PLANTING PLAN SHEET L1.1.)
11. CONTROLLER TO BE LOCATED IN BUILDING ELECTRICAL CLOSET.
12. ALL WIRE SPLICES OR CONNECTIONS SHALL BE MADE WITH APPROVED WATERPROOF WIRE CONNECTORS AND BE IN A VALVE OR SPLICE BOX.
13. ALL CONTROL WIRING DOWNSTREAM OF THE CONTROLLER IS TO BE 14 AWG, UL APPROVED DIRECT BURY.
14. THE DESIGN IS BASED ON THE SITE INFORMATION AND/OR DRAWING SUPPLIED WITH THE DESIGN CRITERIA BEING SET AREA TO BE IRRIGATED, EQUIPMENT MANUFACTURER AND MODEL TO BE USED, WATER SOURCE INFORMATION, ELECTRICAL POWER AVAILABILITY, ETC...).

Irrigation Schedule

SYMBOL	MANUFACTURER/MODEL/DESCRIPTION	QTY
■	Hunter ICZ-101-40 Drip Control Zone Kit. 1" ICV Globe Valve with 1" HY100 filter system. Pressure Regulation: 40Psi. Flow Range: 2 GPM to 20 GPM. 150 mesh stainless steel screen.	2
▨	Area to Receive Dripline Hunter HDL-06-18-CV HDL-06-18-CV: Hunter Dripline w/ 0.6 GPH emitters at 18" O.C. Check valve, dark brown tubing with gray striping. Dripline laterals spaced at 18" apart, with emitters offset for triangular pattern. Install with Hunter PLD barbed or PLD-LOC fittings.	1,878 l.f.
▨	Hunter HDL-06-18-CV HDL-06-18-CV: Hunter Dripline w/ 0.6 GPH emitters at 18" O.C. Check valve, dark brown tubing with gray striping. Install with Hunter PLD barbed or PLD-LOC fittings.	77.8 l.f.
SYMBOL	MANUFACTURER/MODEL/DESCRIPTION	QTY
■	Hunter HQ-44LRC Quick coupler valve, yellow rubber locking cover, red brass and stainless steel, with 1" NPT inlet, 2-piece body.	1
⊕	Zum 95OXL 1" Double Check Valve Assembly	1
⊕	Hunter NODE-200 2-Station Controller, Outdoor, Battery Powered. DC Latching Solenoid Ordered Separately.	1
M	Existing Water Meter 1", Verify Size	1
---	Irrigation Lateral Line: PVC Class 200 SDR 21	390.6 l.f.
---	Irrigation Mainline: PVC Schedule 40	13.2 l.f.
---	Pipe Sleeve: PVC Schedule 40, 2 x Line Size	112.6 l.f.



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Excellence, Inc.

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Lincoln City, Oregon

REGISTERED
643
LANDSCAPE ARCHITECT
LAURA A. ANTONSON
OREGON
11/16/2007

IRRIGATION PLAN

SCALE: 1"=10'-0"
0' 5' 10' 20'
SCALE

May 26th, 2020

REVISIONS

#	DATE	NOTES	INITIALS
1	5.24.21	CITY COMMENTS	LAA

L2.1

SHEET 3 OF 3

PROJECT # 1291C



SHEET INDEX

CS1.1	COVER SHEET
A1.1	ARCHITECTURAL SITE PLAN
A2.1	EXISTING DEMO/NEW FLOOR PLAN
A3.1	ADA BATHROOM LAYOUT
A4.1	EXISTING DEMO/NEW ELEVATIONS
G1.1	GENERAL NOTES
S1.1	EXISTING/DEMO/NEW PLANS
S3.1	BUILDING SECTIONS
S3.2	BUILDING SECTIONS
S5.1	FOUNDATION DETAILS
S8.1	ROOF DETAILS
S8.2	CANOPY DETAILS
S9.1	TRASH ENCLOSURE
C1.0	EXISTING CONDITIONS DEMOLITION & EROSION CONTROL PLAN
C1.1	EROSION CONTROL NOTES
C1.2	EROSION CONTROL DETAILS
C2.0	GRADING, DRAINAGE, AND SURFACING PLAN
C3.0	EXISTING IMPERVIOUS AREA
C4.0	CONSTRUCTION NOTES & TABLE
C5.0	CONSTRUCTION DETAILS
C5.1	CONSTRUCTION DETAILS
L1.1	PLANTING PLAN
L1.2	PLANT LEGEND, NOTES AND DETAILS

NOTE: ALL MECHANICAL, ELECTRICAL, PLUMBING & ACOUSTICAL CEILING TILE TO BE PROVIDED BY OTHERS UNDER DEFERRED SUBMITTAL



VICINITY MAP
 N.T.S.

CODE STUDY	
OCCUPANT CLASSIFICATION: BUSINESS GROUP B	
ALLOWABLE HEIGHT	= 40'-0" (TYPE VB, NON-SPRINKLED)
ACTUAL HEIGHT	= 18'-6 1/2" (TOP OF PARAPET) OK
ALLOWABLE STORIES	= 2
ACTUAL STORIES	= 1 OK
ALLOWABLE AREA	= 9000SF
EXISTING AREA	= 740.44SF
NEW BUILDING AREA	= 245.56SF
NEW CANOPY AREA	= 318.33SF
ACTUAL AREA	= 1304.33SF OK
TYPE OF CONSTRUCTION: TYPE V-B	
NO FIRE RESISTANCE RATED CONSTRUCTION REQUIRED	
MIN DISTANCE TO PROPERTY LINE	= 28'-0"
10' ≤ 28' < 30', VB B OCCUPANCY	= 0 HOUR RATING
OCCUPANT LOAD & EXITING:	
FUNCTION OF SPACE: BUSINESS AREA	
OCCUPANT LOAD FACTOR: 150 OCCUPANTS/SF (GROSS)	
OCCUPANT LOAD = 1304.33SF / 150 OCC/SF => (8) OCCUPANTS	
SPACES WITH ONE EXIT:	
MAX OCCUPANT LOAD	= 49 > 8 OK
MAX PATH OF EGRESS	= 100'
ACTUAL MAX PATH	= 36' OK
EGRESS ILLUMINATION REQUIRED	

JOB NO. 190418
 DRAWN BY: JCP
 REVISION: 05-20-21
 REVISION COMMENTS
 CHECKED BY: CBS
 DATE: 05-26-20

MSC
 ENGINEERS
 SINCE 1925
 CONSULTING STRUCTURAL ENGINEERS
 9700 Macleod Place NE
 Salem, OR 97301
 503.399.1397
 mscengineersinc.com

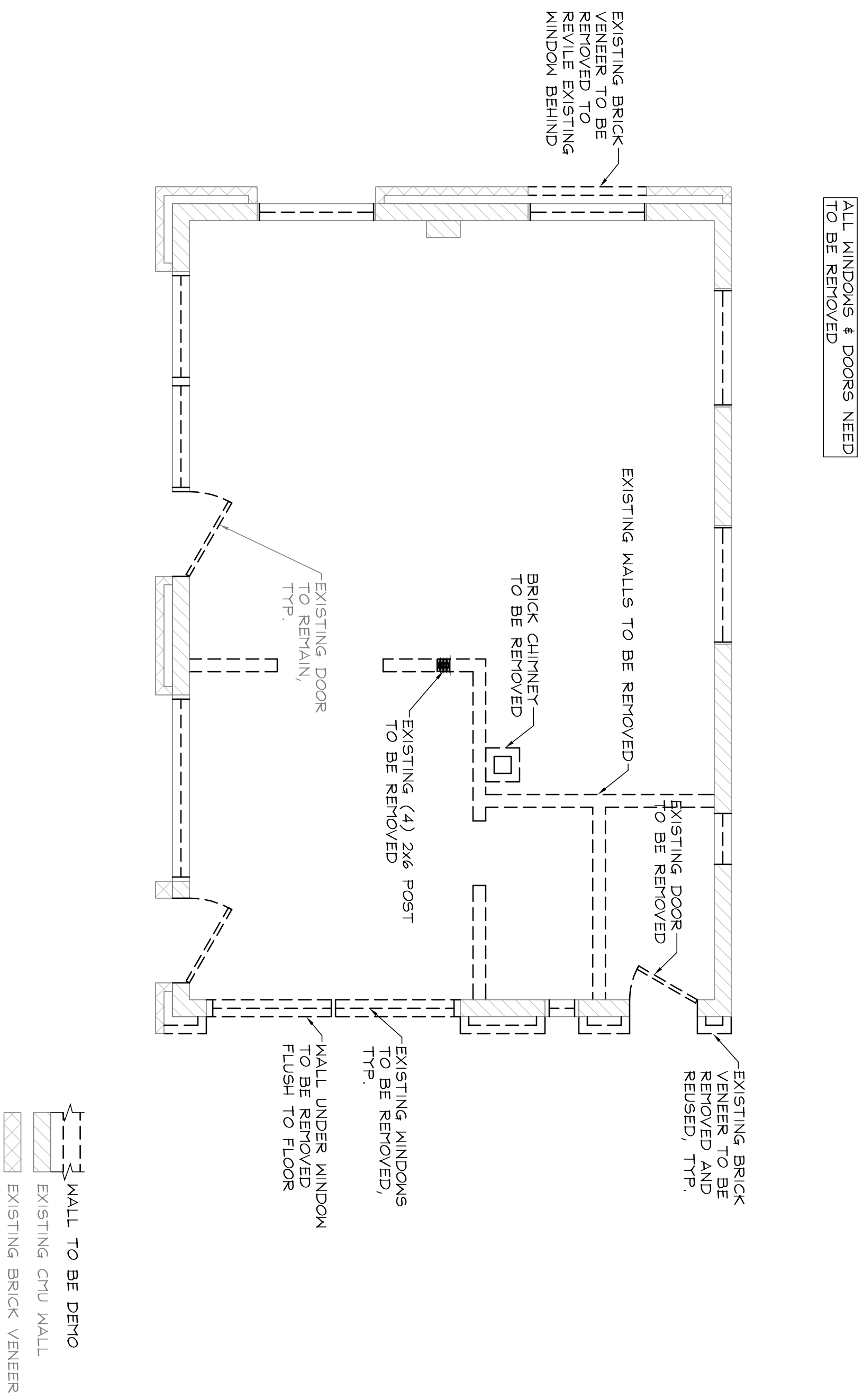
COVER SHEET
 2219 NW HWY 101
 2219 NW HWY 101
 LINCOLN CITY, OREGON
 FOR: EXCELLENCE INC.

REGISTERED PROFESSIONAL ENGINEER
 63202
 DIGITALLY SIGNED
 OREGON
 December 18, 2015
 CAMERON B. SWARTZ
 EXPIRES: JUNE 30, 2022

DWG. NO.
CS1.1
 1 of 13

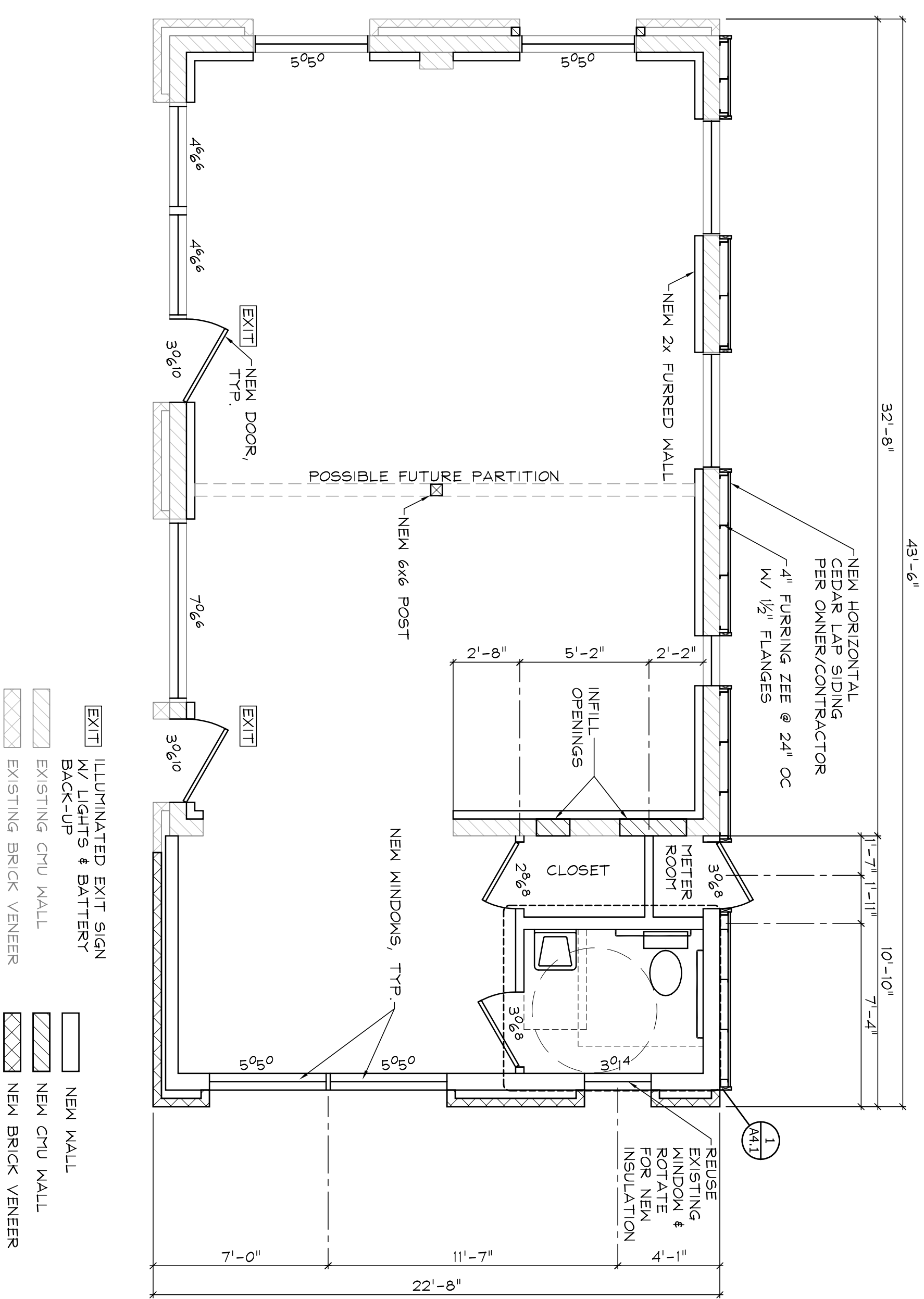
EXISTING/DEMO FLOOR PLAN

1/4" = 1'-0"



NEW PROPOSED FLOOR PLAN

1/4" = 1'-0"



ENERGY CONSERVATION:

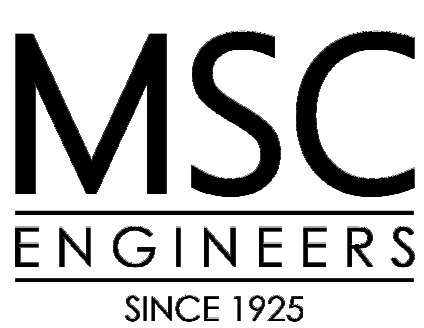
- A. Office - Walls R-15 @ 2x4, R-24 @ 2x8
- B. Windows - Roof R-20, E, K" Air Space
- C. Provide Norm Slide (In Winter) Vapor Barrier
- D. Per ASTM F710 when a floor covering is to be installed over slab, a 10mil vapor retarder with a permeance of 0.1 shall be installed under slab.
- E. Fenestration Product Rating
 1. (303.1.3) U-factors of fenestration products (window, doors and skylights) are determined in accordance with NFRC 100 by an accredited, independent laboratory and labeled and certified by the manufacturer or are determined using the commercial size category values listed in Chapter 15 of the 2017 ASHRAE Handbook of Fundamentals, Table No. 4 and shall include the effects of the window frame. The temporary label affixed to the fenestration products must not be removed prior to inspection.
 2. (303.1.3) Solar heat gain coefficient (SHGC) of glazed fenestration products (windows, doors and skylights) shall be determined in accordance with NFRC 200 by an accredited, independent laboratory, and labeled and certified by the manufacturer or be determined using the Solar Heat Gain Coefficients (SHGC) in Chapter 15 of the 2017 ASHRAE Handbook of Fundamentals, Table No. 10. The overall values must consider type of frame material and operator for the SHGC of fenestration products.
 3. Air Leakage, Insulation, and Component Certification
 4. (502.4.3) Sealing of the building envelope, openings and penetrations in the building envelope are sealed with caulking materials or closed with gasketing systems compatible with the construction materials and location. Joints and seams are sealed in the same manner or taped or covered with a moisture vapor-permeable wrapping material. Sealing materials spanning joints between construction materials allow for expansion and contraction of the construction materials.
 5. (502.4.11) Window and door assemblies that are part of the building envelope are determined in accordance with AIAA/WDMA/GSA 101/52/A440, or NFRC 400 by an accredited, independent laboratory, and labeled and certified by the manufacturer.

ACCESSIBILITY:

- A. All door hardware to be lever handles conforming to ADA Requirements.
- B. Door thresholds to conform to ADA requirements.
- C. Provide blocking in rest room walls for grab bar installation.

EXISTING DEMO/NEW FLOOR PLAN

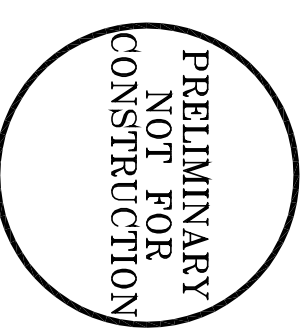
2219 NW HWY 101
 2219 NW HWY 101
 LINCOLN CITY, OREGON
 FOR: EXCELLENCE INC.



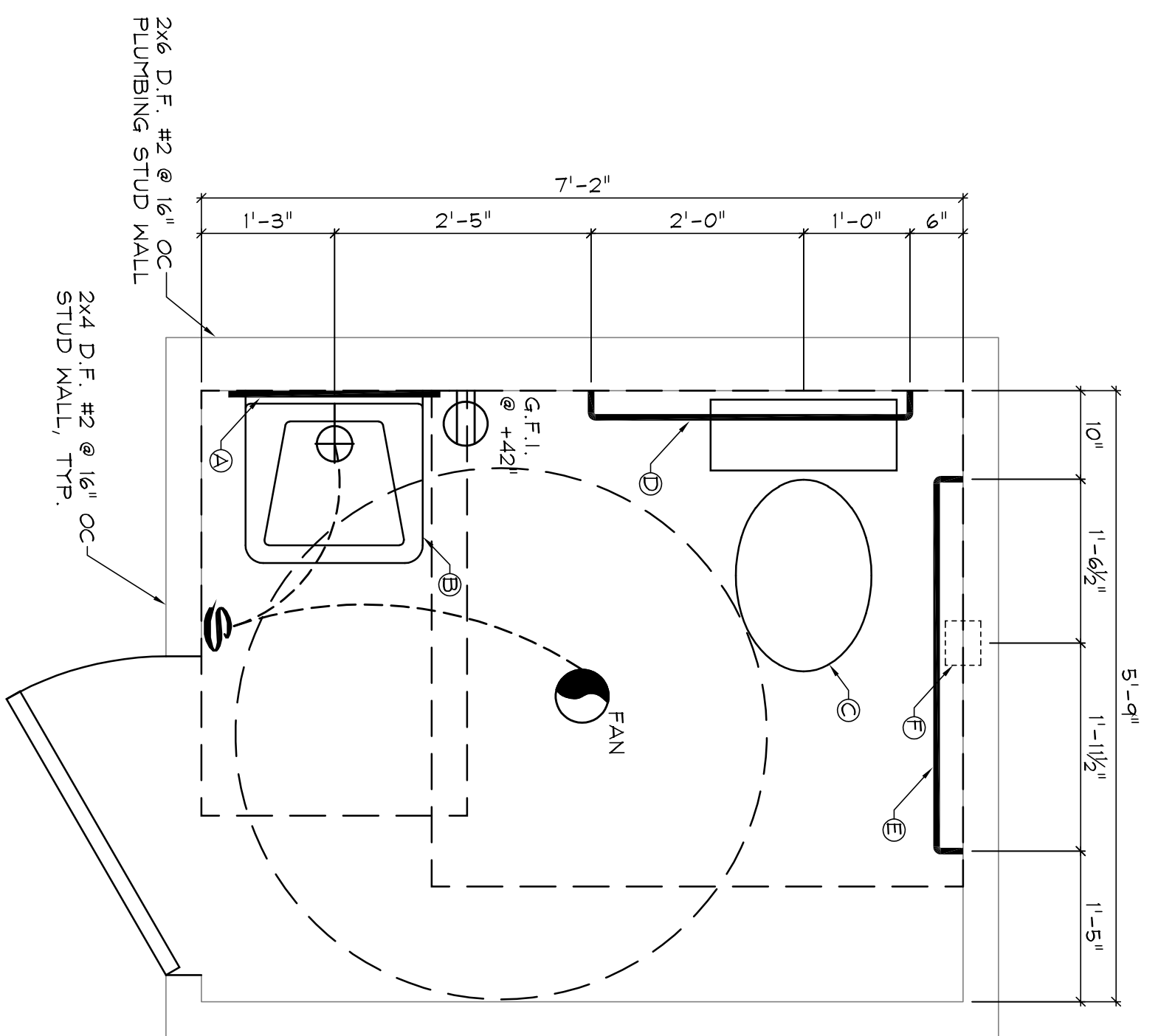
CONSULTING STRUCTURAL ENGINEERS
 3470 Pipeband Place NE
 Suite 120
 Salem, OR 97301
 503.399.1399
 mscengineersinc.com

JOB NO. 190418
 DRAWN BY: JCP
 REVISION:

CHECKED BY: CBS
 DATE: 05-18-20



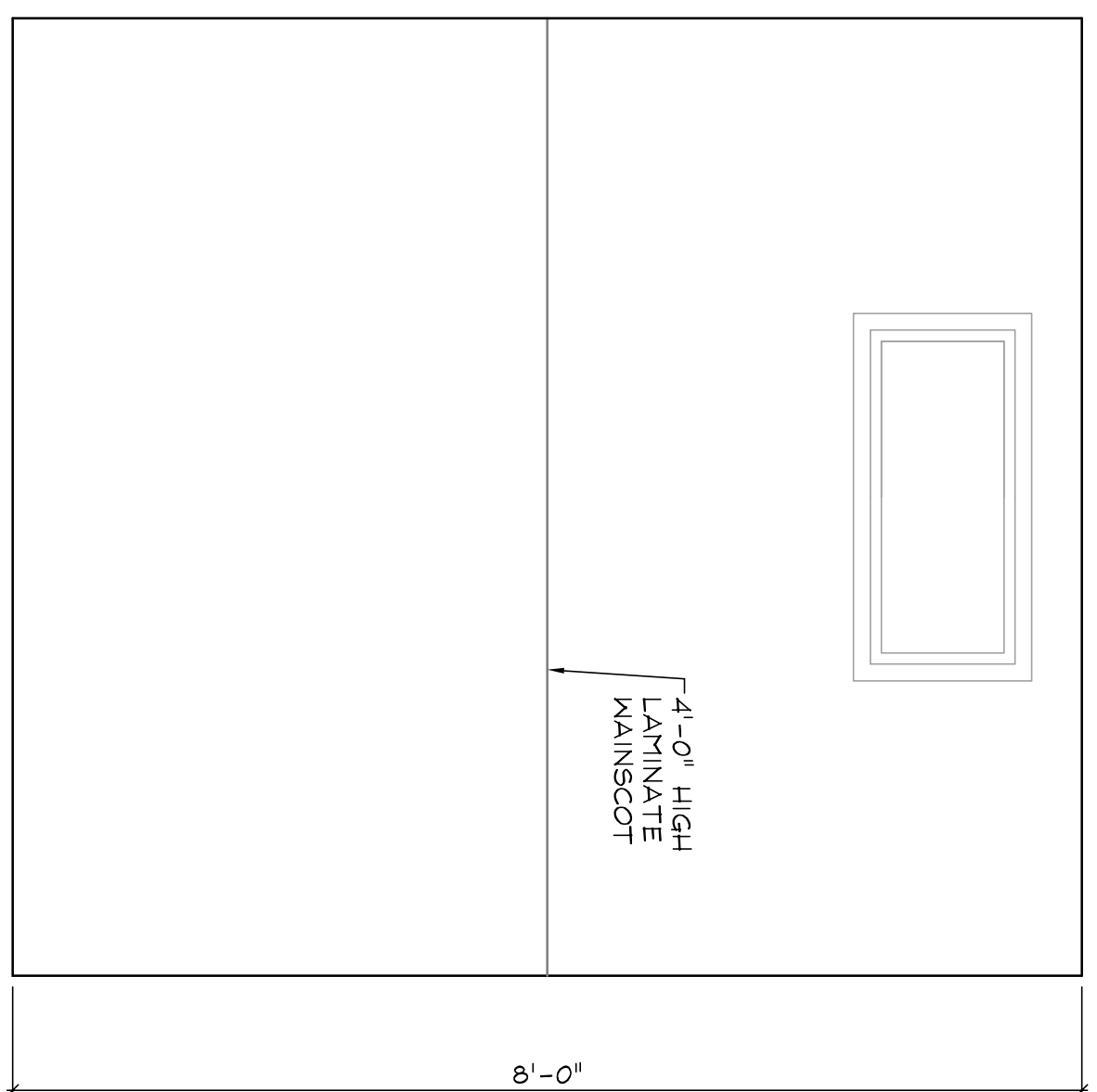
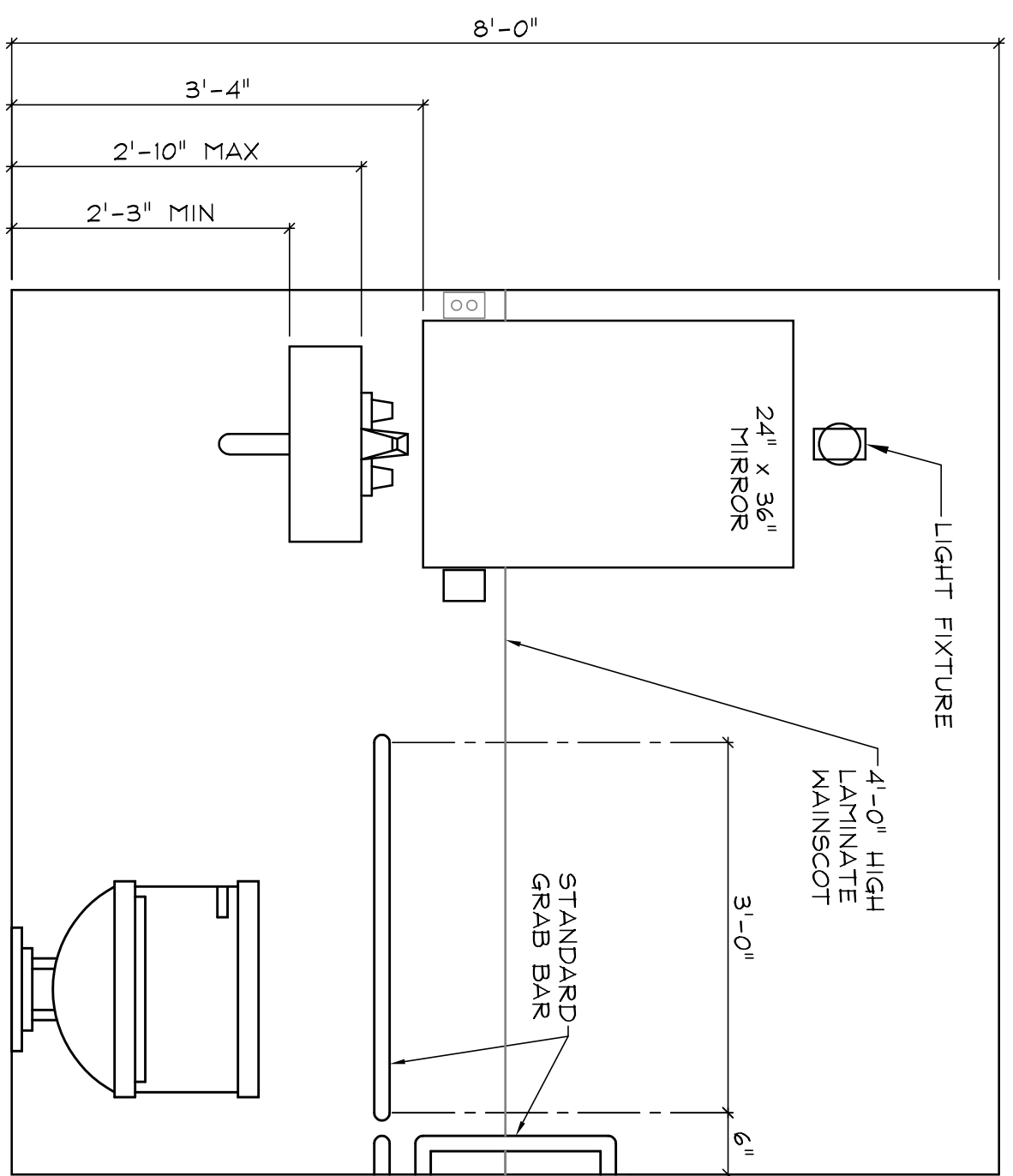
DWG. NO.
A2.1
 3 of 13



- A) 24" x 36" MIRROR BOTTOM @ +3'-4" A.F.F.
- B) 20" x 18" SINK-RIM @ +2'-10" MAX. A.F.F.
- C) MIN. 2'-5" CLEAR UNDER
- D) ELONGATED W.C. BOWLS
- E) W/ OPEN-FRONT SEATS SEAT @ +1'-6" (±1")
- F) 3'-0" GRAB BAR @ +3'-0" A.F.F. *
- G) 3'-6" GRAB BAR @ +3'-0" A.F.F. *
- H) T.P. HOLDER @ +1'-7" MIN. A.F.F.

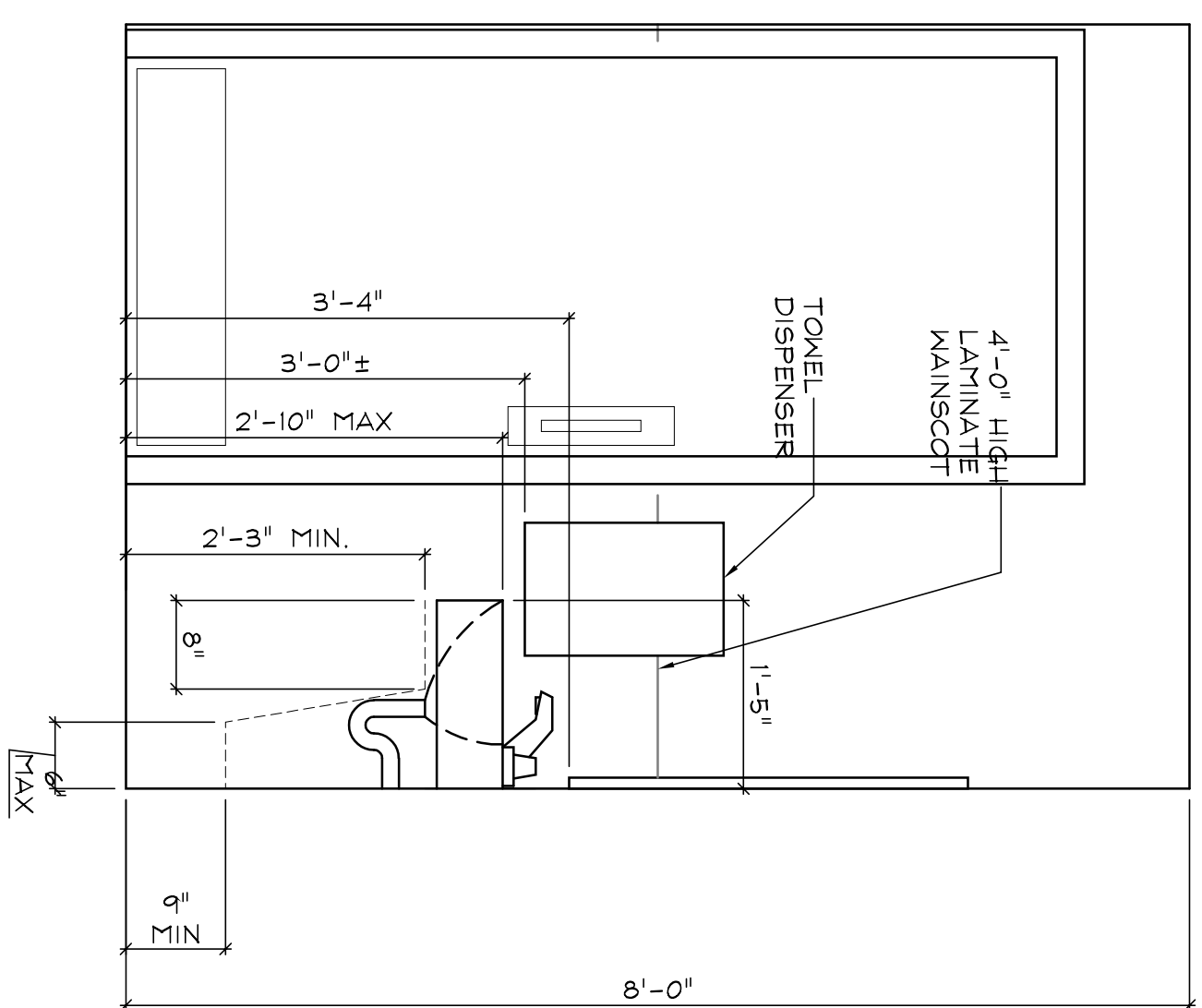
SOAP & TOWEL DISPENSER @ +3'-0" ±
 WALLS: R-13 INSULATION & G.M.B. FINISH
 CEILING: R-19 INSULATION & G.M.B. FINISH
 FLOOR: SH. VINYL OR V.C.T. W/ 5" COVE BASE
 * PROVIDE BLOCKING FOR GRAB BAR INSULATION

HANDICAP ACCESSIBLE TOILET ROOM
 3/4" = 1'-0"
 1
 A3.1

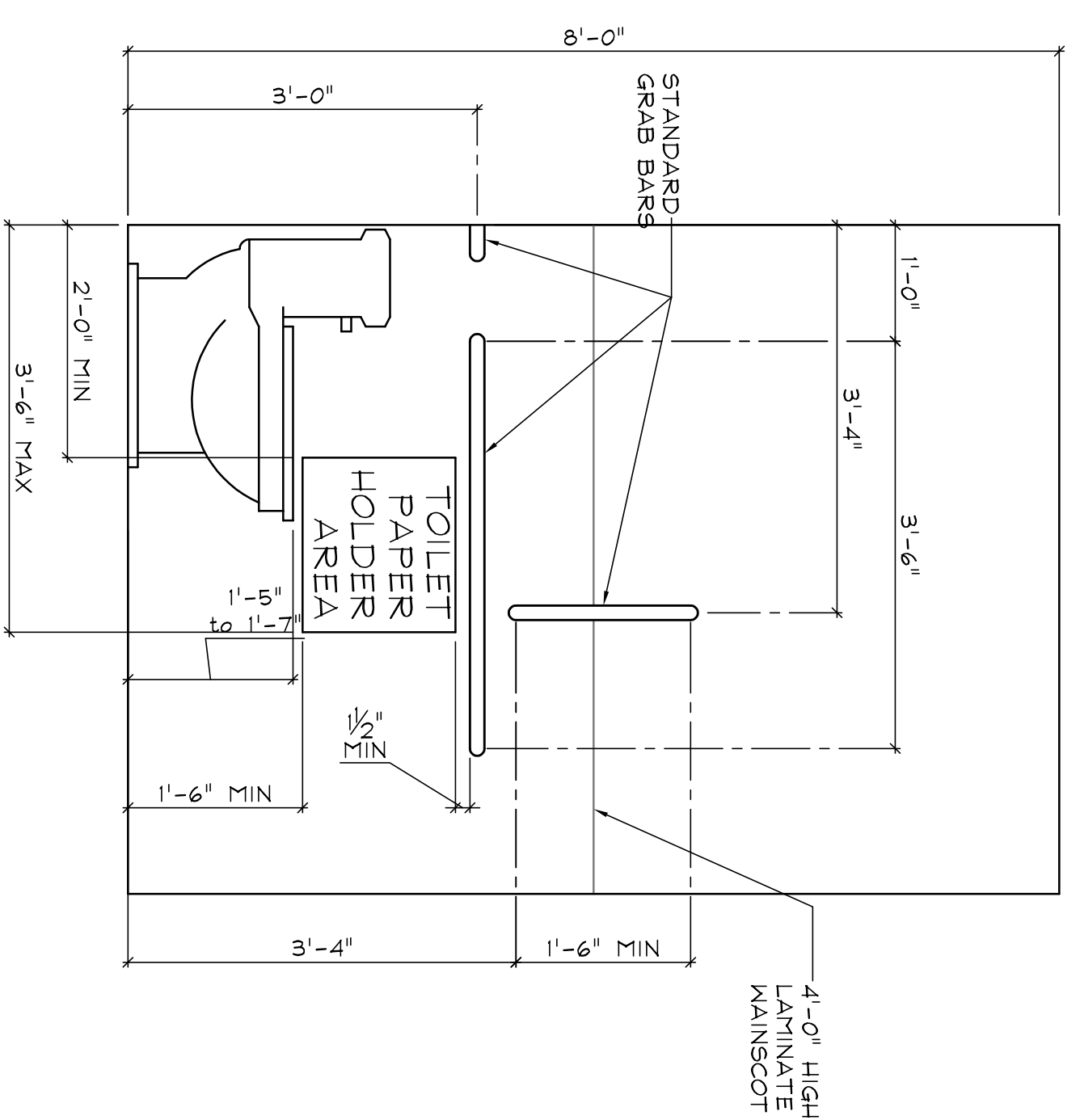


RESTROOM TOILET WALL ELEVATION
 3/4" = 1'-0"
 4
 A3.1

RESTROOM WALL ELEVATION
 3/4" = 1'-0"
 5
 A3.1



RESTROOM LAVATORY SIDE ELEVATION
 3/4" = 1'-0"
 2
 A3.1



RESTROOM TOILET SIDE ELEVATION
 3/4" = 1'-0"
 3
 A3.1

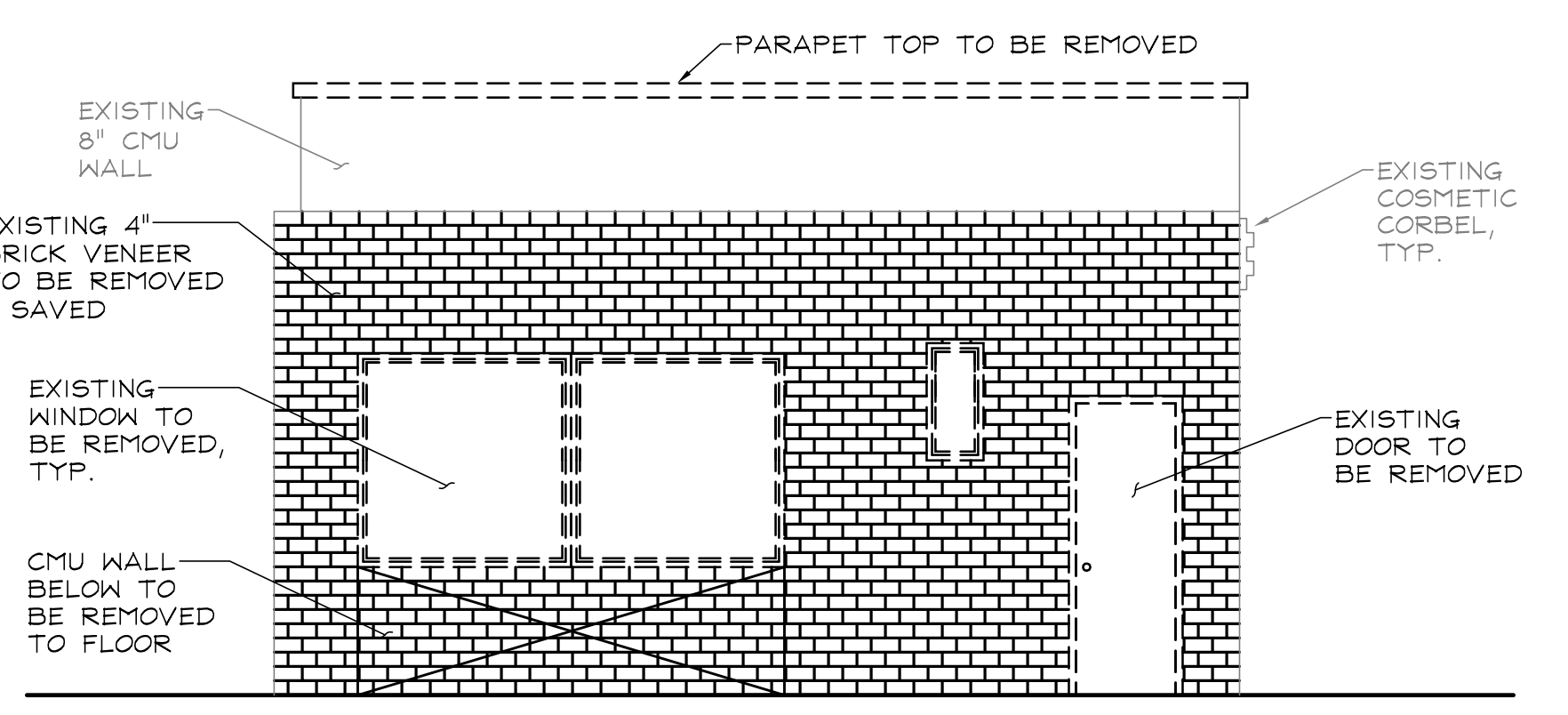
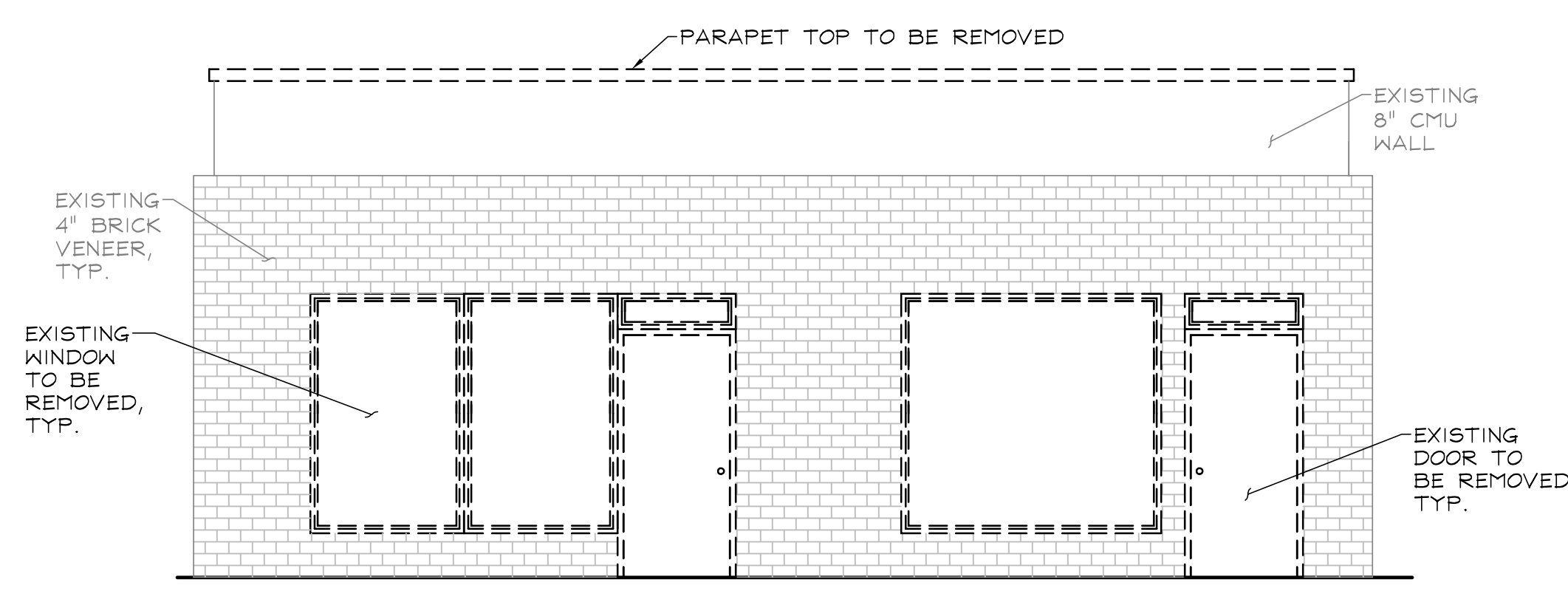
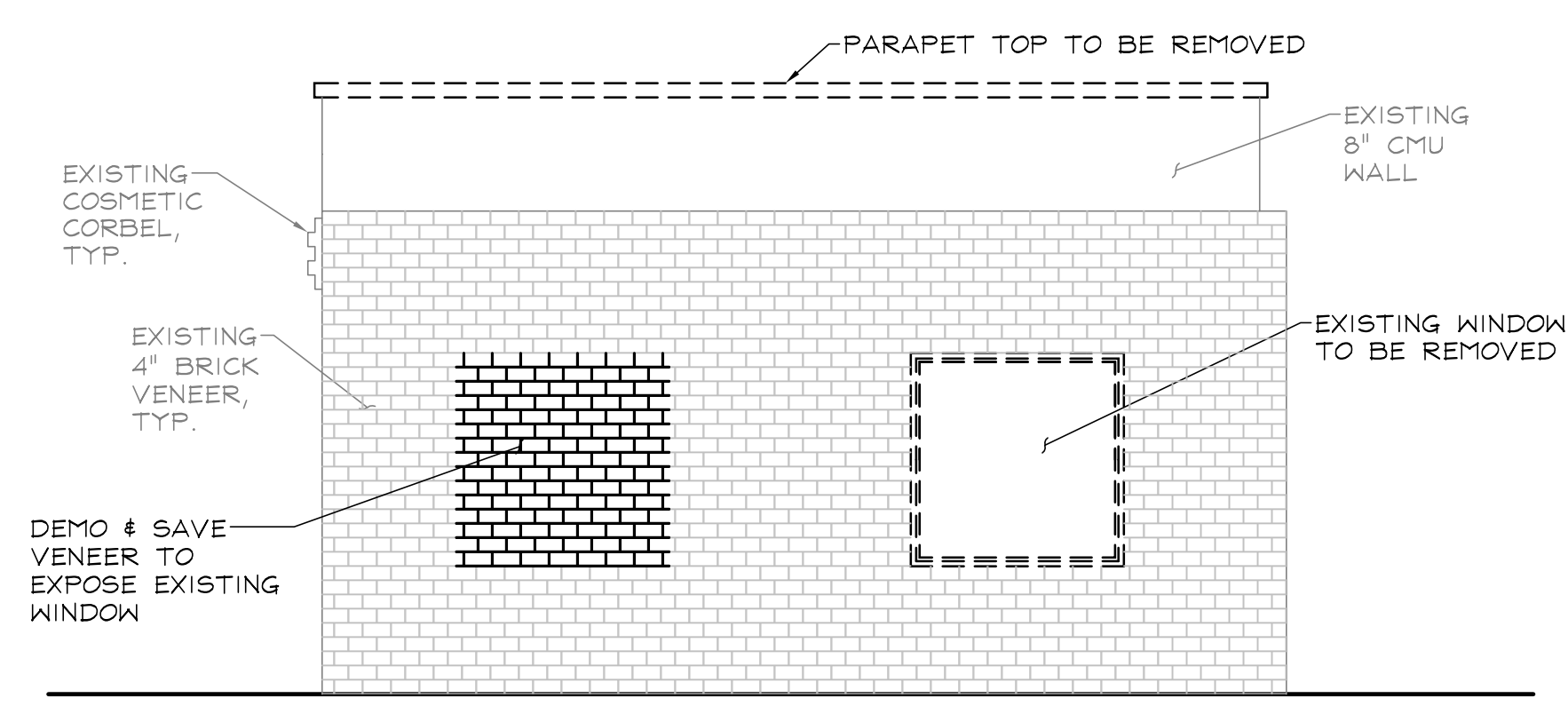
ADA BATHROOM LAYOUT
 2219 NW HWY 101
 2219 NW HWY 101
 LINCOLN CITY, OREGON
 FOR: EXCELLENCE INC.

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DWG. NO.
A3.1
 4 of 13
 190418

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 Suite 120
 Salem, OR 97301
 503.399.1399
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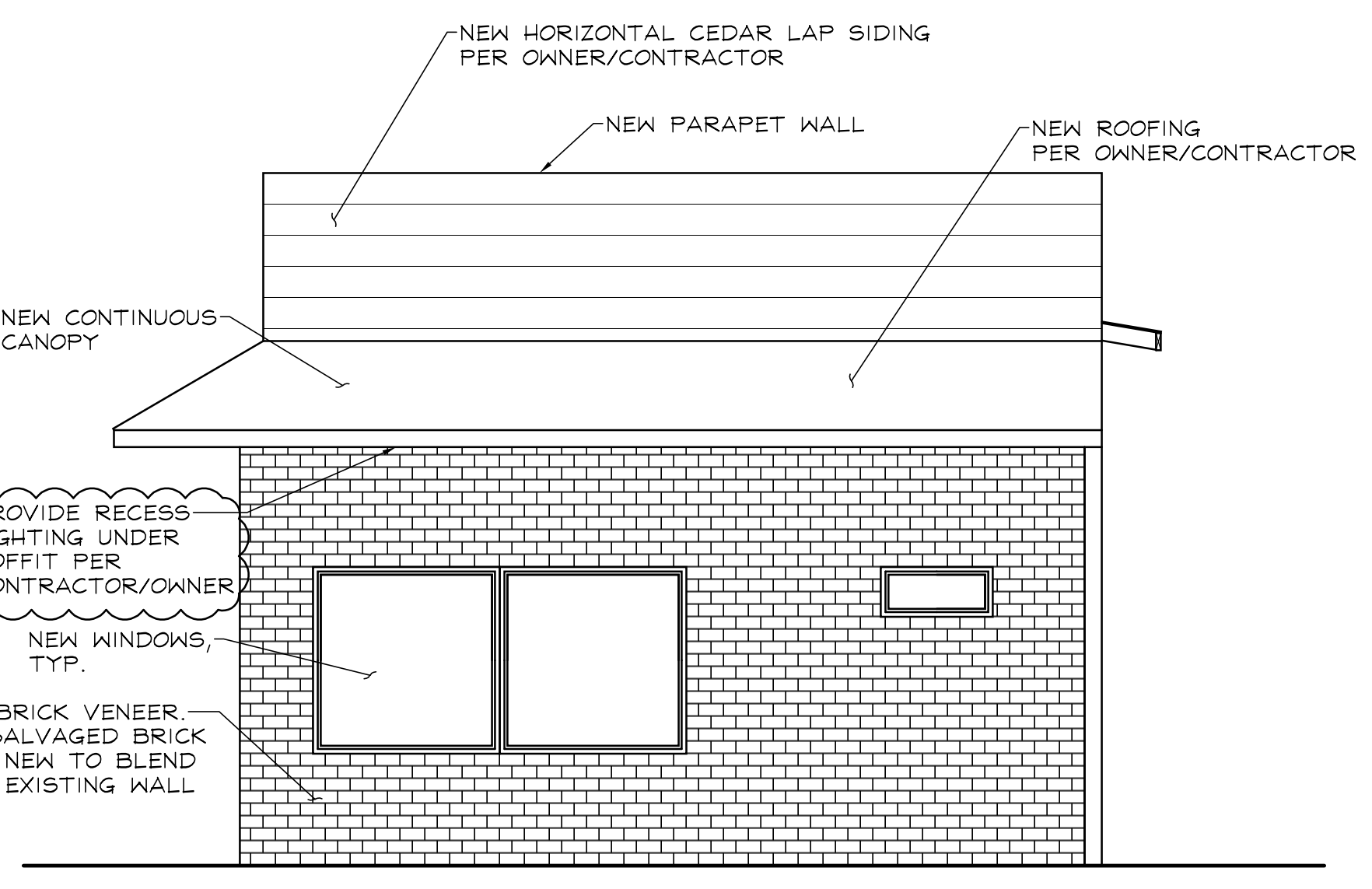
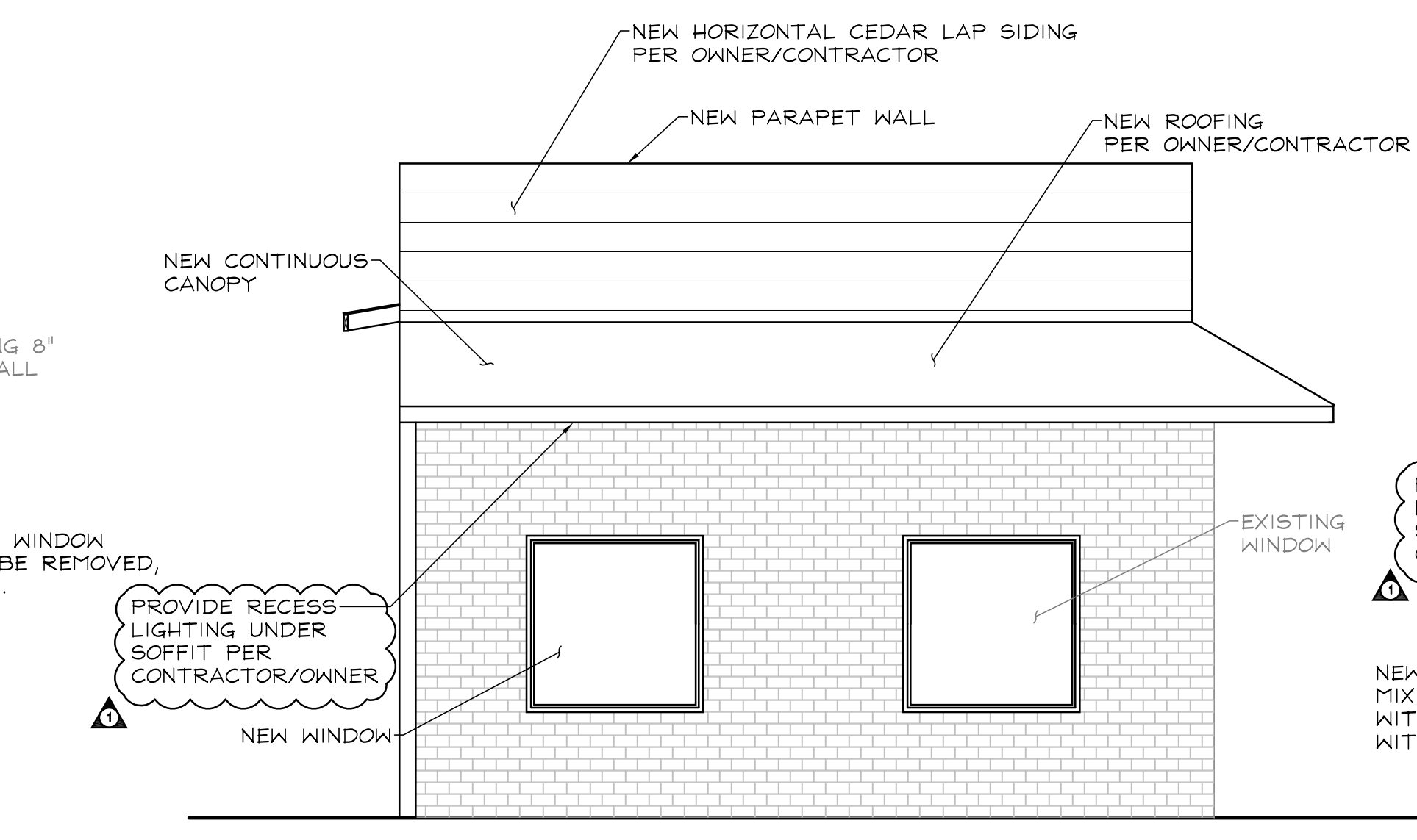
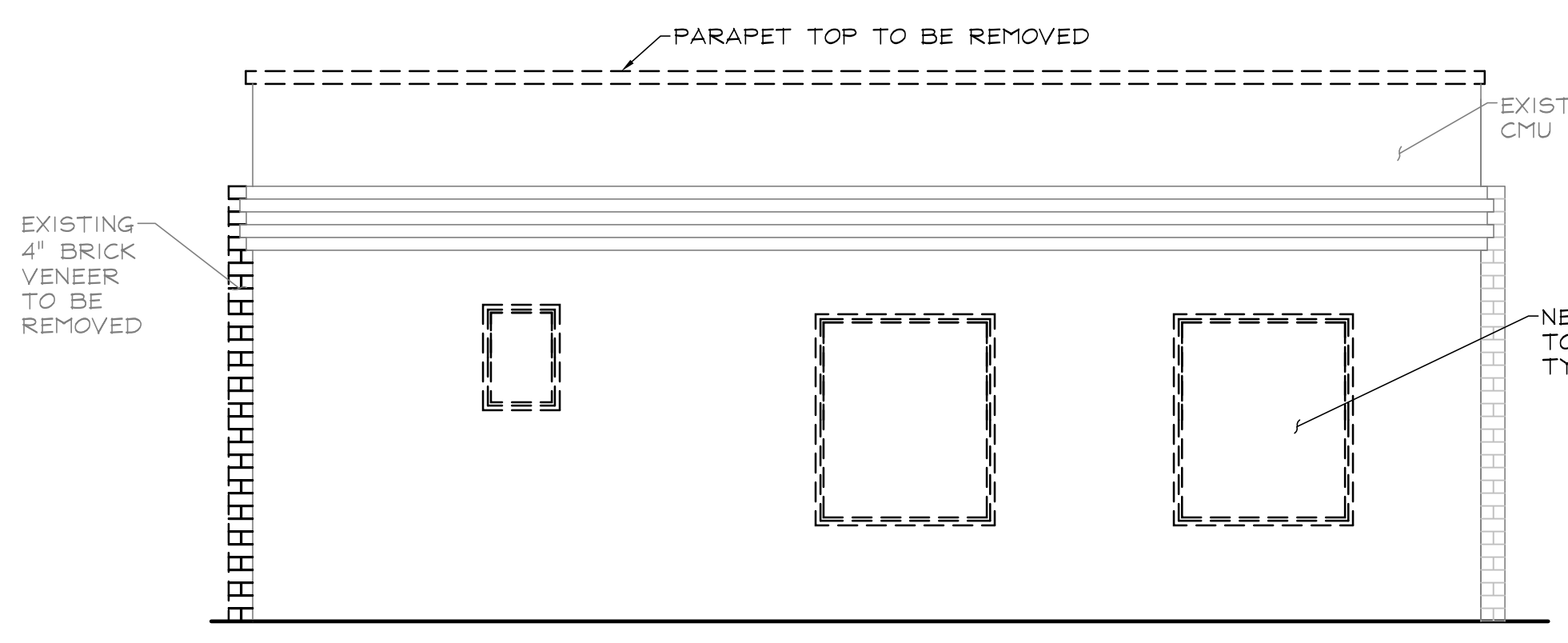
JOB NO. 190418
 DRAWN BY: JCP
 REVISION:
 CHECKED BY: CBS
 DATE: 05-18-20
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EXISTING SOUTH FACE ELEVATION
1/4" = 1'-0"

EXISTING EAST FACE ELEVATION
1/4" = 1'-0"

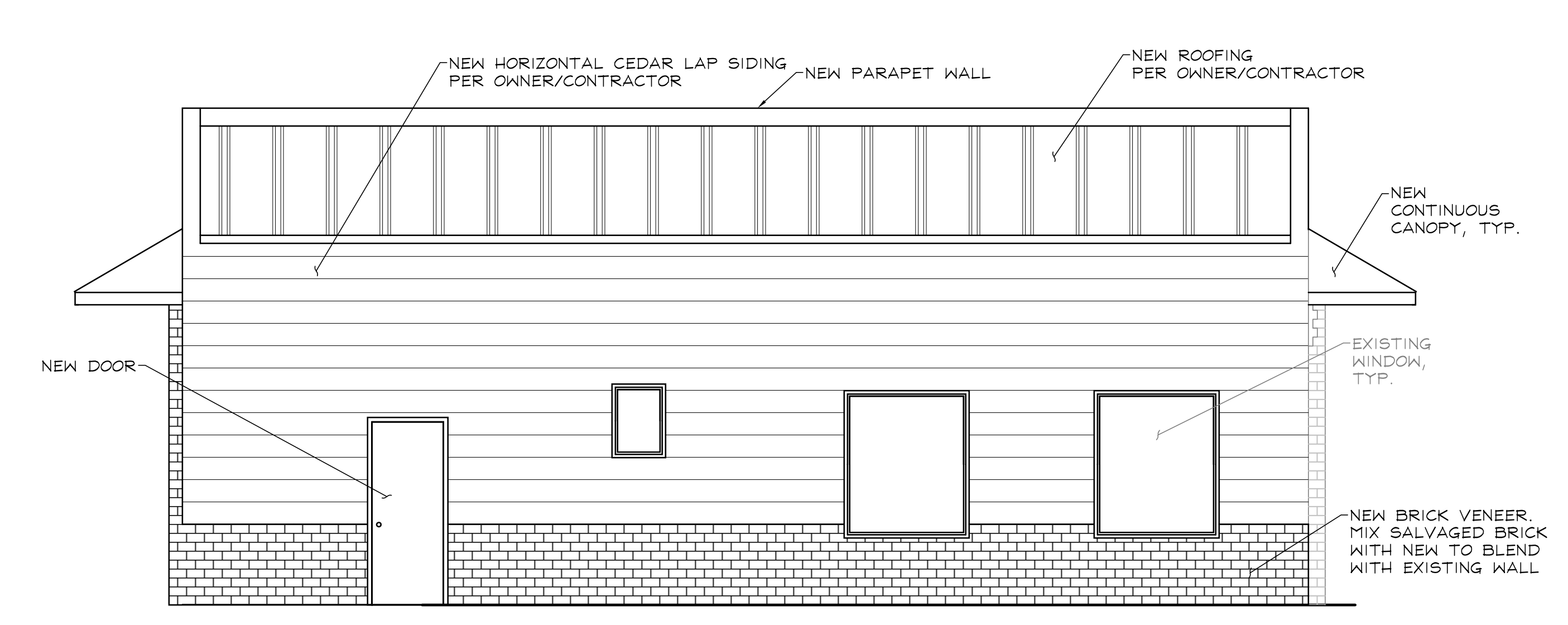
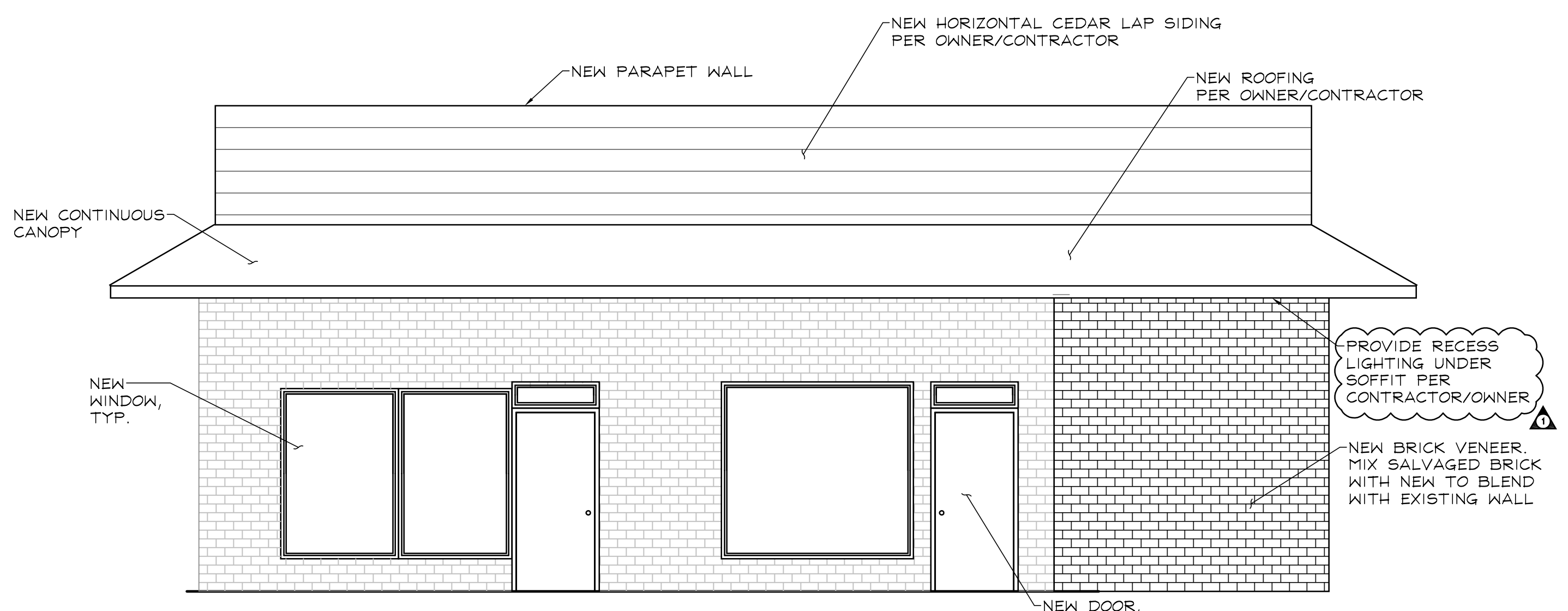
EXISTING NORTH FACE ELEVATION
1/4" = 1'-0"



EXISTING WEST FACE ELEVATION
1/4" = 1'-0"

PROPOSED NEW SOUTH FACE ELEVATION
1/4" = 1'-0"

PROPOSED NEW NORTH FACE ELEVATION
1/4" = 1'-0"



PROPOSED NEW EAST FACE ELEVATION
1/4" = 1'-0"

PROPOSED NEW WEST FACE ELEVATION
1/4" = 1'-0"

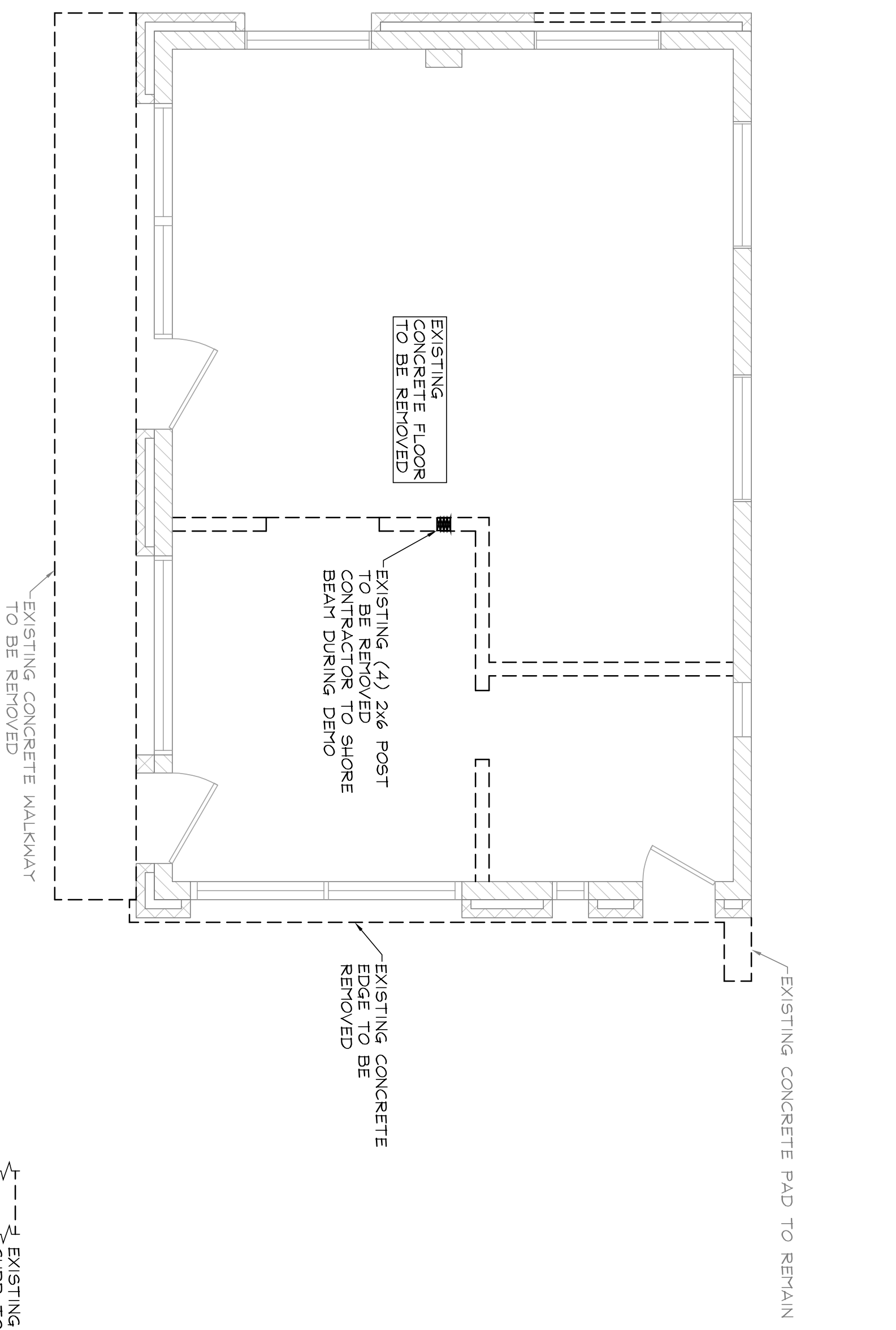
CHECKED BY: CBS
DATE: 05-26-20
JOB NO. 190418
DRAWN BY: JCP
REVISION: 05-20-21
REVISION COMMENTS

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503.399.1397
msceingineers.com

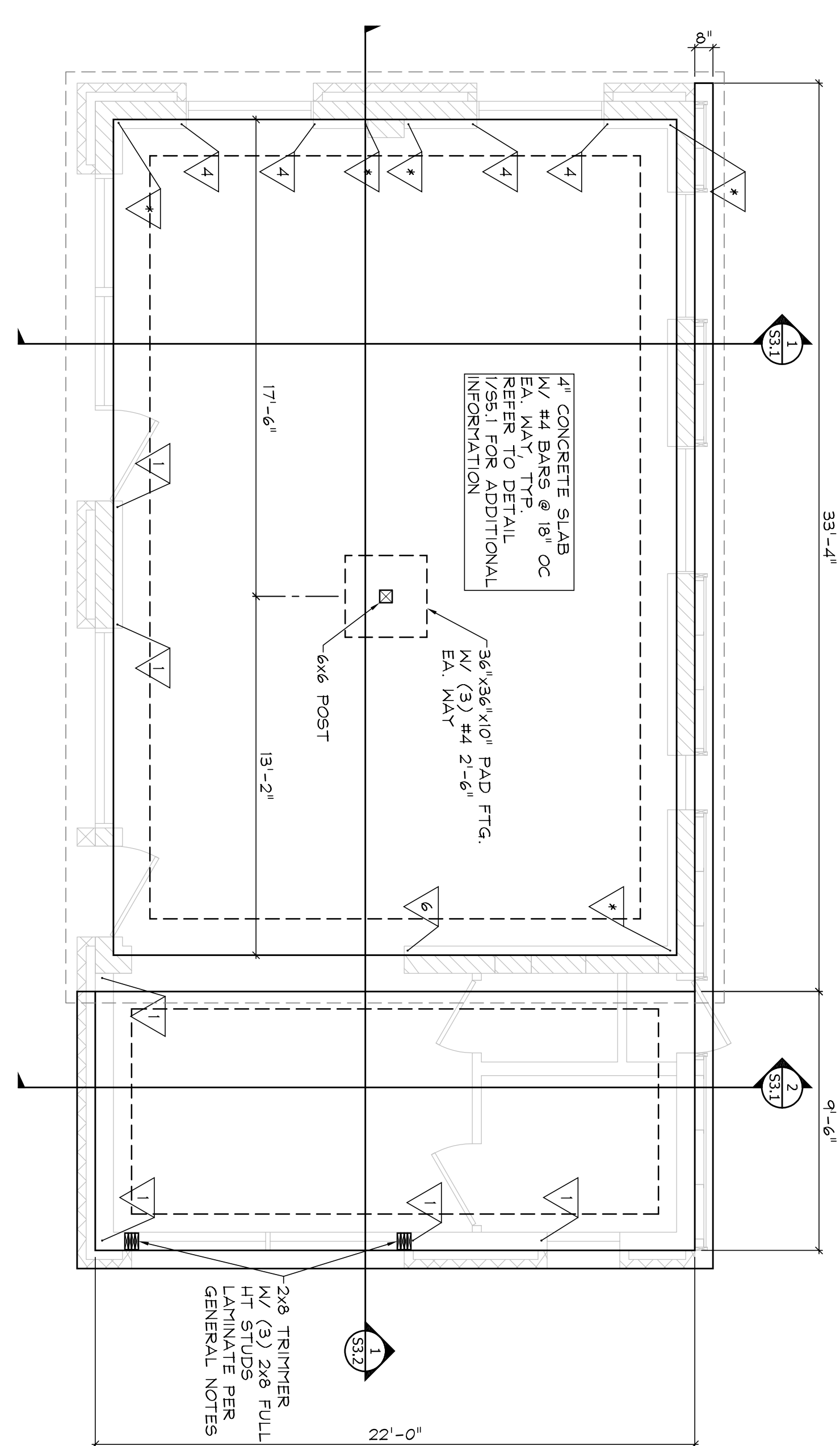
EXISTING DEMO/NEW ELEVATIONS
2219 NW HWY 101
2219 NW HWY 101
LINCOLN CITY, OREGON
FOR: EXCELLENCE INC.

REGISTERED PROFESSIONAL ENGINEER
63202
DIGITALLY SIGNED
December 18, 2021
CAMERON B. SWARTZ/MSCE
EXPIRES: JUNE 30, 2022

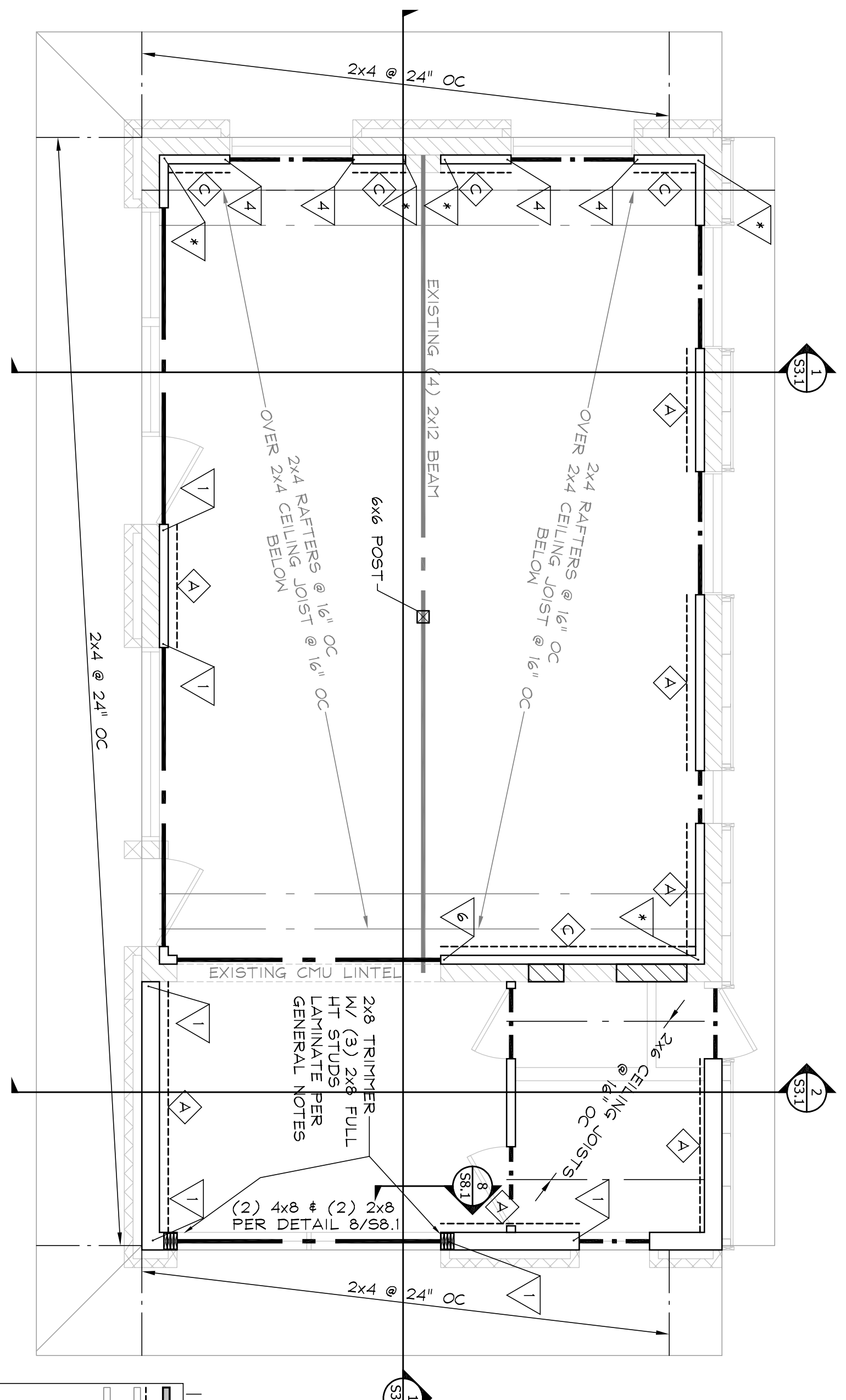
DWG. NO.
A4.1
5 of 13
190418



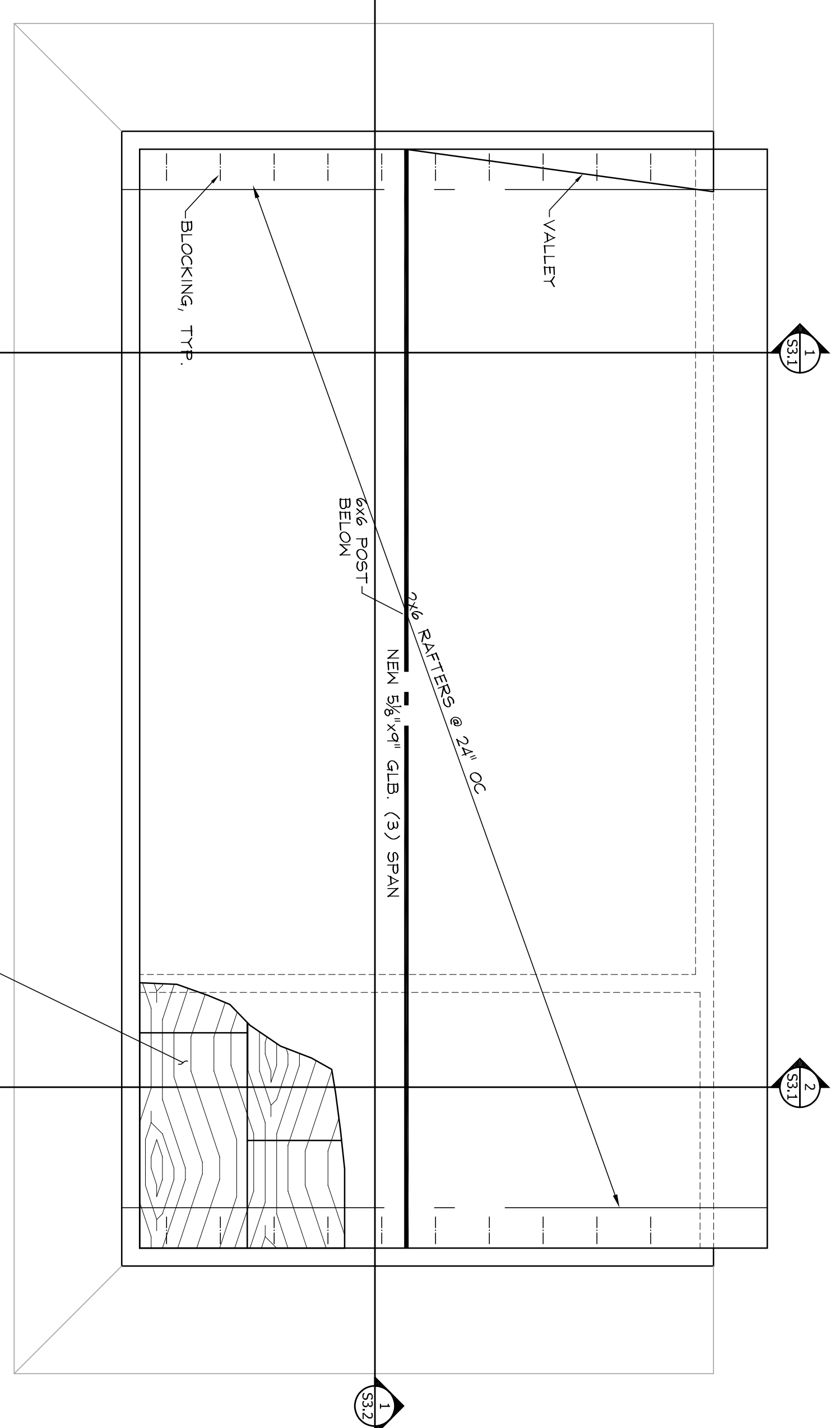
EXISTING/DEMO FOUNDATION PLAN
1/4" = 1'-0"



NEW PROPOSED FOUNDATION PLAN
1/4" = 1'-0"



EXISTING/NEW ROOF/CEILING FRAMING PLAN
1/4" = 1'-0"



NEW UPPER ROOF FRAMING PLAN
1/4" = 1'-0"

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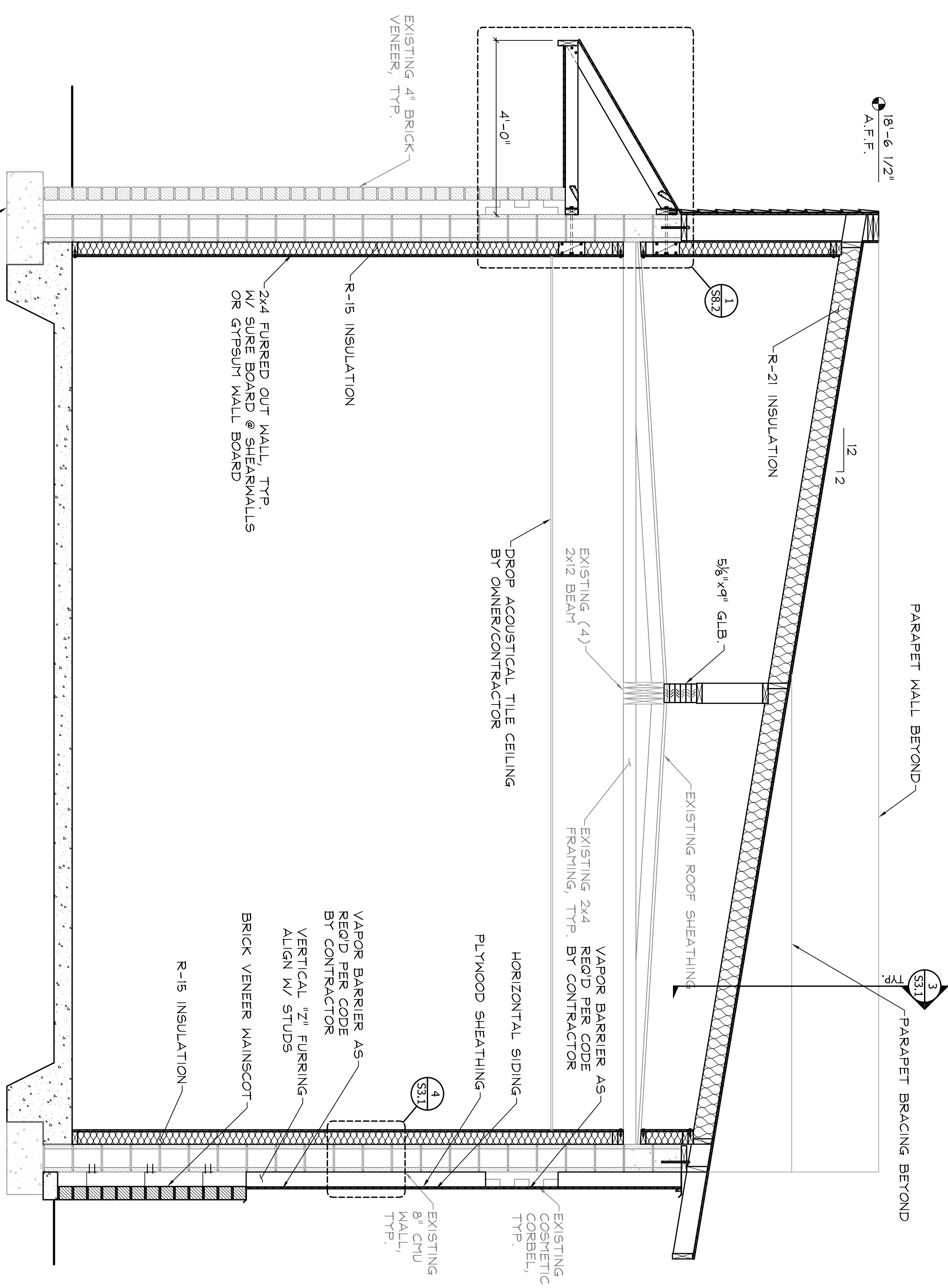
EXISTING/DEMO/NEW PLANS

2219 NW HWY 101
2219 NW HWY 101
LINCOLN CITY, OREGON
FOR: EXCELLENCE INC.

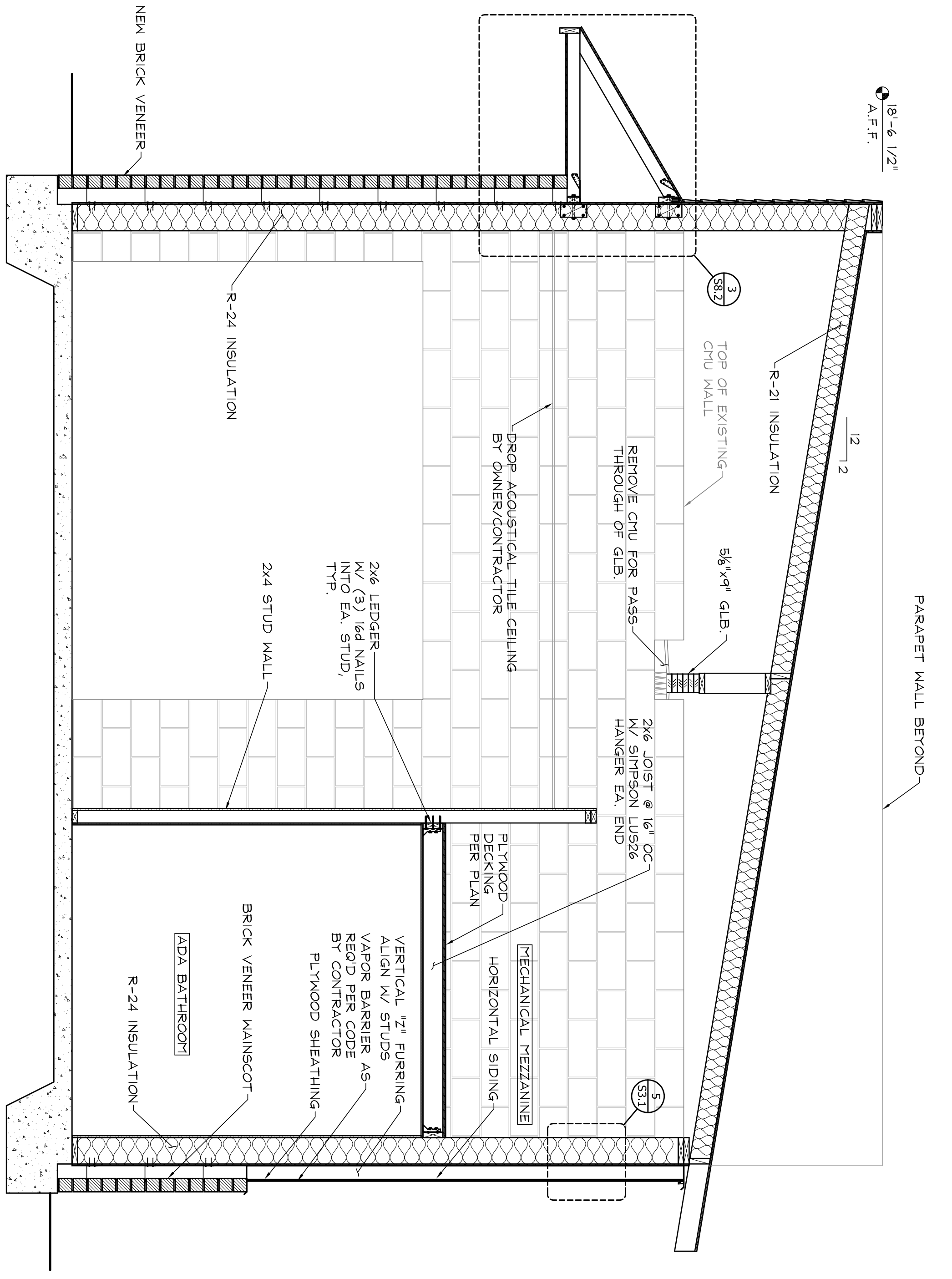
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DWG. NO. S1.1
7 of 13

NOTE: WATERPROOFING & FLASHING PER OWNER/CONTRACTOR

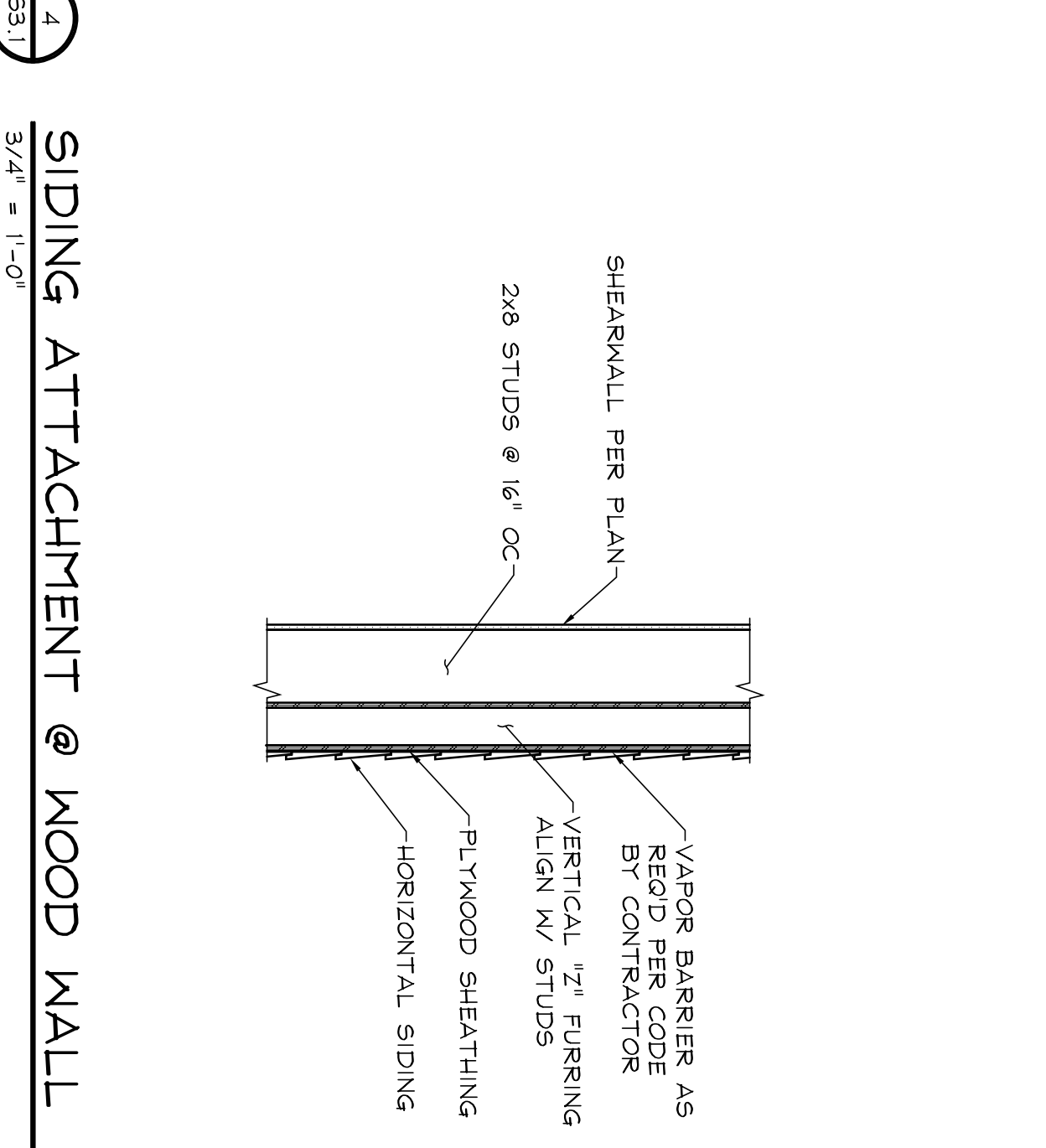
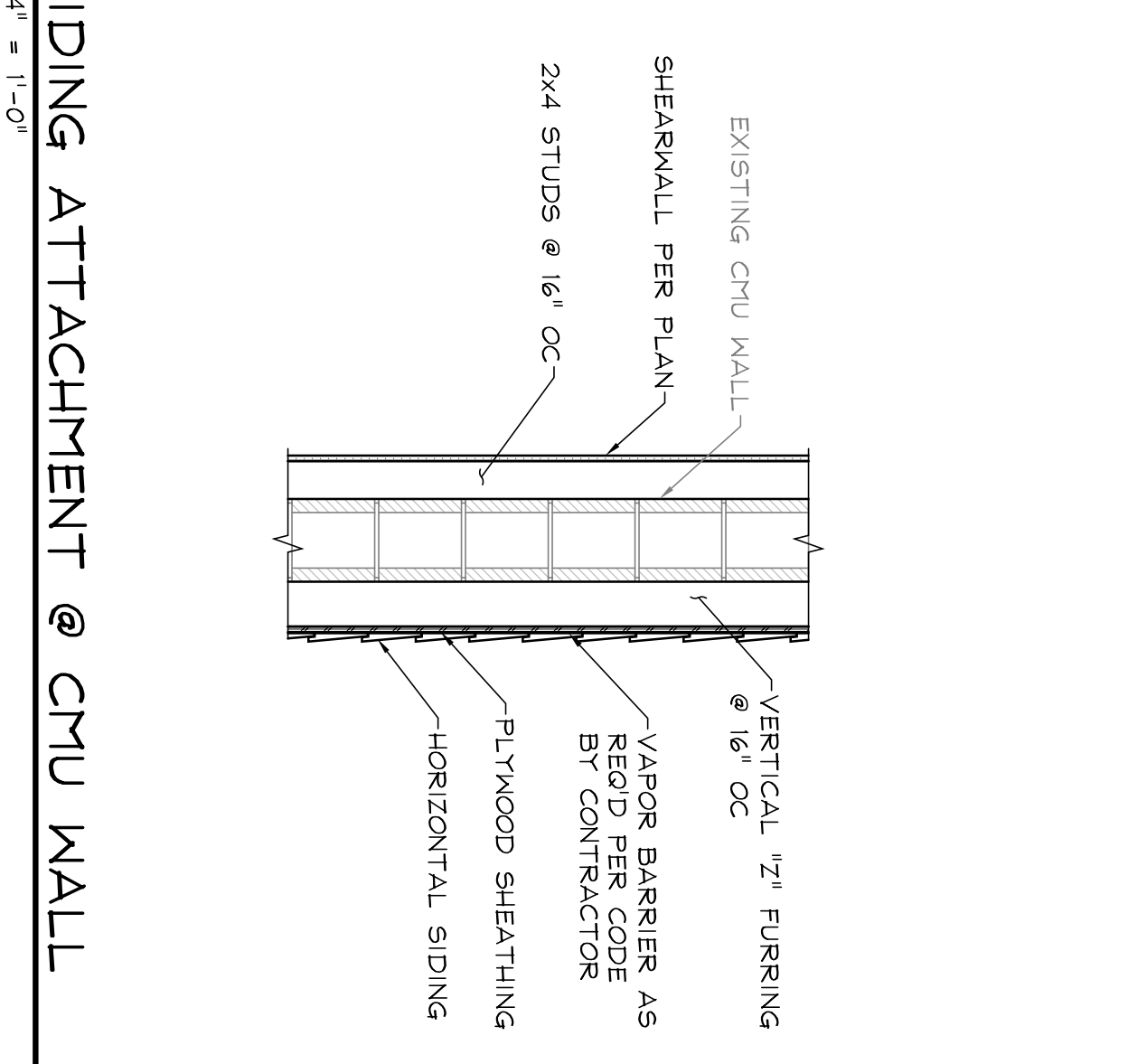
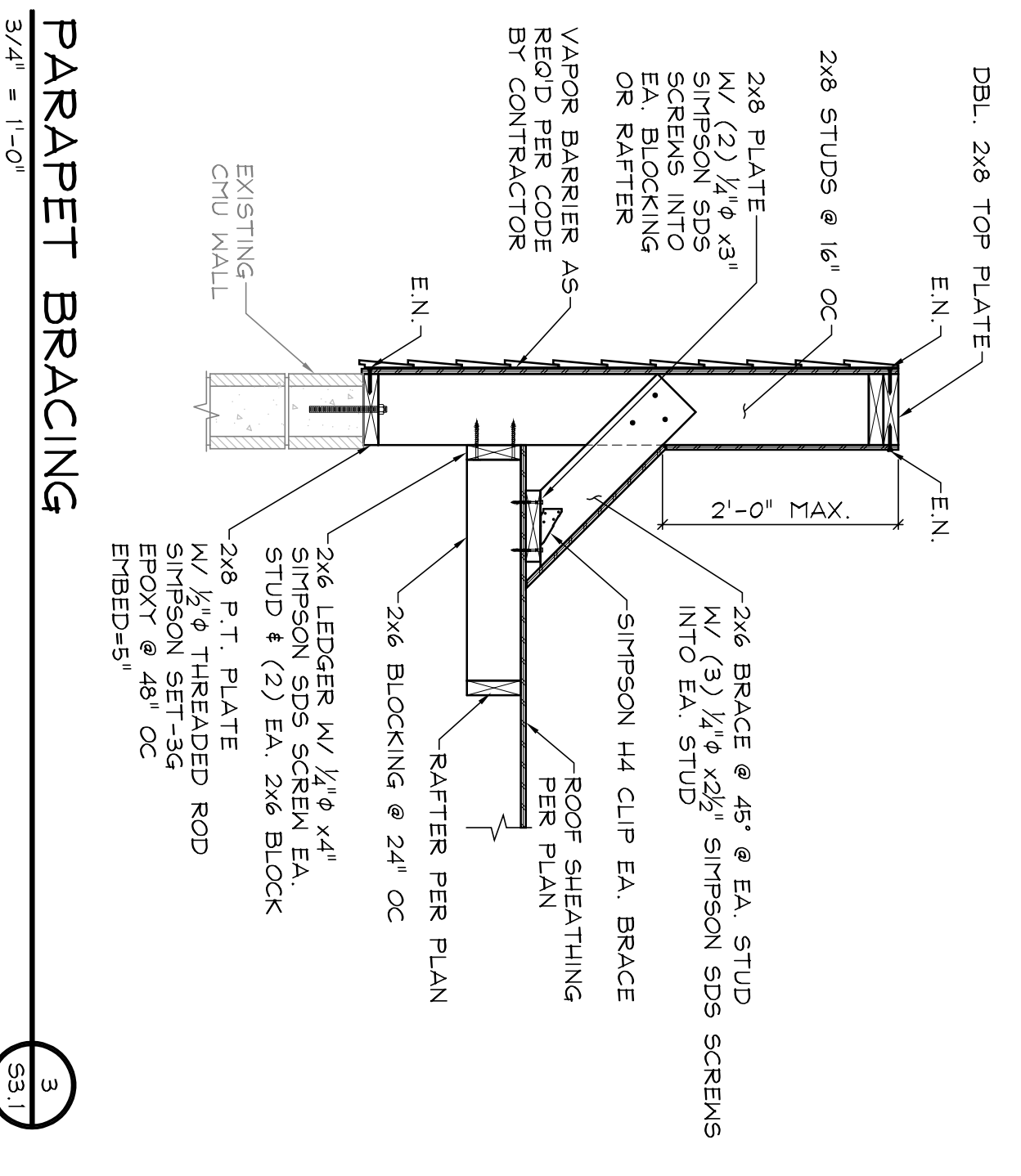


NOTE: WATERPROOFING & FLASHING PER OWNER/CONTRACTOR



BUILDING SECTION @ EXISTING END
1/2" = 1'-0"

BUILDING SECTION @ NEW ADDITION END
1/2" = 1'-0"



PARAPET BRACING
3/4" = 1'-0"

SIDING ATTACHMENT @ CMU WALL
3/4" = 1'-0"

SIDING ATTACHMENT @ WOOD WALL
3/4" = 1'-0"

BUILDING SECTIONS
2219 NW HWY 101
2219 NW HWY 101
LINCOLN CITY, OREGON
FOR: EXCELLENCE INC.

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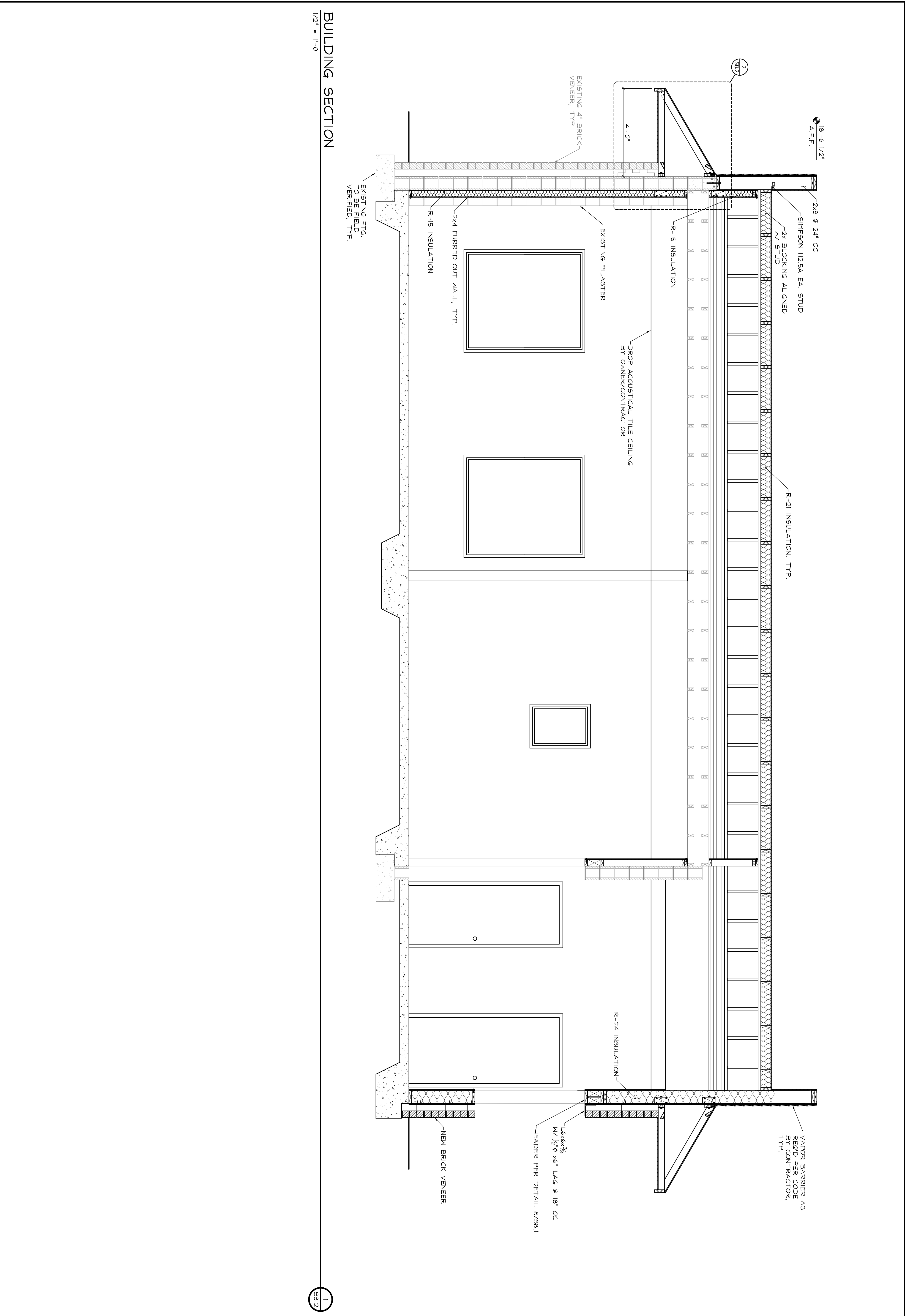
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S3.1
8 of 13



DWG. NO.
S3.2
9 of 13

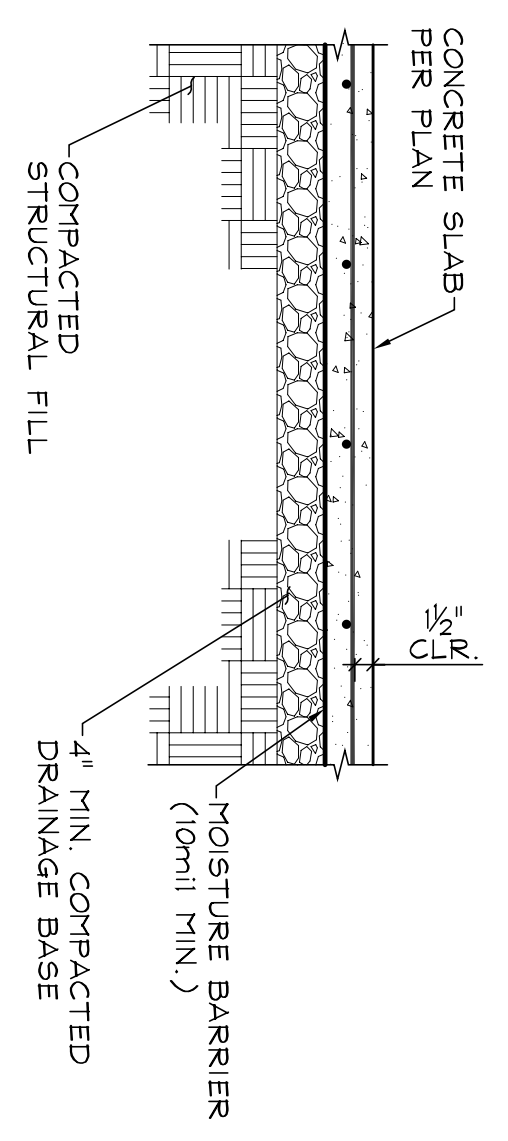
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BUILDING SECTION
2219 NW HWY 101
2219 NW HWY 101
LINCOLN CITY, OREGON
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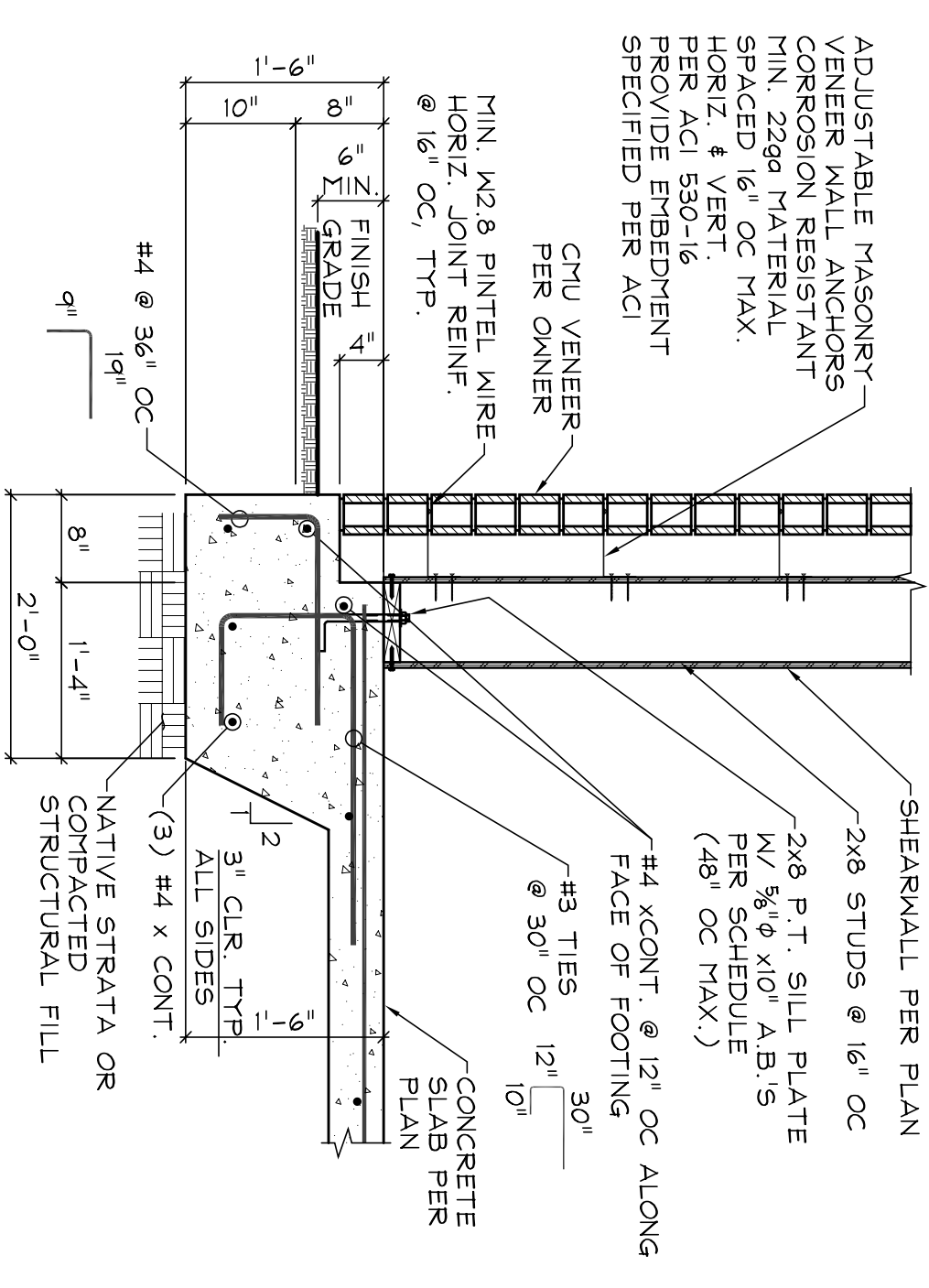
NOTE: FIBERESH MAY BE SUBSTITUTED FOR TEMPERATURE REINF. AT CONTRACTORS DISCRETION. CONTRACTOR TO SUBMIT TYPE AND QUANTITY OF FIBERESH TO EOR FOR REVIEW AND APPROVAL.



TYP. FLOOR SLAB CONSTRUCTION
3/4" = 1'-0"
S5.1

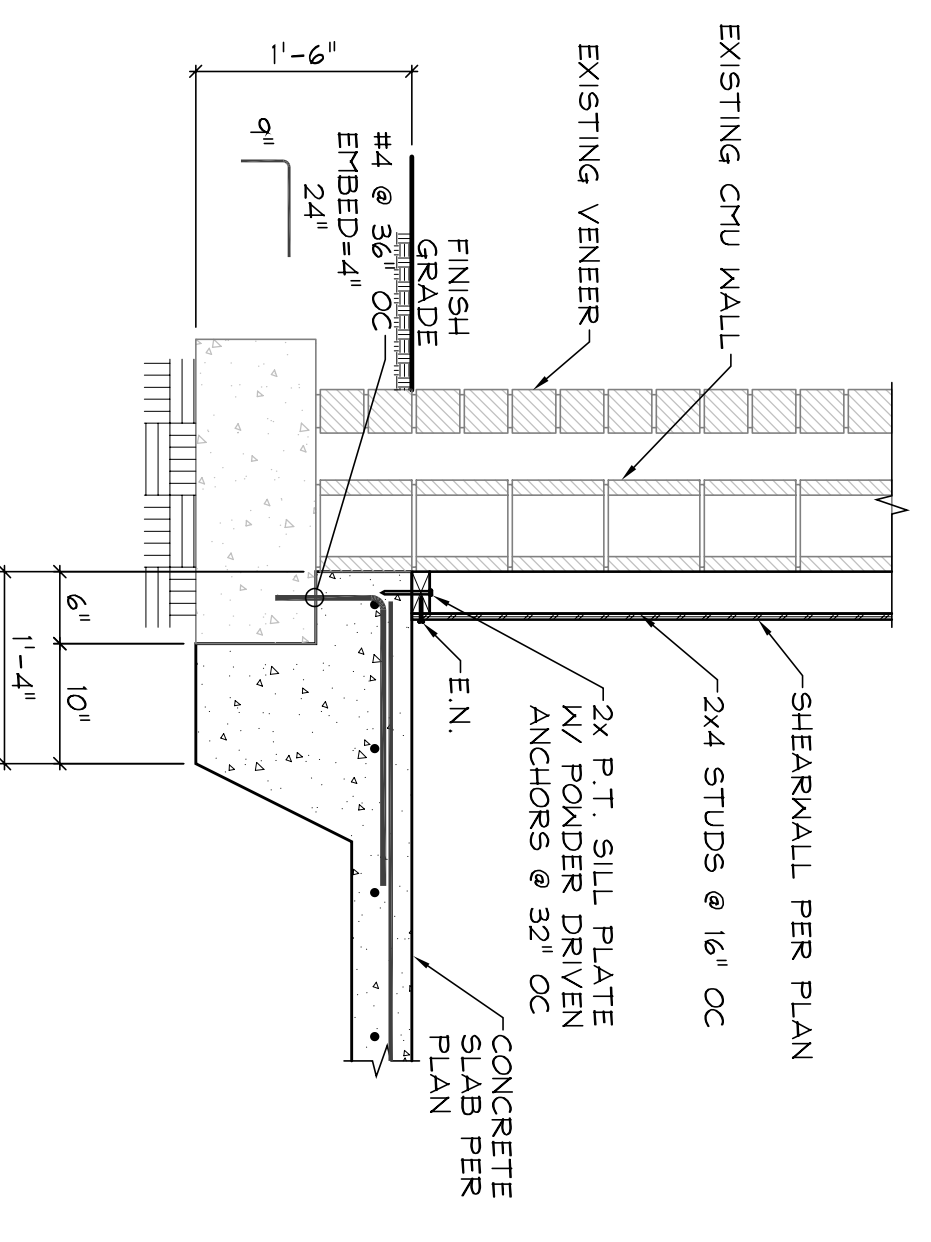
NOTE: FLASHING & WATER PROOFING PER OWNER/CONTRACTOR

VAPOR BARRIER AS REQ'D PER CODE BY CONTRACTOR

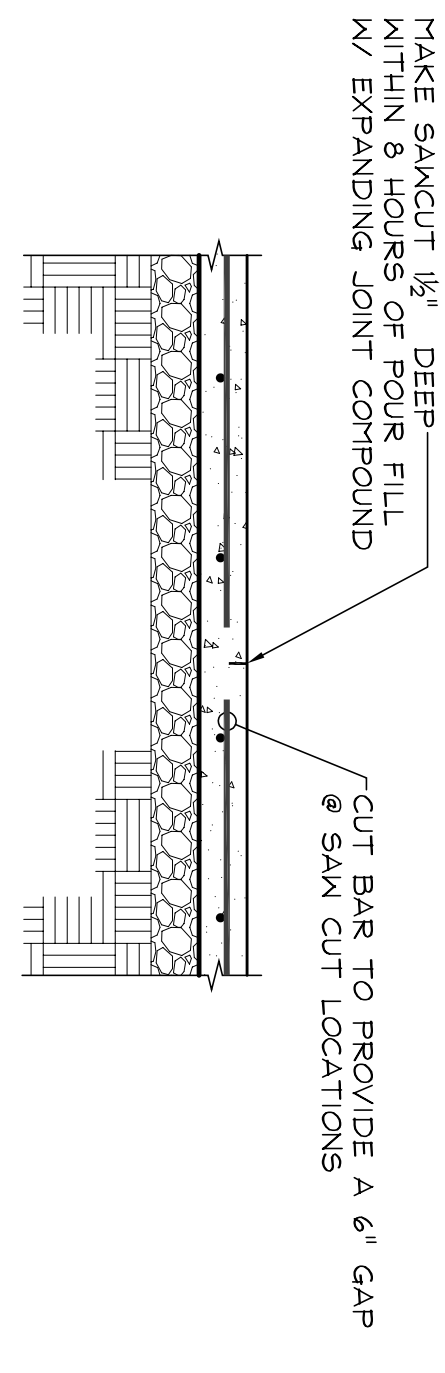


TYPICAL NEW PERIMETER FOOTING
3/4" = 1'-0"
S5.1

VAPOR BARRIER AS REQ'D PER CODE BY CONTRACTOR



INTERIOR SLAB @ EXISTING FTG.
3/4" = 1'-0"
S5.1



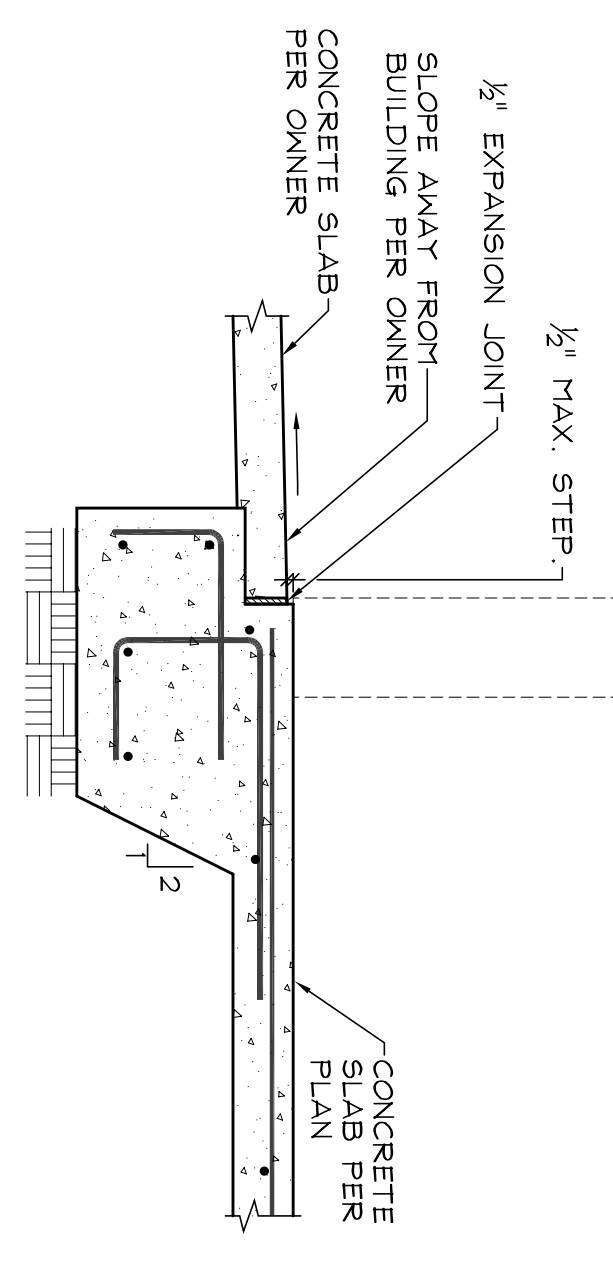
TYP. FLOOR SLAB SAW CUT
3/4" = 1'-0"
S5.1

NOTE: FLASHING & WATER PROOFING PER OWNER/CONTRACTOR

VAPOR BARRIER AS REQ'D PER CODE BY CONTRACTOR

NOTE: REFER TO DETAIL 1/55.1 FOR ADDITIONAL INFORMATION

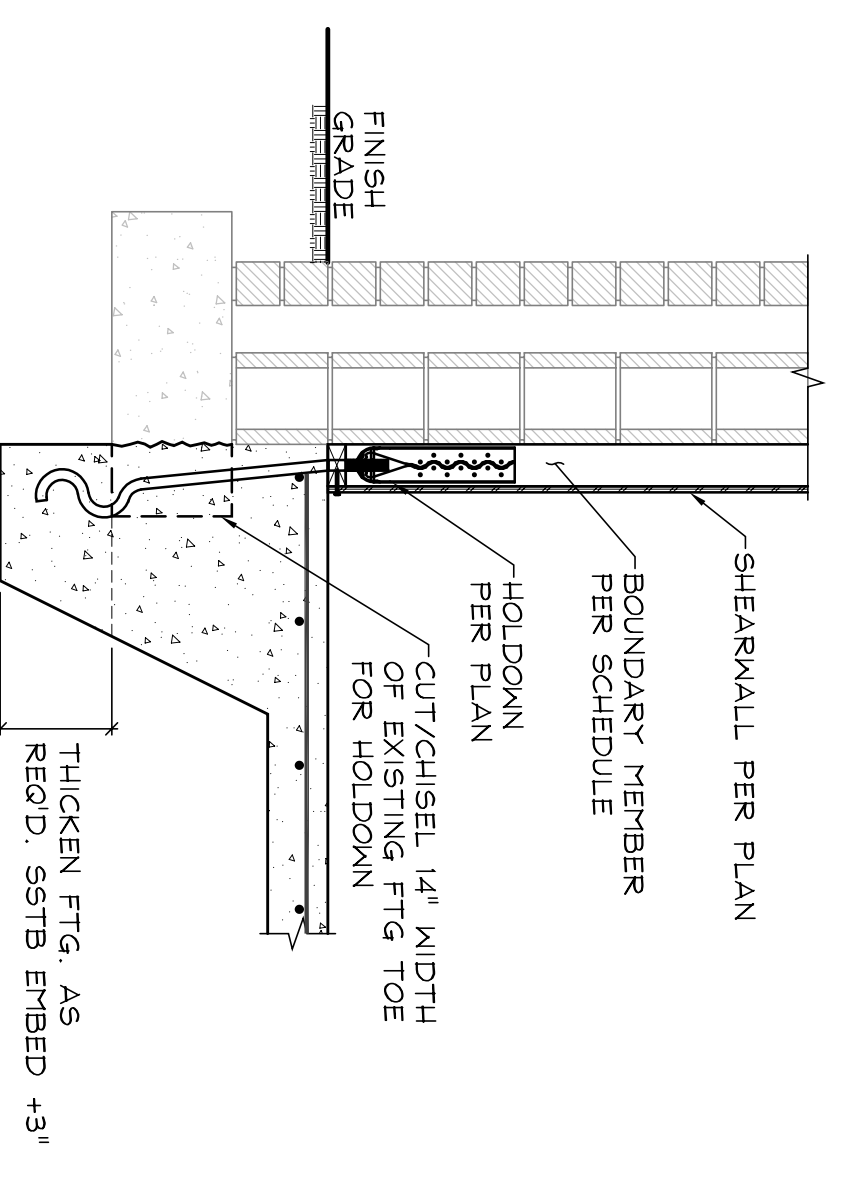
PROVIDE SAWCUT JOINTS @ 300' SQ. FT. MAXIMUM COORDINATE W/ FLOOR FINISHES



TYPICAL SLAB EDGE @ OPENING
3/4" = 1'-0"
S5.1

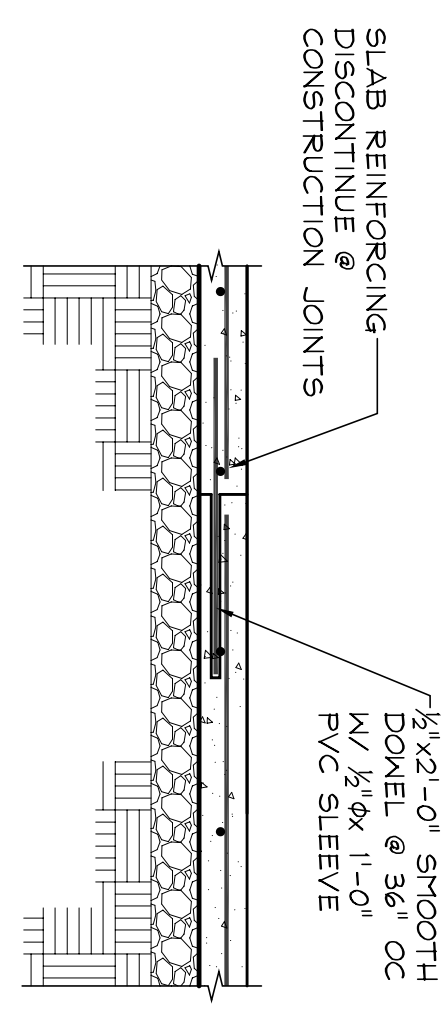
NOTE: REFER TO DETAIL 5/55.1 FOR ADDITIONAL INFORMATION

VAPOR BARRIER AS REQ'D PER CODE BY CONTRACTOR



INTERIOR HOLDDOWN @ EXISTING FTG.
3/4" = 1'-0"
S5.1

NOTE: REFER TO DETAIL 9/55.1 FOR ADDITIONAL INFORMATION

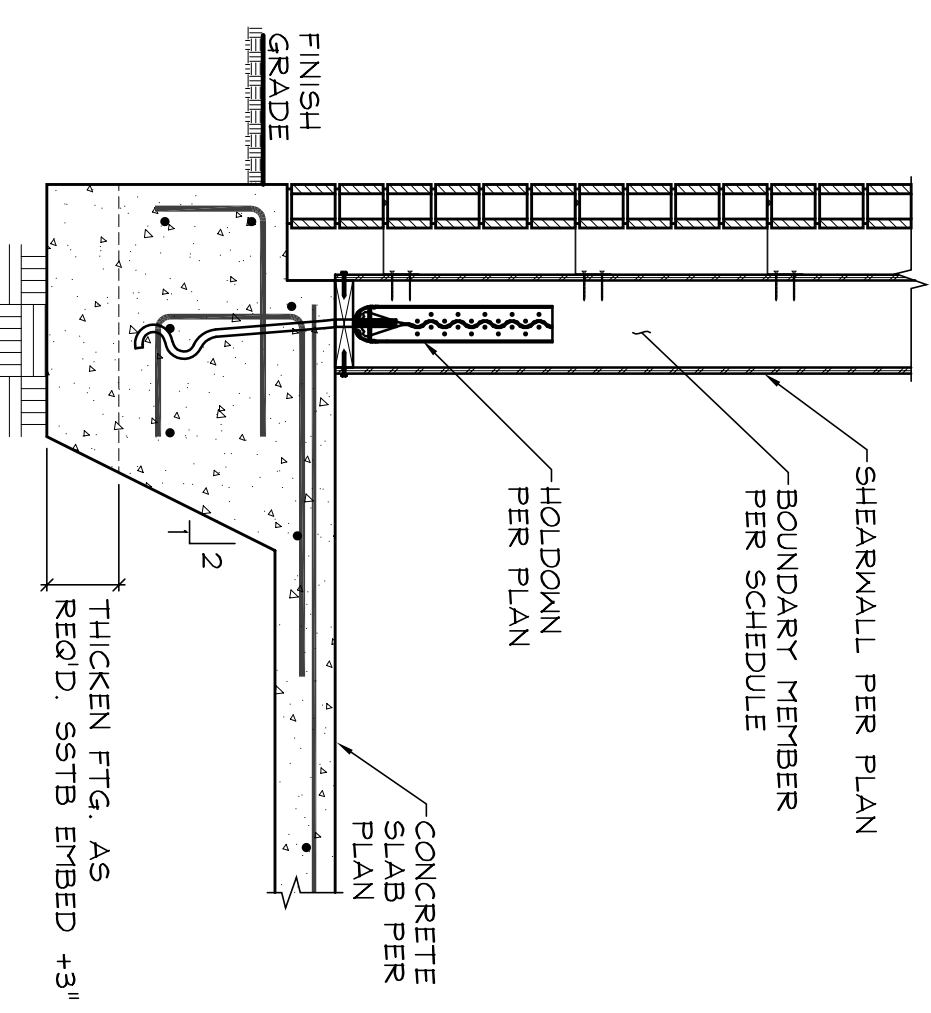


TYP. FLOOR SLAB CONSTRUCTION JOINT
3/4" = 1'-0"
S5.1

NOTE: FLASHING & WATER PROOFING PER OWNER/CONTRACTOR

VAPOR BARRIER AS REQ'D PER CODE BY CONTRACTOR

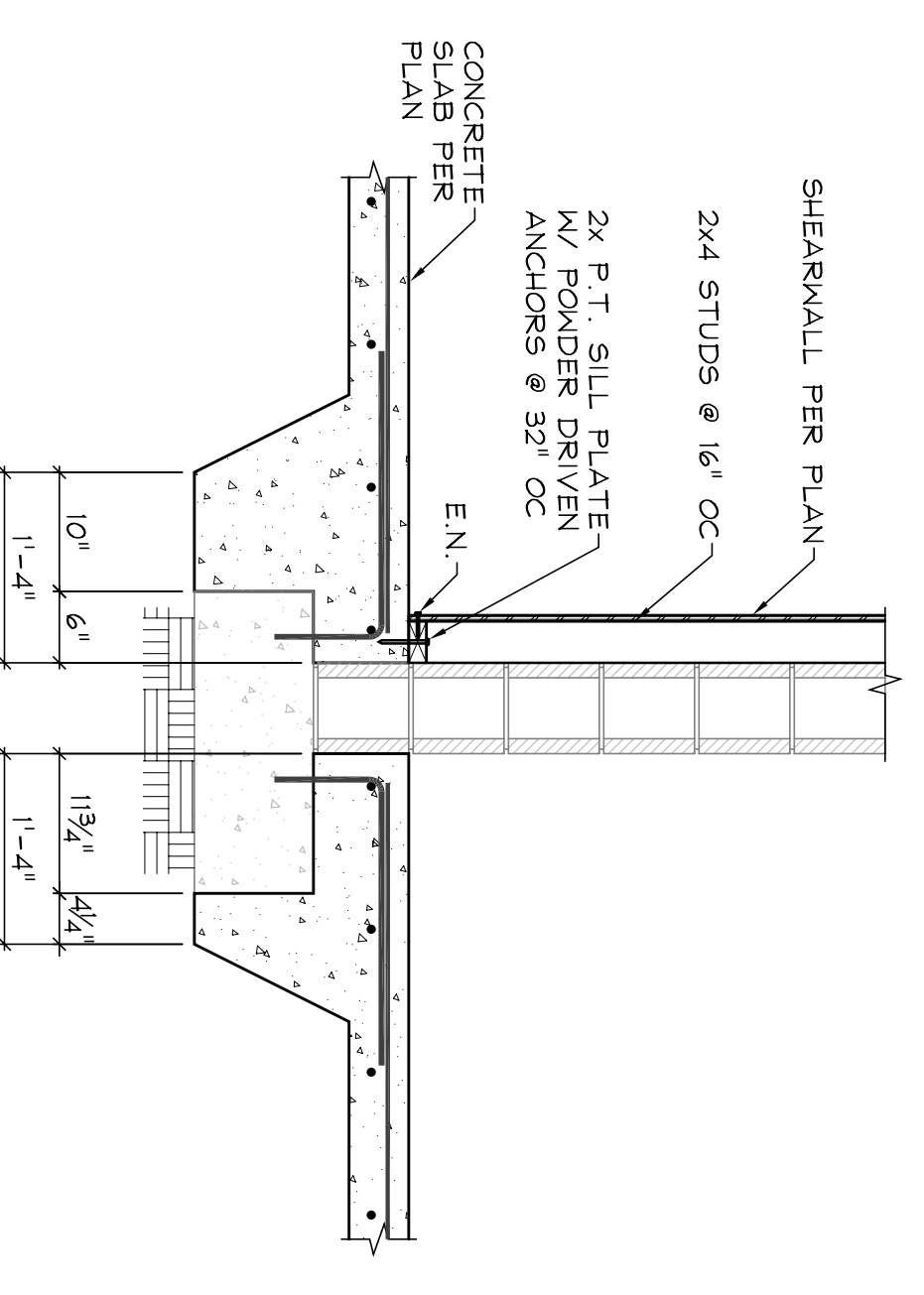
NOTE: REFER TO DETAIL 1/55.1 FOR ADDITIONAL INFORMATION



TYPICAL SLAB EDGE @ HOLDDOWN
3/4" = 1'-0"
S5.1

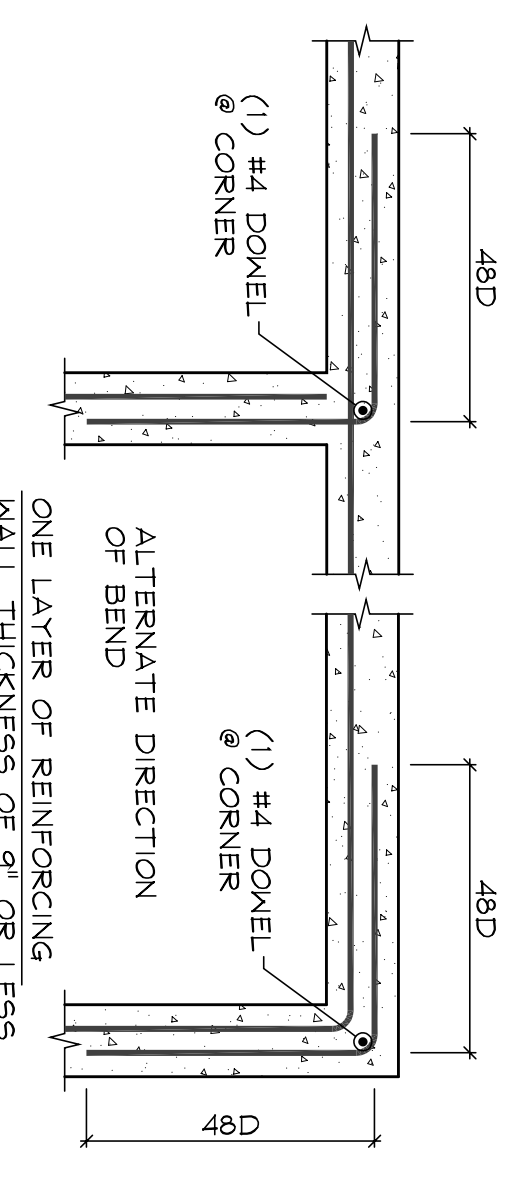
NOTE: REFER TO DETAIL 5/55.1 FOR ADDITIONAL INFORMATION

VAPOR BARRIER AS REQ'D PER CODE BY CONTRACTOR



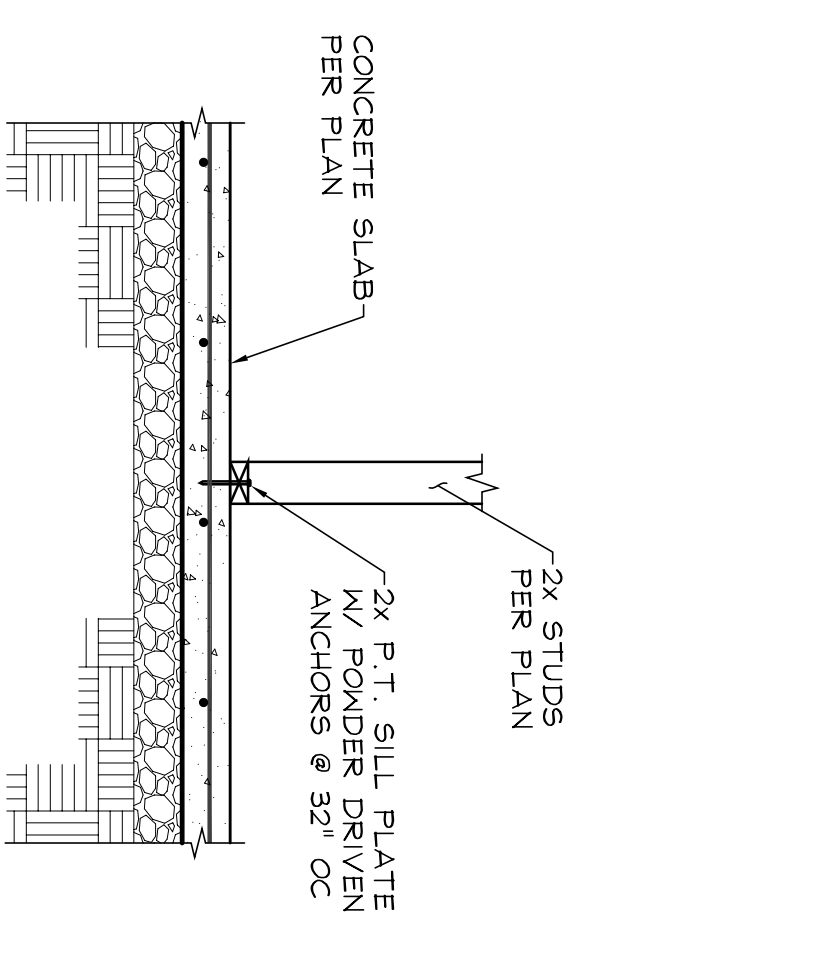
INTERIOR FTG CONNECTION @ EXTERIOR FTG
3/4" = 1'-0"
S5.1

NOTE: REFER TO DETAIL 9/55.1 FOR ADDITIONAL INFORMATION



TYPICAL CONCRETE CORNER REBAR DETAIL
3/4" = 1'-0"
S5.1

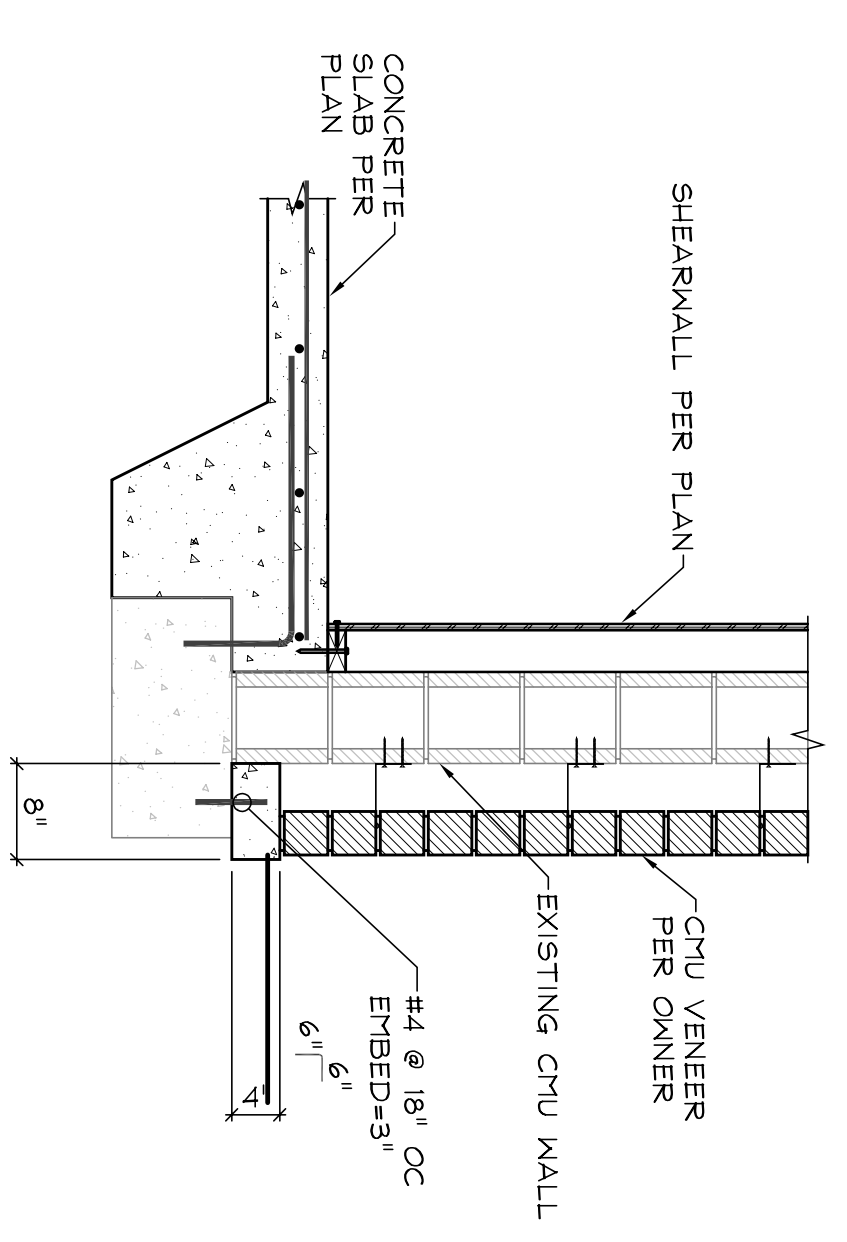
NOTE: CORNER BARS TO MATCH THE SIZE OF HORIZONTAL REINFORCEMENT (D) = DIAMETER OF BAR



TYPICAL NON-BEARING STUD WALL
3/4" = 1'-0"
S5.1

VAPOR BARRIER AS REQ'D PER CODE BY CONTRACTOR

NOTE: REFER TO DETAIL 5/55.1 & 9/55.1 FOR ADDITIONAL INFORMATION



INTERIOR FTG CONNECTION @ EXTERIOR FTG
3/4" = 1'-0"
S5.1

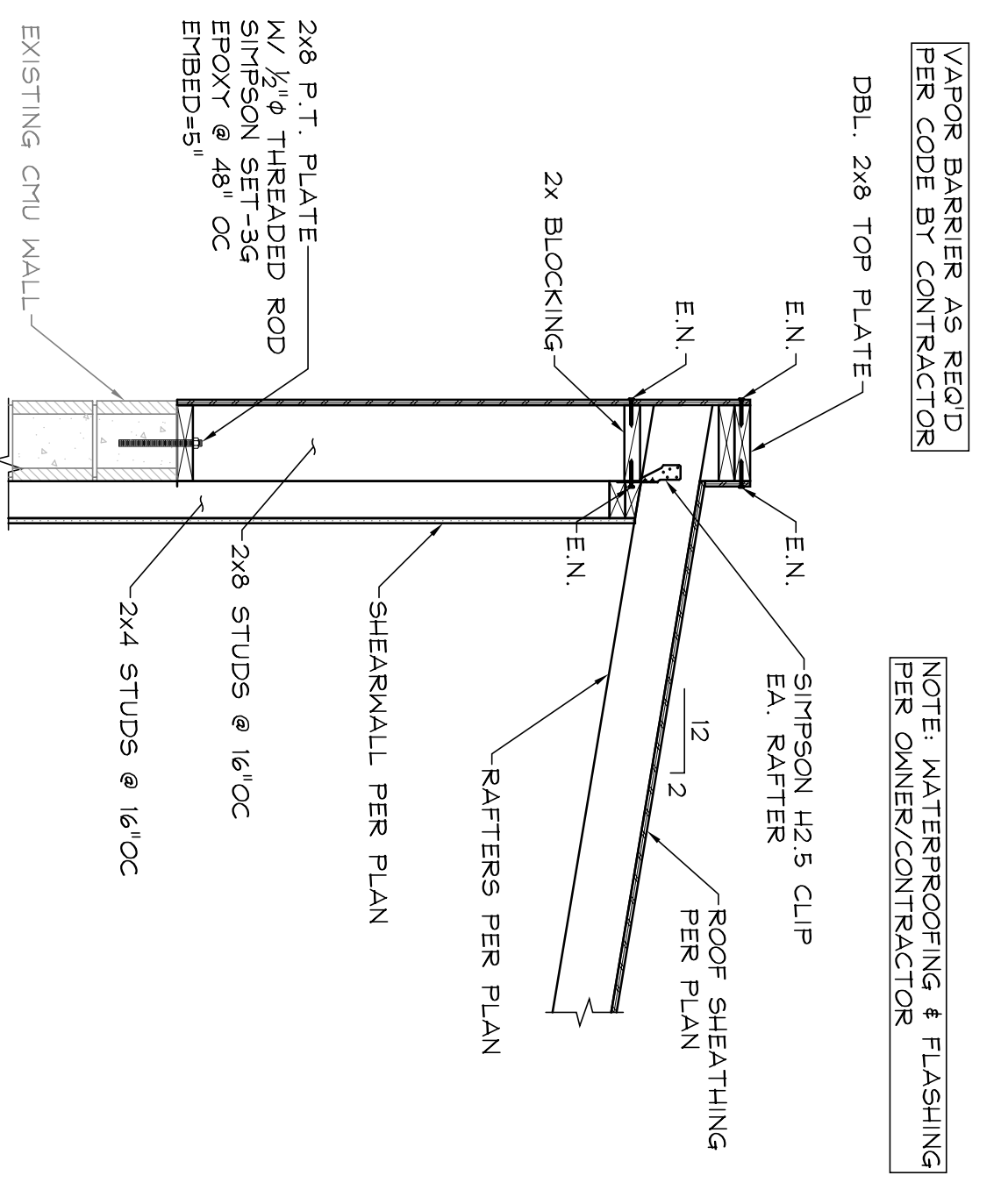
FOUNDATION DETAILS
2219 NW HWY 101
2219 NW HWY 101
LINCOLN CITY, OREGON
FOR: EXCELLENCE INC.

MSC ENGINEERS
CONSULTING STRUCTURAL ENGINEERS
3470 Pipeband Place NE
Suite 120
Salem, OR 97301
503.399.1399
mscengineersinc.com
SINCE 1925

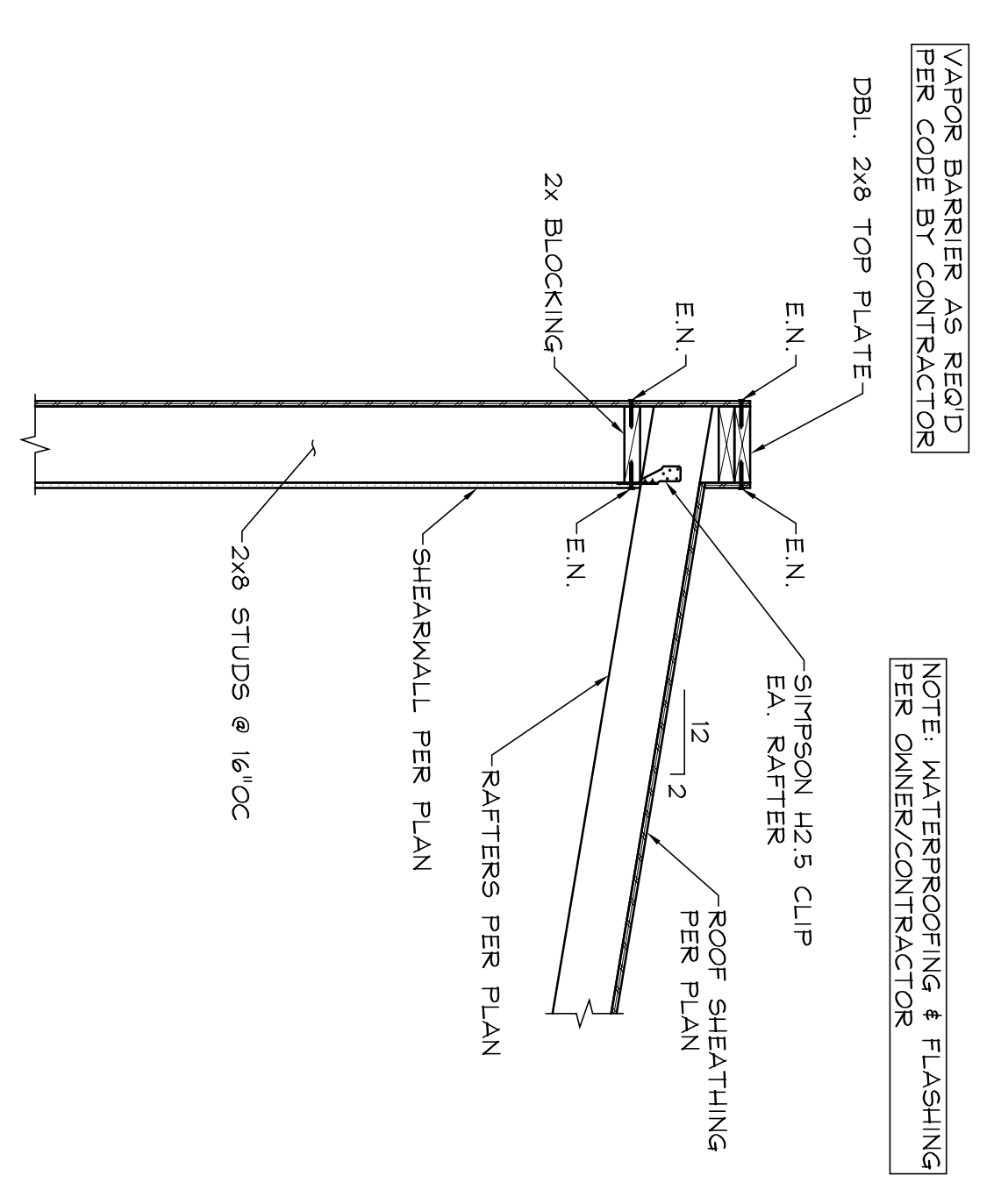
JOB NO. 190418	CHECKED BY: CBS
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REVISION:	

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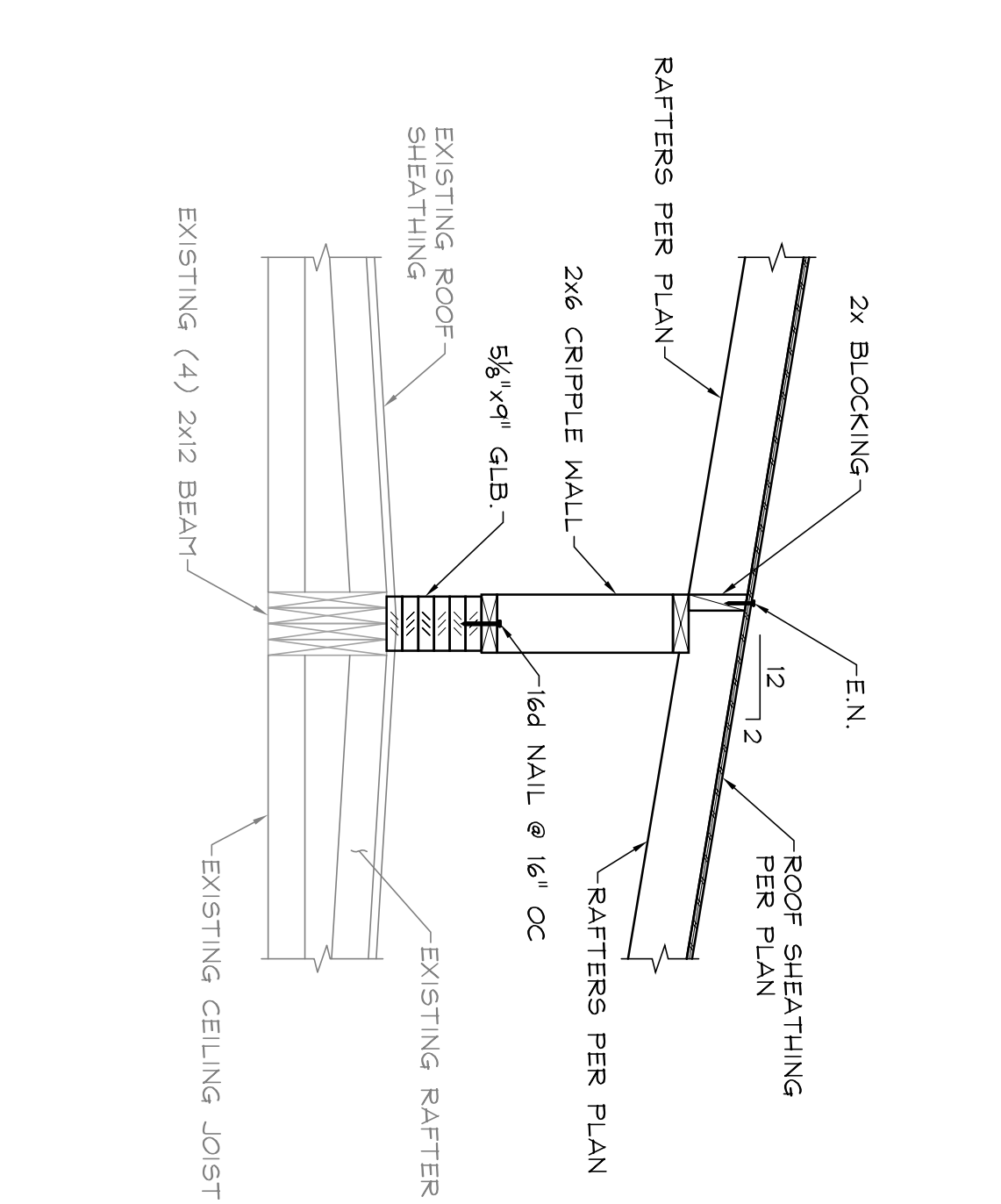
DWG. NO. S5.1
10 of 13
190418



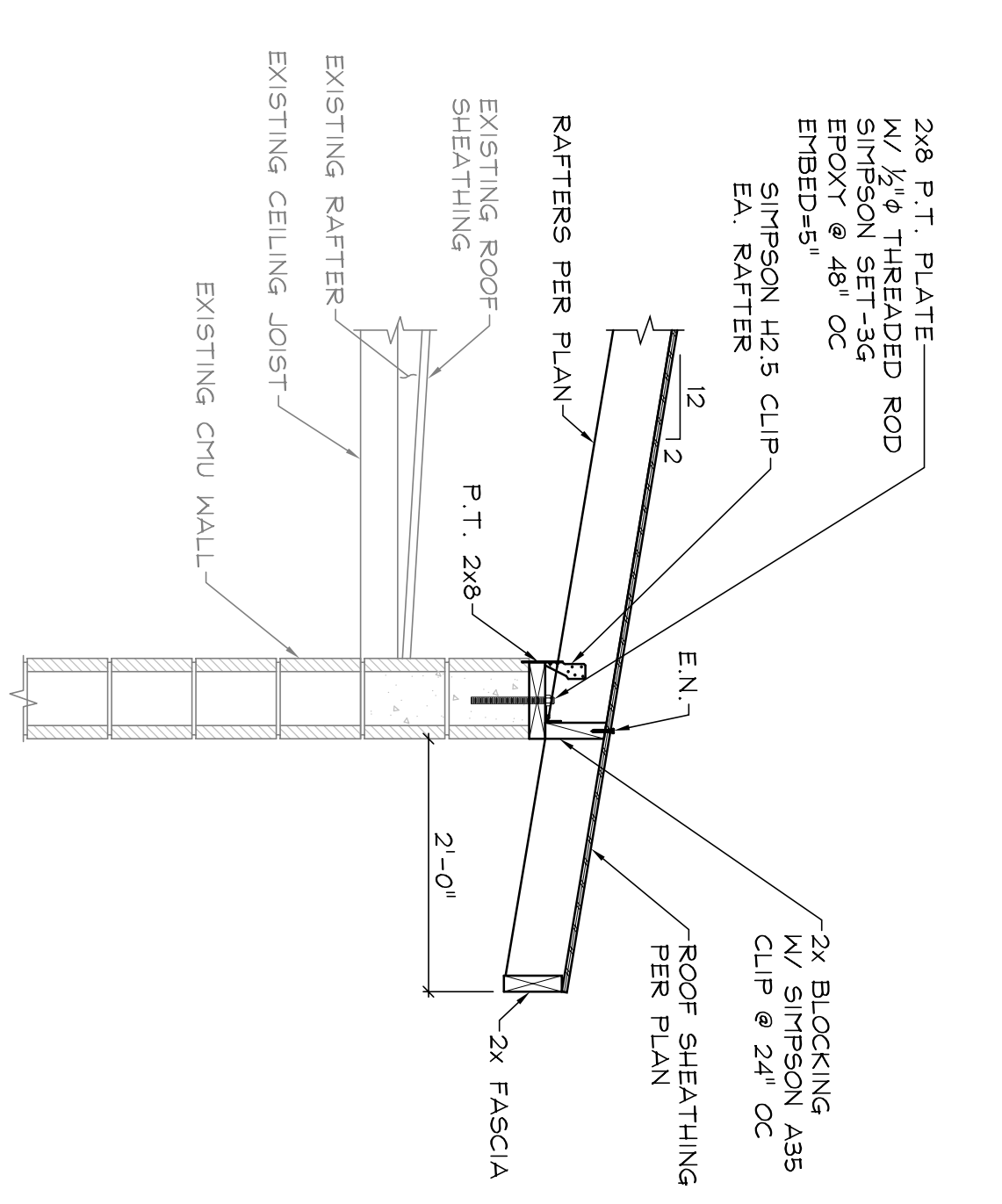
RAFTER CONNECTION @ NEW PARAPET
 3/4" = 1'-0"
 58.1



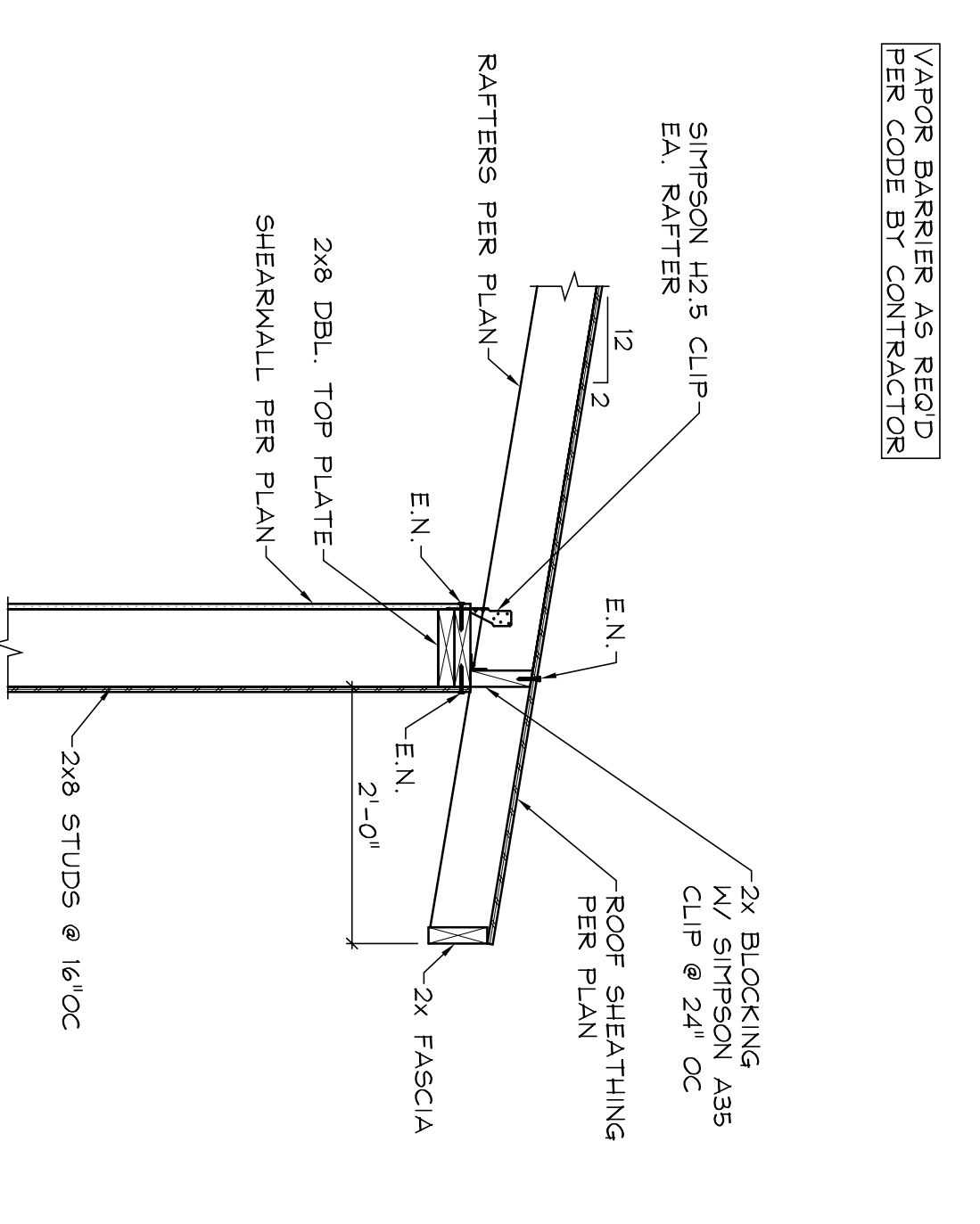
RAFTER CONNECTION NEW WALL
 3/4" = 1'-0"
 58.1



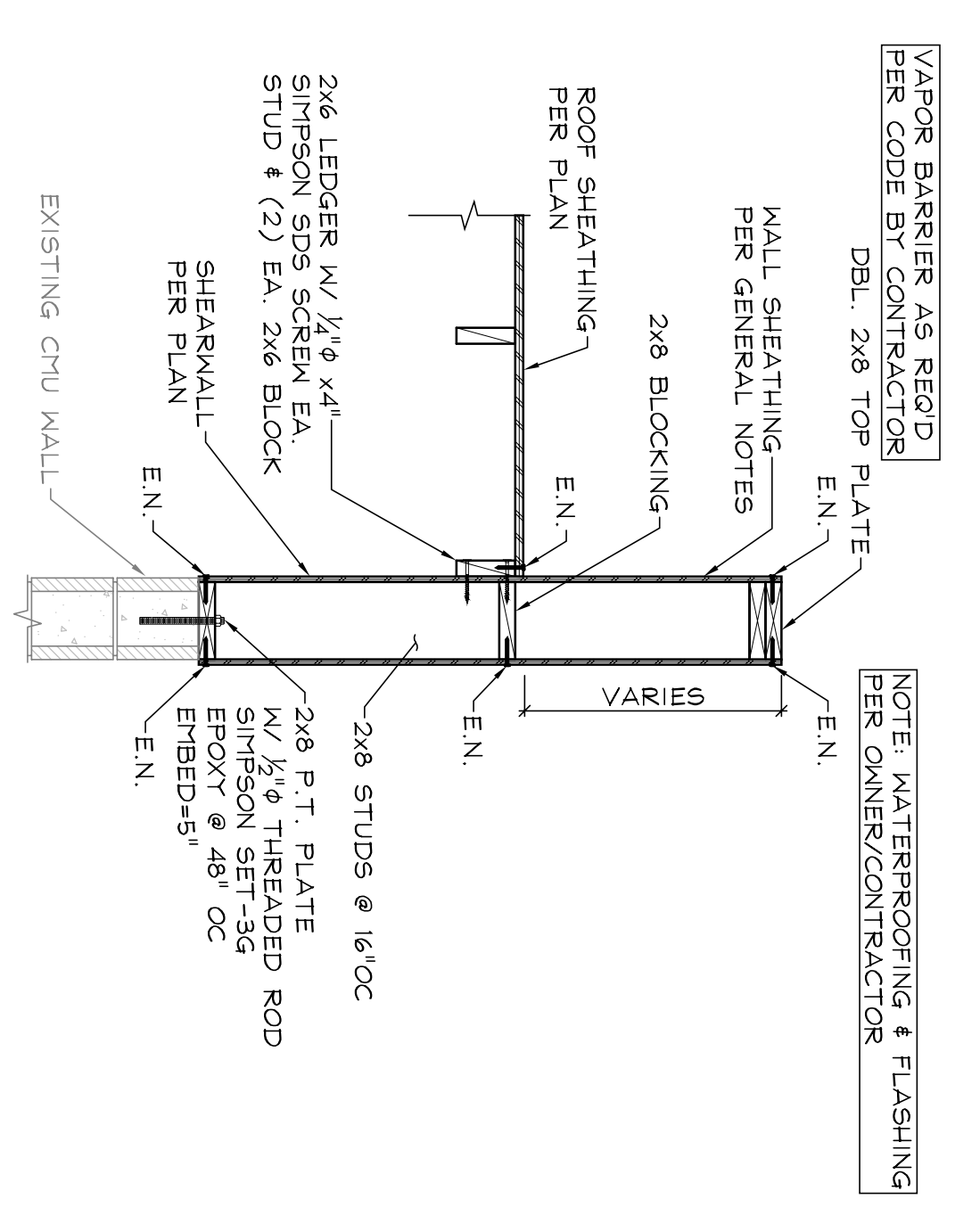
RAFTER CONNECTION @ CRIPPLE WALL
 3/4" = 1'-0"
 58.1



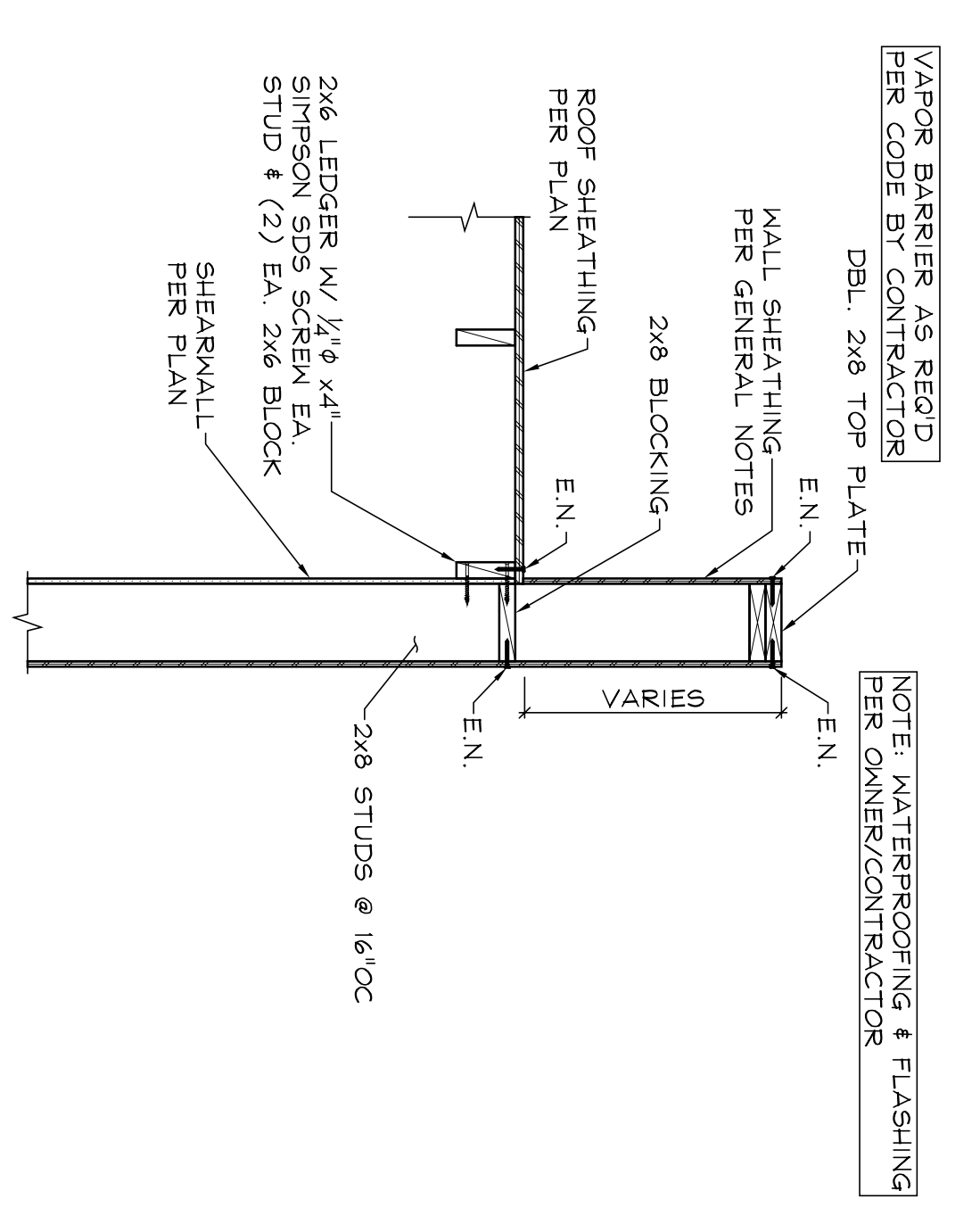
RAFTER CONNECTION @ CMU
 3/4" = 1'-0"
 58.1



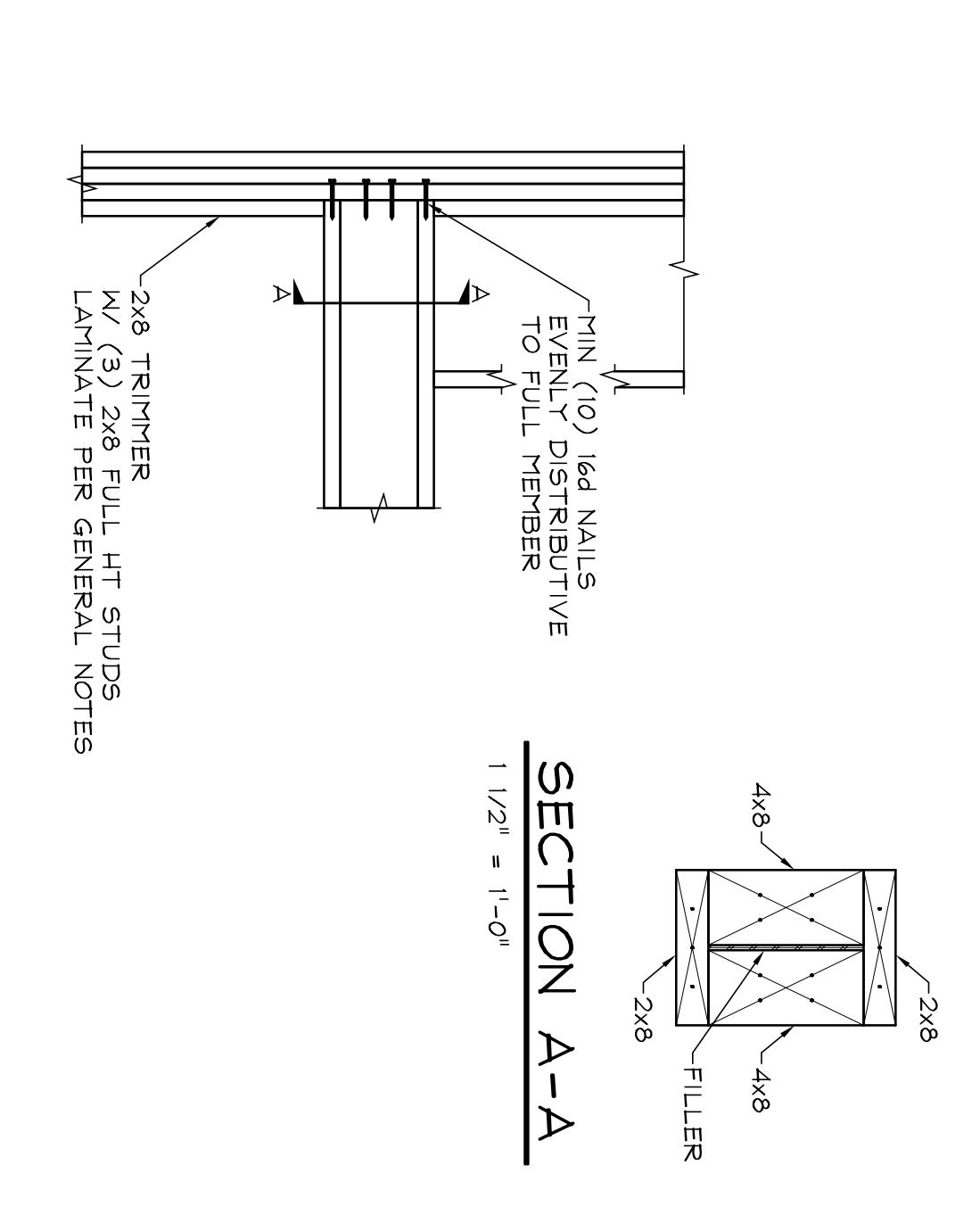
RAFTER CONNECTION @ WALL
 3/4" = 1'-0"
 58.1



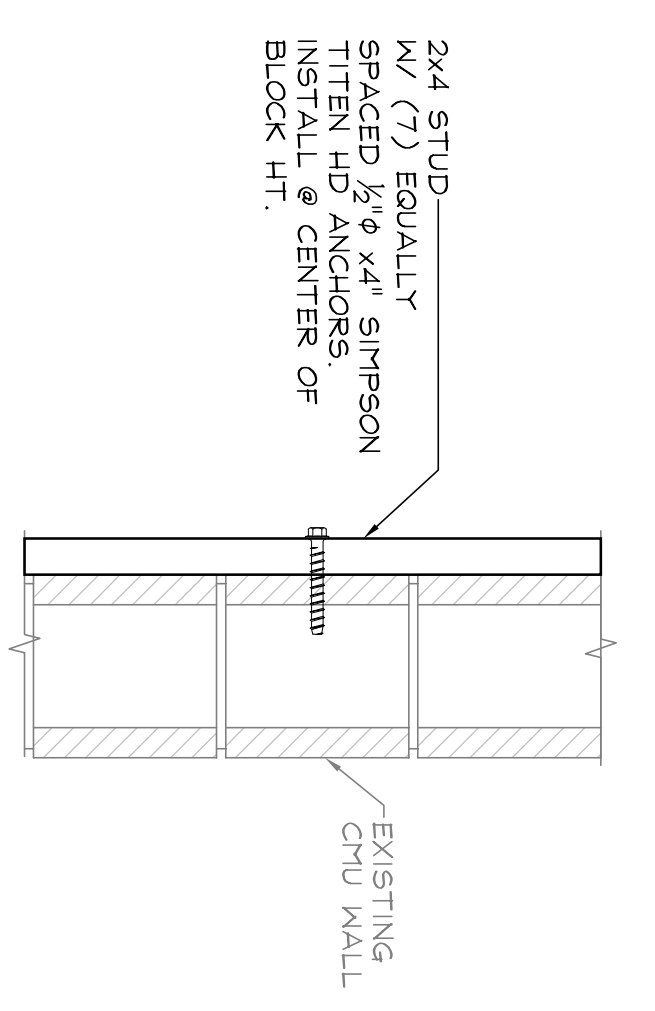
SHEAR TRANSFER @ NEW PARAPET WALL
 3/4" = 1'-0"
 58.1



SHEAR TRANSFER @ NEW WALL
 3/4" = 1'-0"
 58.1



BEAM CONNECTION @ JAMB
 3/4" = 1'-0"
 58.1



HOLDOWN ANCHOR @ CMU WALL
 1/2" = 1'-0"
 58.1

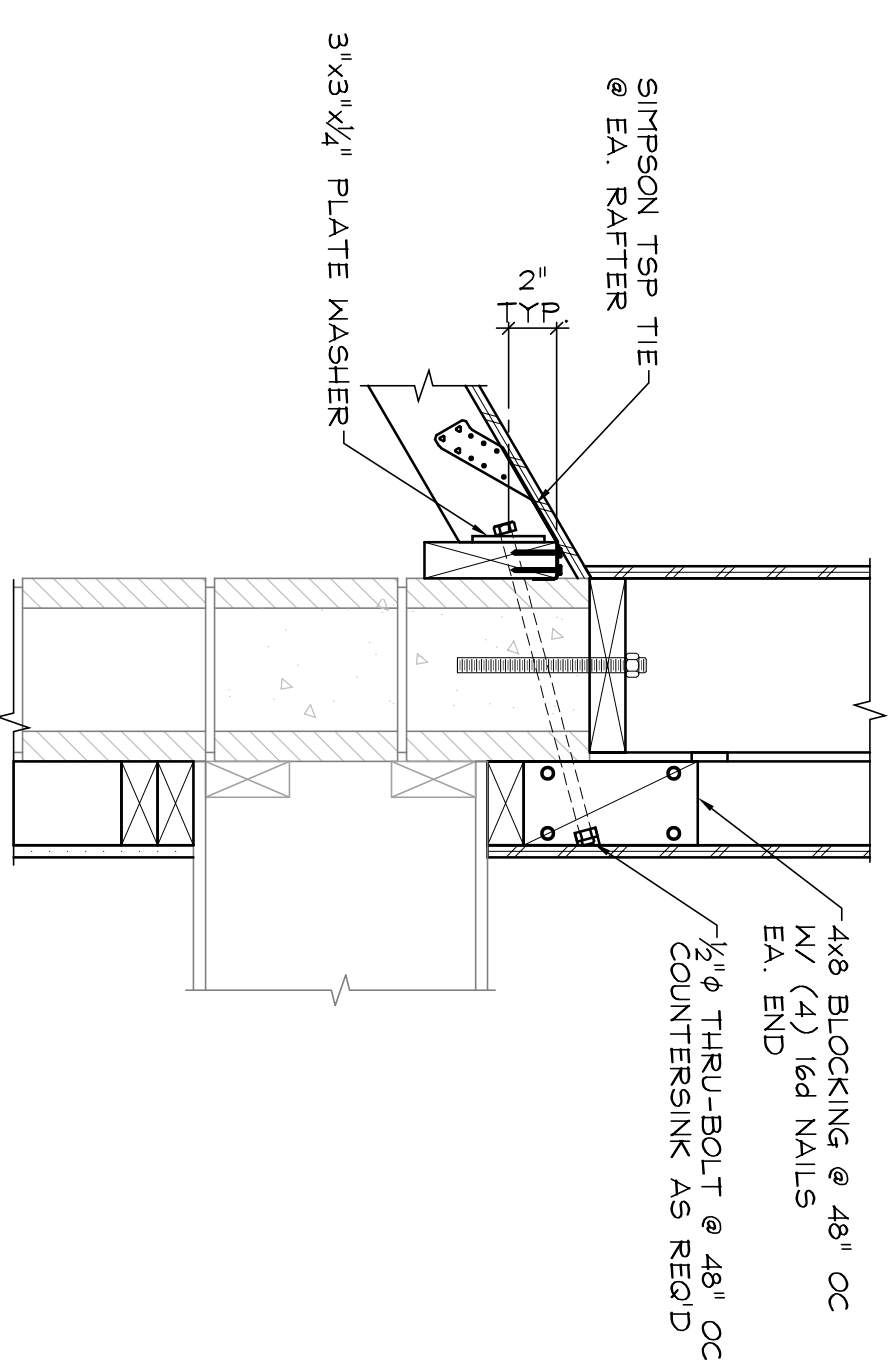
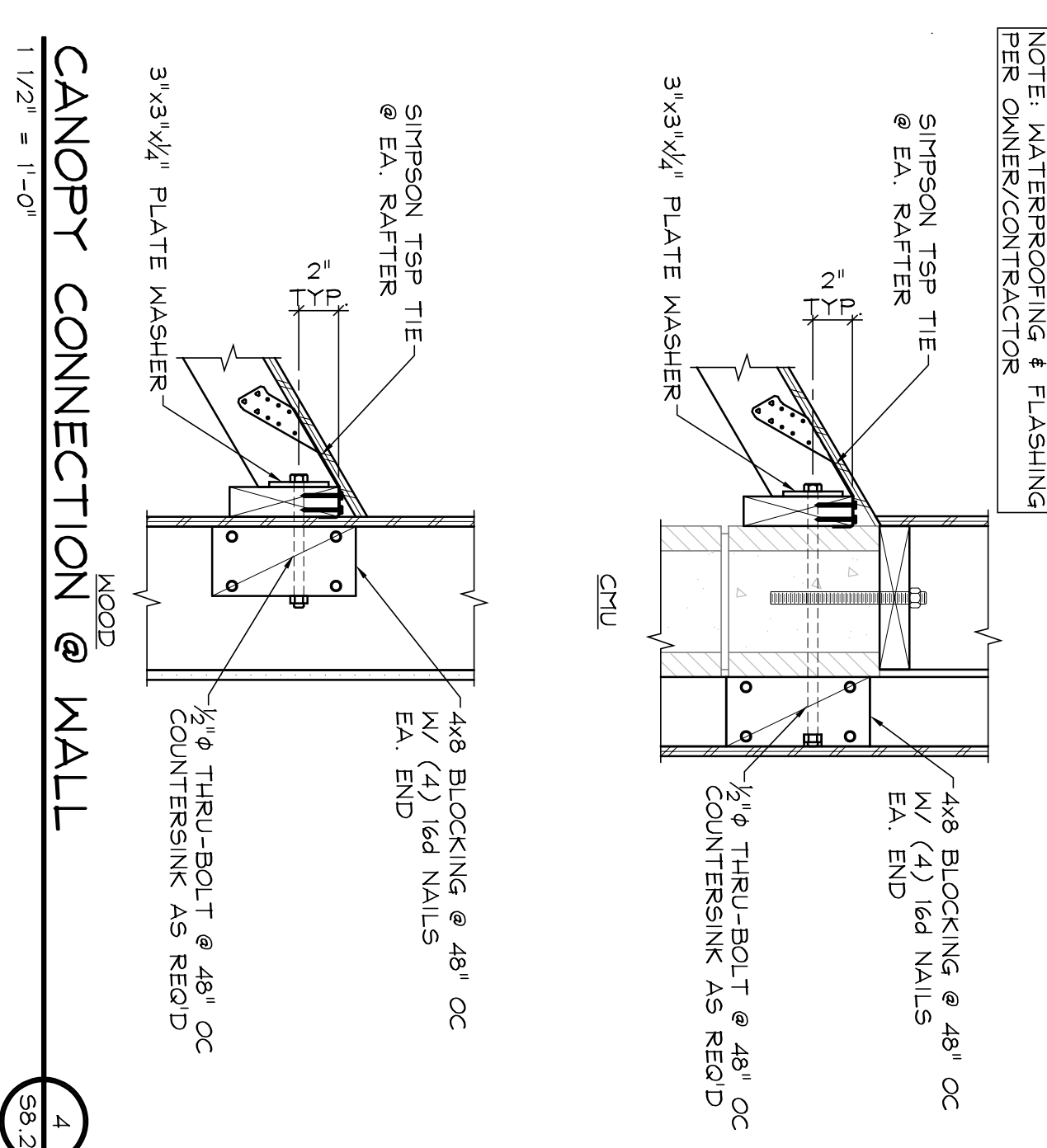
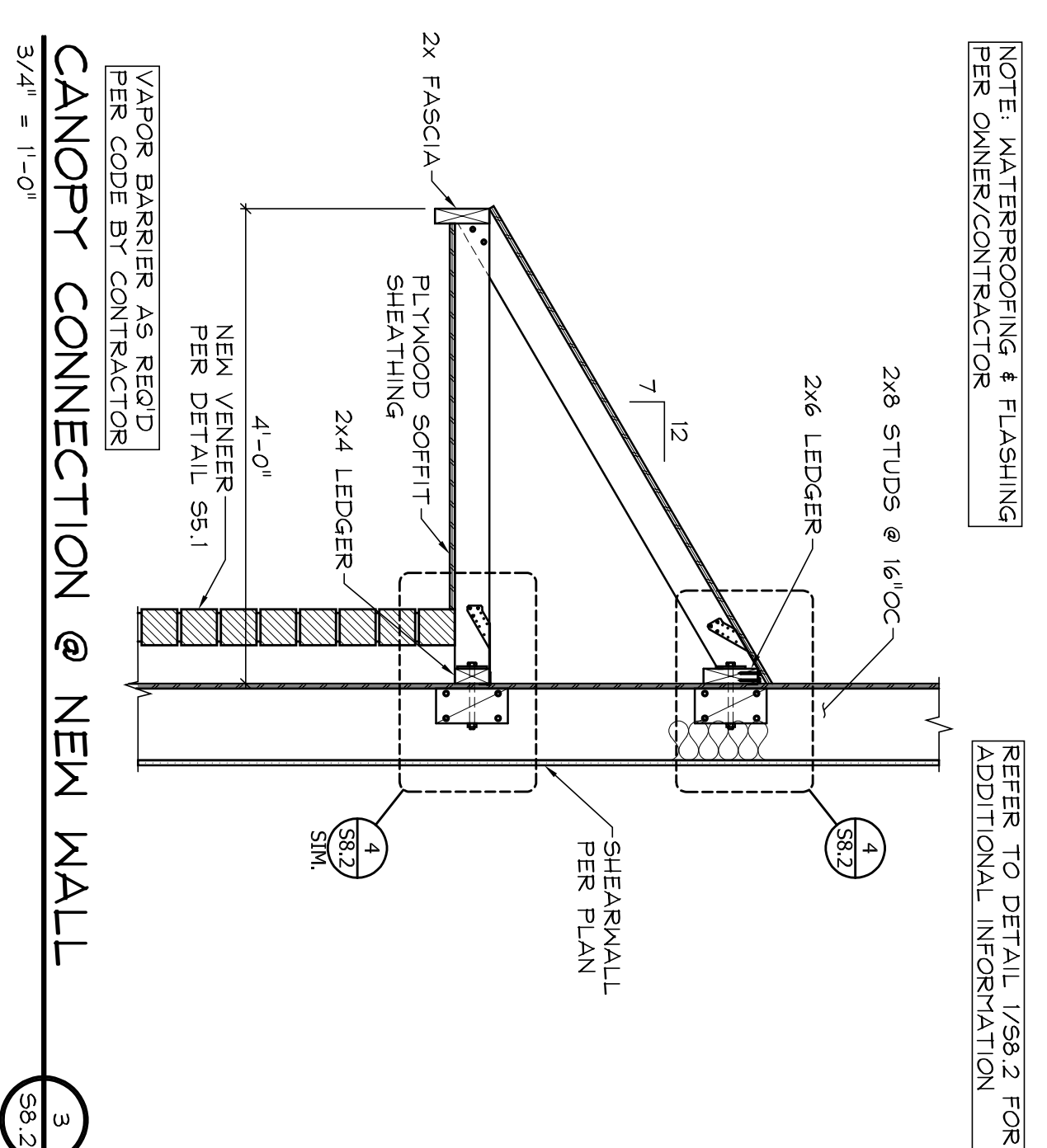
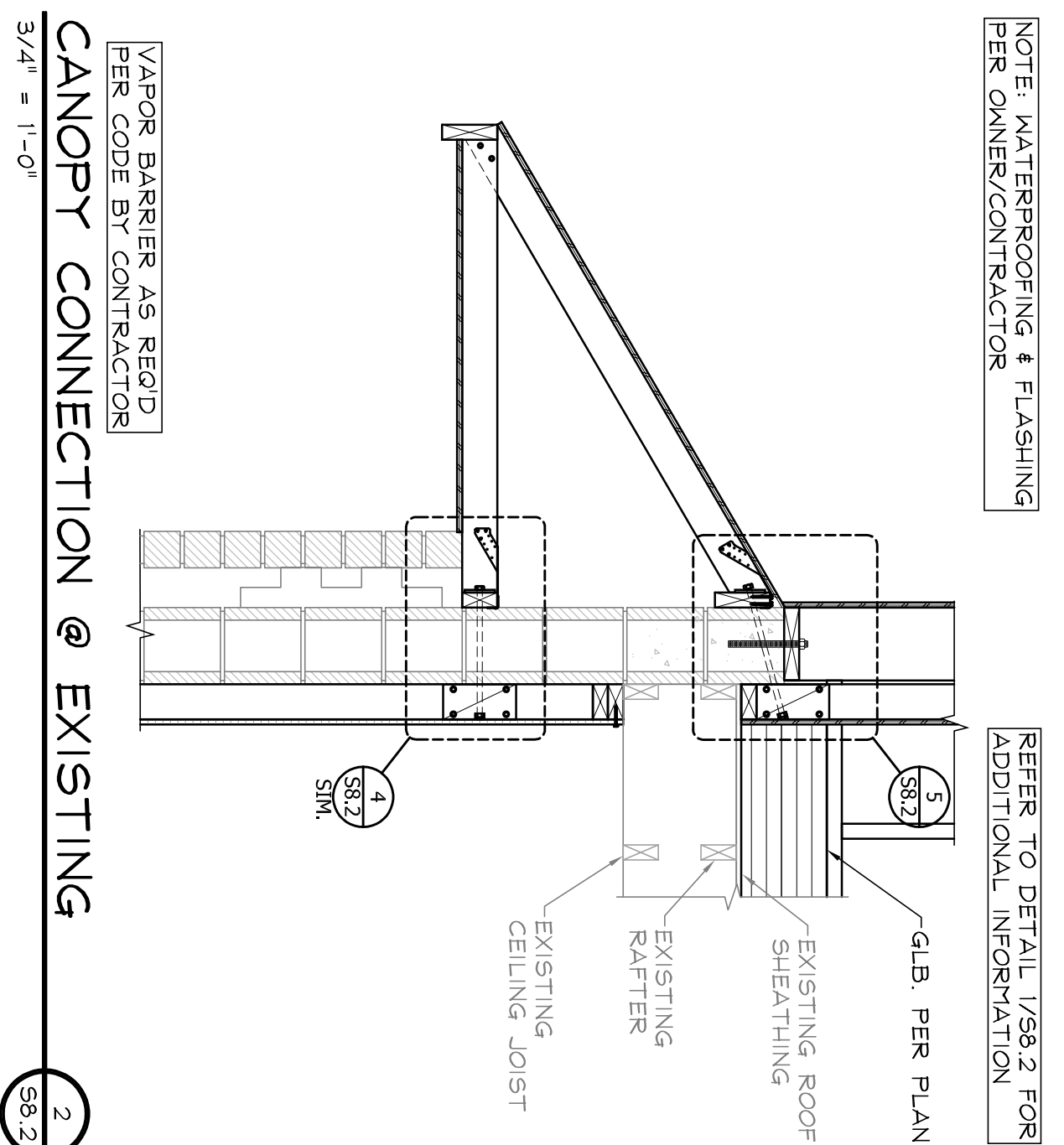
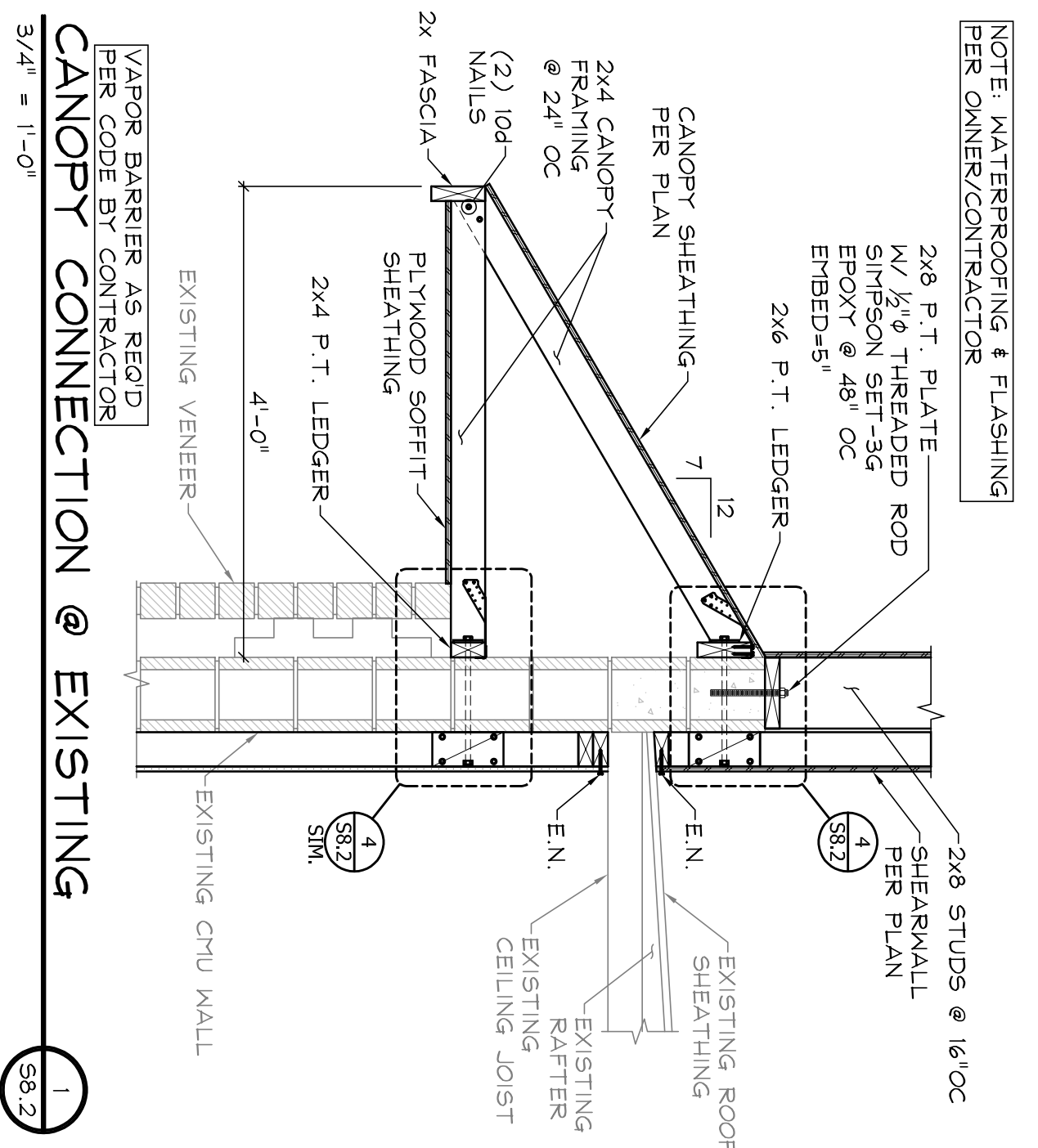
ROOF DETAILS
 2219 NW HWY 101
 2219 NW HWY 101
 LINCOLN CITY, OREGON
 FOR: EXCELLENCE INC.

MSC ENGINEERS
 CONSULTING STRUCTURAL ENGINEERS
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S8.1
 11 of 13
 190418



CANOPY CONNECTION @ CMU WALL

5

1 1/2" = 1'-0"

58.2

CANOPY DETAILS

2219 NW HWY 101
2219 NW HWY 101
LINCOLN CITY, OREGON
FOR: EXCELLENCE INC.

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CHECKED BY: CBS
DATE: 05-18-20

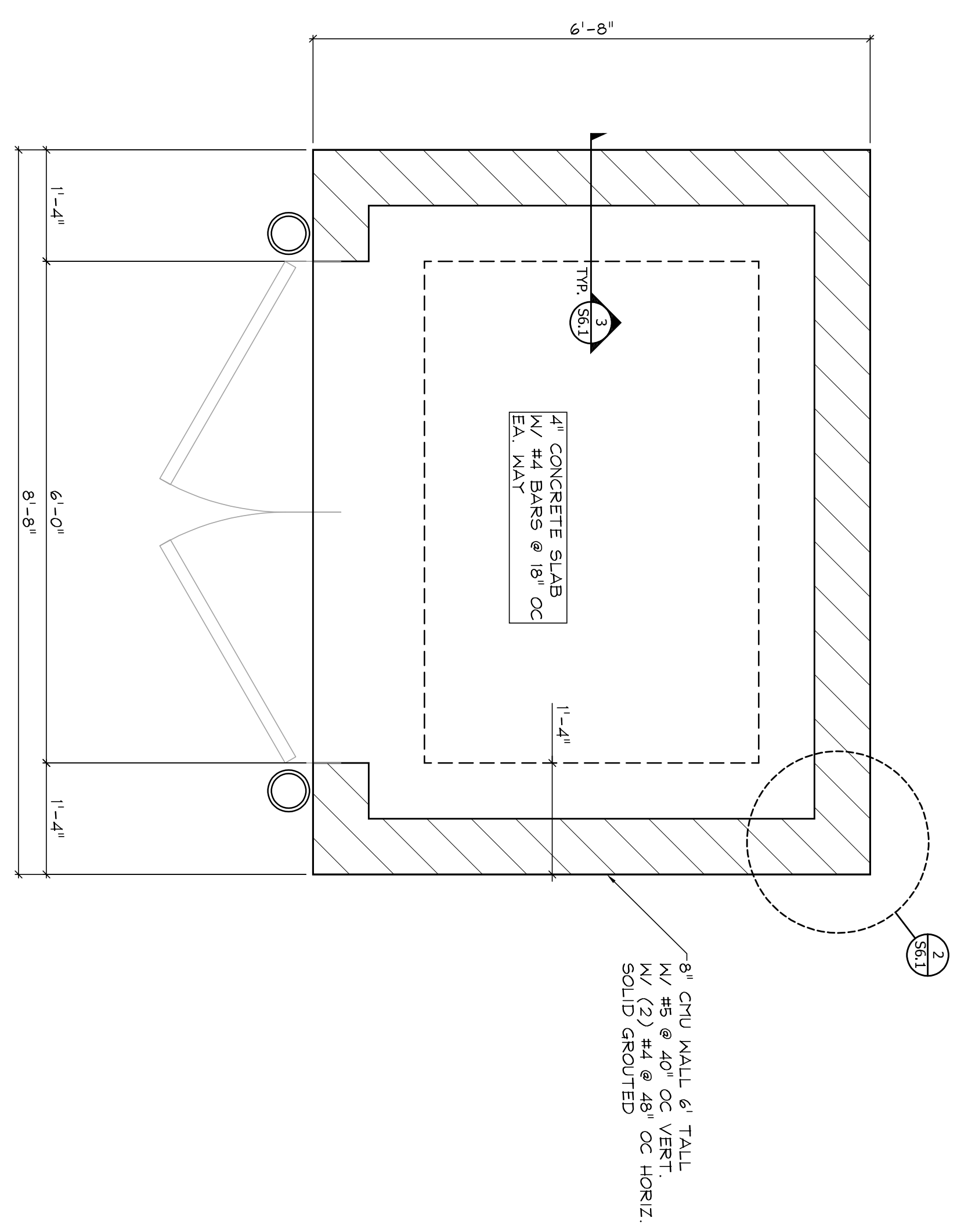
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S8.2
12 of 13

TRASH ENCLOSURE PLAN
3/4" = 1'-0"

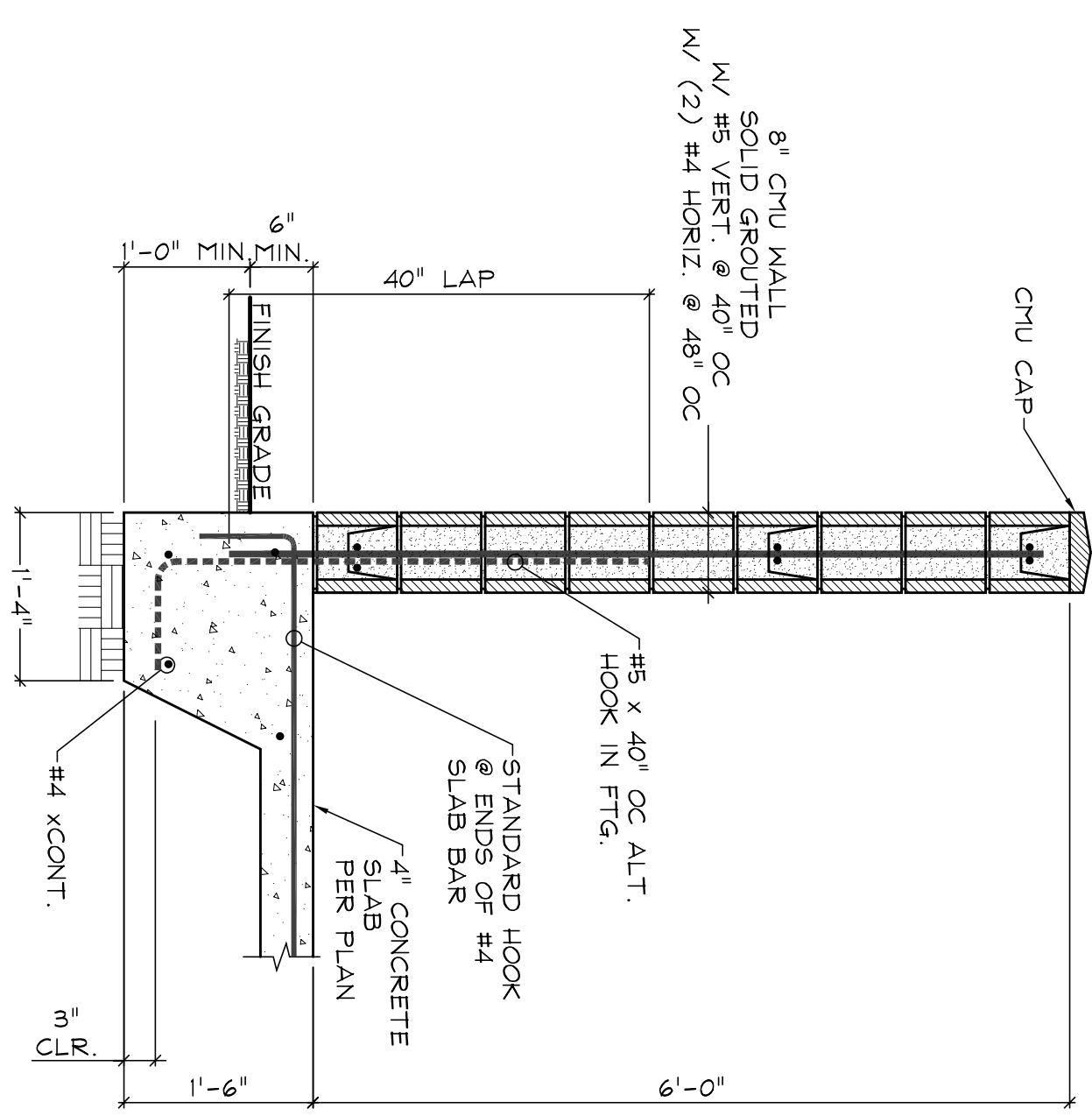
ENCLOSURE GATES & SUPPORT BY OTHERS
NOT TO BE ATTACHED TO CMU



1
3/4" = 1'-0"

TYPICAL TRASH ENCLOSURE WALL
3/4" = 1'-0"

3
3/4" = 1'-0"

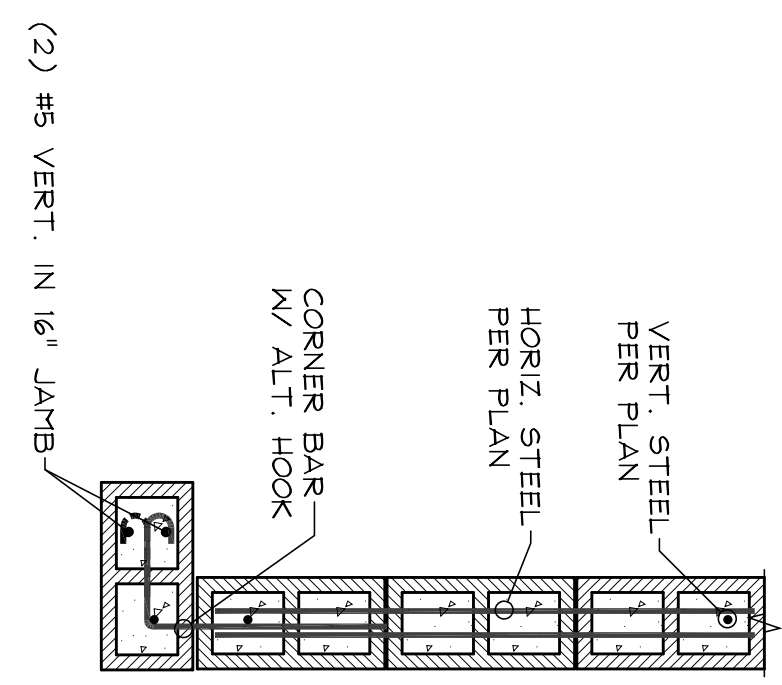
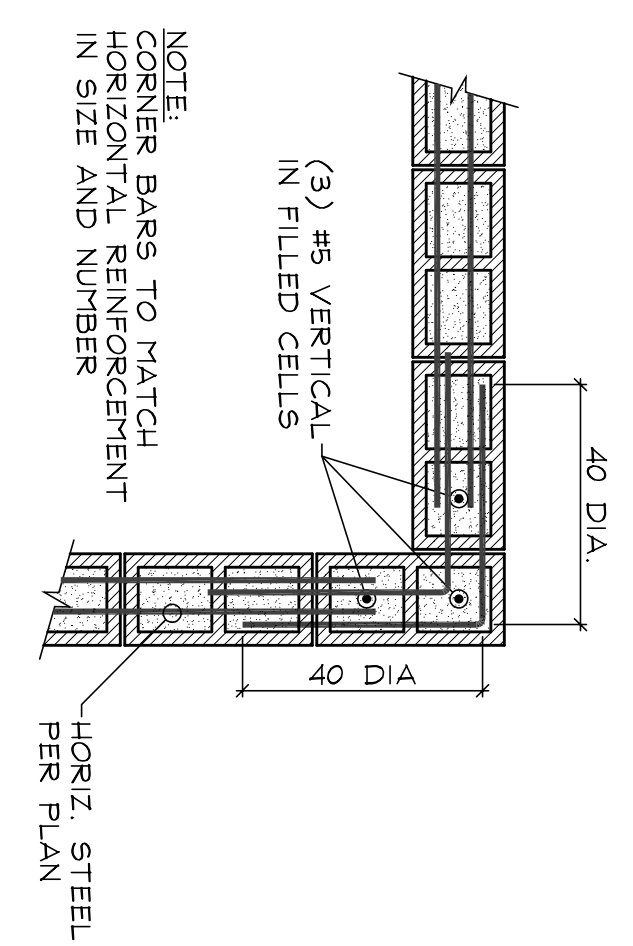


TYPICAL MASONRY WALL CORNER
3/4" = 1'-0"

2
3/4" = 1'-0"

MASONRY WALL END @ JAMB
3/4" = 1'-0"

3
3/4" = 1'-0"



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TRASH ENCLOSURE
2219 NW HWY 101
2219 NW HWY 101
LINCOLN CITY, OREGON
FOR: EXCELLENCE INC.

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REVISION:

CHECKED BY: CBS
DATE: 05-18-20

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Applicant Gregory Wescott
 PO Box 1255
 Lincoln City, OR 97367
excellenceinc@charter.net

Owner Wescott Gregory D Trustee
 PO Box 1255
 Lincoln City, OR 97367
 excellenceinc@charter.net

Site Information

Address: 2219 NW Highway 101
 Location: Northwest corner of the Highway 101/22nd Street intersection
 Map/Tax Lot: 07-11-10-AD-02800
 Area: 0.24 acre or 10,454.4 square feet
 Zoning: General Commercial (GC)

Surrounding Zones

North General Commercial (GC)
 South General Commercial (GC)
 East Highway 101 and General Commercial (GC)
 West Single-Family Residential (R-1-5)

Existing Conditions

The subject property (site) contains asphalt paving and an existing 740.44-square-foot building that is vacant. The building was previously used as a gas station. The site has an existing approach from Highway 101 and an existing approach from NW 22nd Street.

The site does not contain any significant natural resources. There are no significant wetlands, area of significant riparian habitat, or floodplain/floodway. The site does not contain any geologic hazards or identified areas of erosion. The site does not contain any historic structures or buildings, nor does the site contain any known archaeological resources. The applicant is not aware of any resource constraints that would prevent development as described.

Proposed Project

The applicant proposes to convert the existing vacant structure from a former gas station to office/retail. Professional and business offices and convenience service/retail uses are permitted uses in the GC zoning district.

The project requires a minimum of seven parking spaces, and seven parking spaces are being provided. Required parking area landscaping is being provided along with wheel stops and curbing. Bicycle parking will be provided in the form of one bicycle parking rack for a total of two spaces. The required solid waste container area will be enclosed with a 6-foot-tall sight-obscuring CMU wall. The required landscaping areas and pedestrian space are being provided. This application does not include any requests for signage. Sign permit applications will be submitted at a later date.

Lincoln City Municipal Code (LCMC) Title 17 provides the planning and zoning requirements for the development. Compliance with the applicable chapters and sections of LCMC Title 17 is detailed below. The project meets all applicable standards.

Access will be provided from the two existing approaches. The existing approach on Highway 101 will serve as a right-in/right-out only access. The existing approach on NW 22nd Street will allow full entering and exiting. The necessary permit for the existing driveway approach on Highway 101 will be obtained from the Oregon Department of Transportation to change the use from a gas station approach to an office/retail approach. No new driveway approaches are being requested.

City sewer and water services already exist at the site. Storm water will be captured on the site and treated per Lincoln City standards. Final civil engineering drawings, stamped and signed by an Oregon-licensed civil engineer, will be submitted as part of the structural permit application package after development review approval has been received.

Proposed Variance

The required development review application has already been submitted and is under review. For the development review application to be approved, approval of a variance is needed to provide the required parking behind the building. Per LCMC 17.56.030, seven parking spaces are required for the site. Per LCMC 17.56.080, the required spaces must be located on the same lot or parcel as the use; above, beneath, to the rear, or to the side of the building; outside of the required building areas; at least 10 feet from any public right-of-way; such that no backing or maneuvering takes place in right-of-way; and, in no case shall any parking spaces be located between the front of the building and the front property line.

The location of the proposed parking spaces meets all the criteria but one. The required spaces are located on the same lot as the use. The spaces are located to the rear of the building. The spaces are more than 10 feet from any public right-of-way. The spaces are located such that all backing and maneuvering takes place on the site and not within any right-of-way. Not a single parking space is between the front of the building and the front property line. However, the spaces encroach into the 12-foot rear building setback by approximately 4 feet.

The reasons for the encroachment are as follows:

1. The spaces are prohibited from being in front of the building.
2. The building is an existing building constructed in 1940. The property owner is preserving and repurposing the existing building rather than demolishing it. The building is located roughly in the center of the site. New construction would have allowed the building to be placed close to the front property line, but this isn't new construction. Preserving and repurposing the building, rather than demolishing it, was chosen as the most economical, environmentally-friendly, and sustainable option. Additionally, even though the building is not specifically designated as historic by the state, buildings constructed more than 50 years ago are considered historic. Accordingly, preservation of the building in its current location preserves a historic building.
3. There isn't enough space on either side of the building to place all the required parking spaces, provide the required drive aisle width and site circulation, and provide the required percentage of landscaping, parking landscaping, and the required 4-foot-deep landscape buffer at the perimeter of parking spaces and drive aisles.
4. Standard spaces are required to be 20 feet deep. While compact spaces are only required to be 16 feet deep, no more than 50% of the required spaces can be compact. By providing the required depth for the standard parking spaces, the required width of the drive aisle, and the required 4-foot-deep landscape buffer, a portion of the front of the parking spaces ends up in the 12-foot rear building setback.

LCMC TITLE 17

CHAPTER 17.77 APPLICATIONS

17.77.140 Variance

- A. *Procedure. Variance applications are subject to the Type III procedure, as described in LCMC 17.76.050.*

Response: The applicant understands that the variance application is subject to the Type III procedure.

- B. *Submittal Requirements. Type III application submittal requirements are set forth in LCMC 17.76.050 and more specific submittal requirements are provided on application forms and checklists as authorized in LCMC 17.76.110.*

Response: The required materials are included with the submission.

- C. *Approval Criteria. To approve a variance, the planning commission shall make findings of fact, based on evidence provided, that all of the following circumstances exist:*
1. *Exceptional or extraordinary circumstances apply to the property which do not apply generally to other properties in the same zone or vicinity and result from lot size or shape legally existing prior to the date of the ordinance codified in this chapter, topography, or other circumstances over which the property owner has no control;*

Response: The site contains an existing building that was constructed in 1940, which is prior to the date of the ordinance codified in this chapter. The placement of the building, roughly in the center of the site and not adjacent to the front property line, would not be allowed under the current code. The unusual placement of the building, in conjunction with the criteria in LCMC Chapter 17.56 regarding the size and location of parking spaces, drive aisles, and parking screening and landscaping, prevent compliance with the criterion regarding parking spaces not being in the building setback area.

The property owner does not have any control over the placement of the building because the building is existing. The property owner does not have any control over the requirements in LCMC Chapter 17.56. It is recognized that the requirements in LCMC Chapter 17.56 apply generally to other properties in the same zone, but other properties in the same zone are not preserving and repurposing an existing building, built in 1940 prior to the codification of the existing code and that is placed on the site contrary to existing code because that code didn't exist when the building was placed. The property owner has chosen to preserve and repurpose the existing building, rather than demolishing it and building a new building in the proper location, as this is the more economical, environmentally-friendly, sustainable, and history-preserving option.

2. *The variance is necessary for the preservation of a property right of the property owner which is substantially the same as owners of other property in the same zone or vicinity possess;*

Response: The site's GC zone allows retail and office uses, which are allowed uses on anyone's property in the GC zone. These uses require one parking space per 400 square feet of floor area. Accordingly, for these uses to be allowed the required parking must be provided. Without the required parking, the site could not be utilized for retail and office uses. The only way to provide the required parking and still meet all the other criteria is to have the approximate 4-foot encroachment into the rear building setback area.

3. *The variance should not be materially detrimental to the purposes of this title, or to property in the zone or vicinity in which the property is located, or otherwise conflict with the objectives of any city planning policy;*

Response: The site contains an existing retaining wall along the west boundary. The project proposes construction of another retaining at the front end of the parking spaces with a 4-foot-deep landscape buffer on the other side of the retaining wall. The west boundary line is adjacent to the Oar Street public right-of-way. On the other side of the public right-of-way is the residential zone. The retaining walls and landscaping will provide an effective screen and buffer along the public right-of-way. With the retaining walls, landscaping, sidewalk, and street between the parking spaces and the residential zone, the 4-foot encroachment into the west building setback should not be detrimental to the properties in the residential zone across Oar Street. The objective of the criterion requiring no parking spaces in a building setback area is to provide distance and buffering between properties and parking areas. In this case, there is already a public right-of-way (street and sidewalk) between the site and the residential zone on the west side of Oar Street. The addition of the retaining walls and landscaping provides even greater buffering, so the objective is still being met with the minor encroachment.

4. *The variance requested is the minimum variance which would alleviate the hardship.*

Response: The hardship is providing the required number of parking spaces outside of the building setback along the west boundary while still meeting all the parking, drive aisle, and parking landscaping and screening requirements. All the parking, drive aisle, and parking landscaping and screening requirements are being met except the requirement for having the parking spaces outside of the rear building setback area. The building setback along the west boundary, since it is adjacent to a residential zone, is 12 feet. The parking spaces encroach into the 12-foot setback by approximately 4 feet. The variance being requested is to allow the front 4 feet of the parking spaces to be in the 12-foot setback. This is the minimum variance that will allow for placement of the required parking spaces. No more than the 4-foot-encroachment is necessary, and no more than 4 feet is being requested.

DRAWINGS FOR:

SITE IMPROVEMENT 2219 NW HWY 101 LINCOLN CITY, OR

FOR:

EXCELLENCE INC.
1441 NE 11th St,
Lincoln City, OR 97367



Know what's below.
Call before you dig.

NOTES

1. BASIS OF BEARINGS AND COORDINATE SYSTEM IS BASED ON OREGON STATE PLANE NORTH ZONE, NAD83(2011), EPOCH 2010.00. ALL DISTANCES SHOWN HEREON ARE GROUND DISTANCES.
2. ELEVATIONS WERE ESTABLISHED FROM GPS OBSERVATIONS PROCESSED THROUGH THE NOAA'S ONLINE POSITIONING USER SERVICE (OPUS) AT CONTROL POINT NUMBER 1. ELEVATION = 122.70' (NAVD88)
3. PER ORS 209.150, ANY SURVEY MONUMENT REMOVED, DISTURBED OR DESTROYED SHALL BE REPLACED BY A PROFESSIONAL LAND SURVEYOR WITHIN 90 DAYS AT THE EXPENSE OF THE PERSON OR PUBLIC AGENCY RESPONSIBLE FOR SAID REMOVAL, DISTURBANCE OR DESTRUCTION.
4. FIELD SURVEYED JUNE, 2019.

PROJECT LOCATION



VICINITY MAP

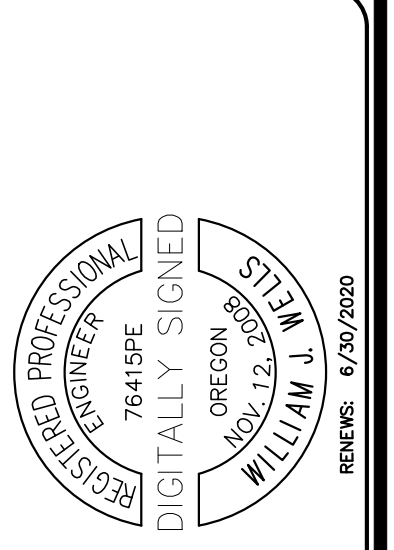
LEGEND

- FOUND MONUMENT
- CATCH BASIN
- SANITARY SEWER MANHOLE
- CLEANOUT
- WATER METER
- WATER VALVE
- FIRE HYDRANT
- ELECTRICAL BOX
- POWER METER
- ELECTRICAL TRANSFORMER
- LIGHT POLE
- UTILITY POLE
- GUY WIRE
- COMMUNICATION RISER
- TRAFFIC SIGNAL POLE
- TRAFFIC SIGNAL BOX
- SIGN
- AIR CONDITIONING UNIT
- MONITOR WELL
- CONCRETE
- GRAVEL
- SEWER LINE UNDERGROUND
- STORM LINE UNDERGROUND
- WATERLINE UNDERGROUND
- COMMUNICATION LINE UNDERGROUND
- ELECTRICAL LINE UNDERGROUND
- GAS LINE UNDERGROUND
- AC ASPHALT
- CONC CONCRETE
- E.O.P. EDGE OF PAVEMENT
- HCR HANDICAP RAMP
- FF FINISH FLOOR
- IR IRON ROD
- IP IRON PIPE
- W/YPC WITH YELLOW PLASTIC CAP
- P.S.C. POINT OF SPIRAL CURVE
- DOC. NO. DOCUMENT NUMBER
- SF SQUARE FEET

SHEET INDEX

SHT NO	DESCRIPTION
CO.0	COVER SHEET, VICINITY MAP, & SHEET INDEX
C1.0	EXISTING CONDITIONS, DEMOLITION, & EROSION CONTROL PLAN
C1.1	EROSION CONTROL NOTES
C1.2	EROSION CONTROL DETAILS
C2.0	GRADING, DRAINAGE, AND SURFACING PLAN
C3.0	EXISTING IMPERVIOUS AREA
C4.0	CONSTRUCTION NOTES & TABLE
C5.0	CONSTRUCTION DETAILS
C5.1	CONSTRUCTION DETAILS

NO.	DATE	PER CITY COMMENTS	DESCRIPTION	BY
1	6-3-2021			IH



WESTTECH ENGINEERING, INC.
CONSULTING ENGINEERS AND PLANNERS

WE

3841 Fairview Industrial Dr. S.E., Suite 100, Salem, OR 97302
Phone: (503) 585-2474 Fax: (503) 585-3966
E-mail: westtech@westtech-eng.com

EXCELLENCE INC.

**COVER SHEET, VICINITY MAP,
& SHEET INDEX**

**DRAWING
CO.0**

**JOB NUMBER
3194.0000.0**

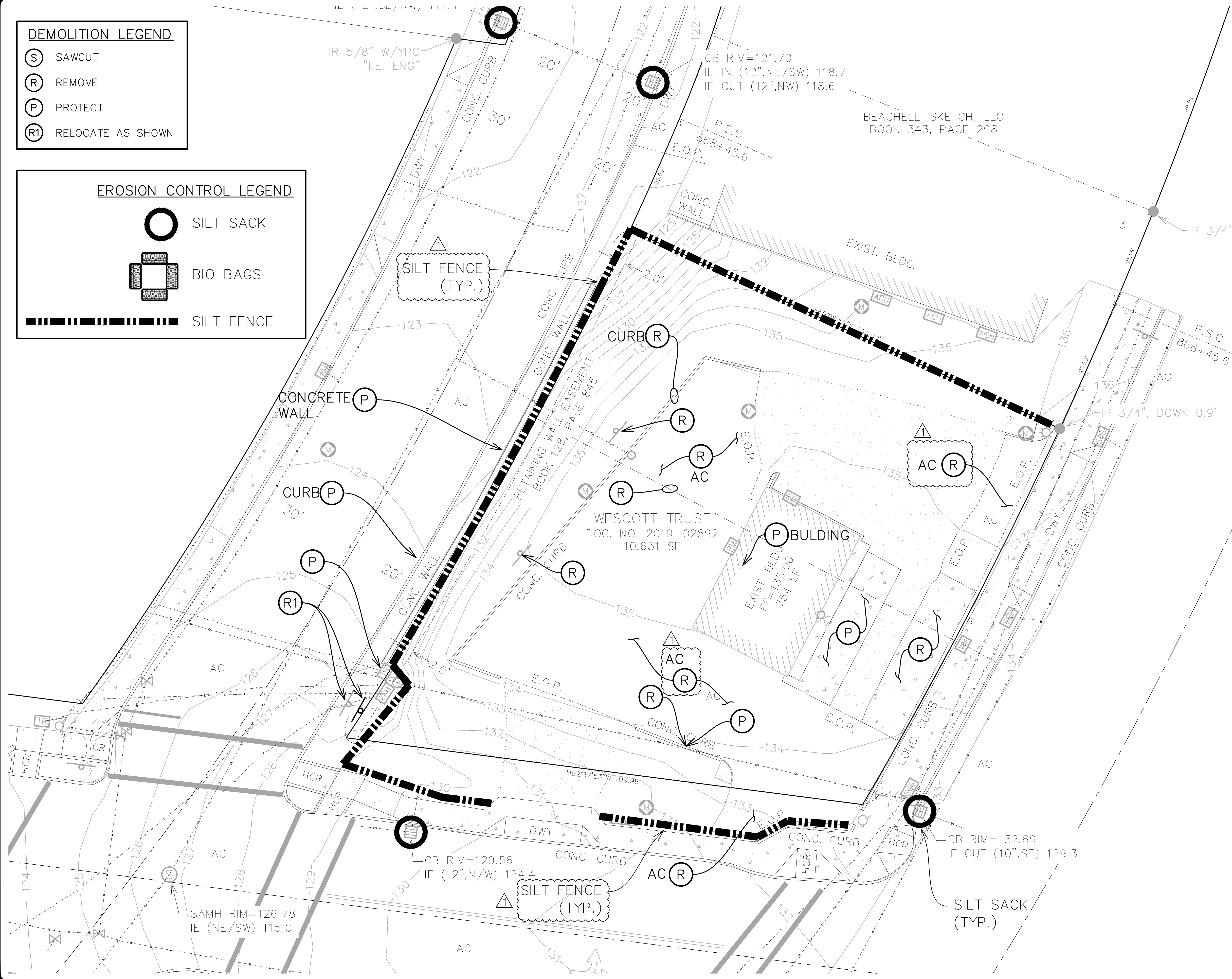
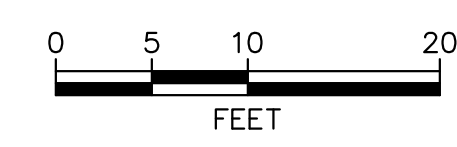
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DEMOLITION LEGEND

(S)	SAWCUT
(R)	REMOVE
(P)	PROTECT
(R1)	RELOCATE AS SHOWN

EROSION CONTROL LEGEND

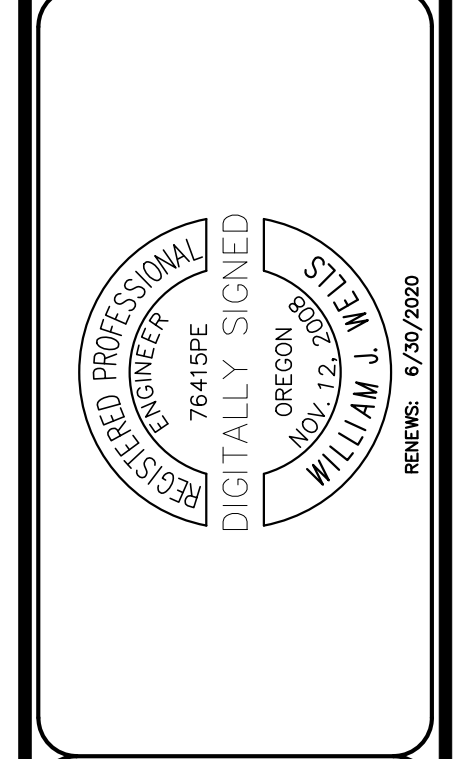
	SILT SACK
	BIO BAGS
	SILT FENCE



NO.	DATE	PER CITY COMMENTS	DESCRIPTION	BY
1	6-3-2021			JH

VERIFY SCALE
 BAR IS ONE INCH ON ORIGINAL DRAWING
 IF NOT ONE INCH ON SCALES ACCURACLY

DSN. JW
 DRN. JH
 CKD. JW
 DATE: JUNE 2019



WESTTECH ENGINEERING, INC.
 CONSULTING ENGINEERS AND PLANNERS

3841 Fairview Industrial Dr. S.E., Suite 100, Salem, OR 97302
 Phone: (503) 585-2474 Fax: (503) 585-3966
 E-mail: westtech@westtech-eng.com

EXCELLENCE INC.

**EXISTING CONDITIONS,
 DEMOLITION, & EROSION
 CONTROL PLAN**

DRAWING
C1.0

JOB NUMBER
3194.0000.0

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DEQ EROSION CONTROL STANDARD NOTES:

- 1. Hold a pre-construction meeting of project construction personnel that includes the inspector to discuss erosion and sediment control measures and construction limits. (Schedule A.8.c.i.(3))
2. All inspections must be made in accordance with DEQ 1200-C permit requirements. (Schedule A.12.b and Schedule B.1)
3. Inspection logs must be kept in accordance with DEQ's 1200-C permit requirements. (Schedule B.1.c and B.2)
4. Retain a copy of the ESCP and all revisions on site and make it available on request to DEQ, Agent, or the local municipality. During inactive periods of greater than seven (7) consecutive calendar days, the above records must be retained by the permit registrant but do not need to be at the construction site. (Schedule B.2.c)
5. All permit registrants must implement the ESCP. Failure to implement any of the control measures or practices described in the ESCP is a violation of the permit. (Schedule A 8.a)
6. The ESCP must be accurate and reflect site conditions. (Schedule A.12.c.i)
7. Submission of all ESCP revisions is not required. Submittal of the ESCP revisions is only under specific conditions. Submit all necessary revision to DEQ or Agent within 10 days. (Schedule A.12.c.iv. and v)
8. Phase clearing and grading to the maximum extent practical to prevent exposed inactive areas from becoming a source of erosion. (Schedule A.7.a.iii)
9. Identify, mark, and protect (by construction fencing or other means) critical riparian areas and vegetation including important trees and associated rooting zones, and vegetation areas to be preserved. Identify vegetative buffer zones between the site and sensitive areas (e.g., wetlands), and other areas to be preserved, especially in perimeter areas. (Schedule A.8.c.i.(1) and (2))
10. Preserve existing vegetation when practical and re-vegetate open areas. Re-vegetate open areas when practicable before and after grading or construction. Identify the type of vegetative seed mix used. (Schedule A.7.a.v)
11. Maintain and delineate any existing natural buffer within the 50-feet of waters of the state. (Schedule A.7.b.i.and (2)(a)(b))
12. Install perimeter sediment control, including storm drain inlet protection as well as all sediment basins, traps, and barriers prior to land disturbance. (Schedule A.8.c.i.(5))
13. Control both peak flow rates and total stormwater volume, to minimize erosion at outlets and downstream channels and streambanks. (Schedule A.7.c)
14. Control sediment as needed along the site perimeter and at all operational internal storm drain inlets at all times during construction, both internally and at the site boundary. (Schedule A.7.d.i)
15. Establish concrete truck and other concrete equipment washout areas before beginning concrete work. (Schedule A.8.c.i.(6))
16. Apply temporary and/or permanent soil stabilization measures immediately on all disturbed areas as grading progresses. Temporary or permanent stabilizations measures are not required for areas that are intended to be left unvegetated, such as dirt access roads or utility pole pads.(Schedule A.8.c.ii.(3))
17. Establish material and waste storage areas, and other non-stormwater controls. (Schedule A.8.c.i.(7))
18. Prevent tracking of sediment onto public or private roads using BMPs such as: construction entrance, graveled (or paved) exits and parking areas, gravel all unpaved roads located onsite, or use an exit tire wash. These BMPs must be in place prior to land-disturbing activities. (Schedule A 7.d.ii and A.8.c.i.(4))
19. When trucking saturated soils from the site, either use water-tight trucks or drain loads on site. (Schedule A.7.d.ii.(5))
20. Control prohibited discharges from leaving the construction site, i.e., concrete wash-out, wastewater from cleanout of stucco, paint and curing compounds. (Schedule A.6)
21. Use BMPs to prevent or minimize stormwater exposure to pollutants from spills; vehicle and equipment fueling, maintenance, and storage; other cleaning and maintenance activities; and waste handling activities. These pollutants include fuel, hydraulic fluid, and other oils from vehicles and machinery, as well as debris, fertilizer, pesticides and herbicides, paints, solvents, curing compounds and adhesives from construction operations. (Schedule A.7.e.i.(2))
22. Implement the following BMPs when applicable: written spill prevention and response procedures, employee training on spill prevention and proper disposal procedures, spill kits in all vehicles, regular maintenance schedule for vehicles and machinery, material delivery and storage controls, training and signage, and covered storage areas for waste and supplies. (Schedule A. 7.e.iii.)
23. Use water, soil-binding agent or other dust control technique as needed to avoid wind-blown soil. (Schedule A 7.a.iv)
24. The application rate of fertilizers used to reestablish vegetation must follow manufacturer's recommendations to minimize nutrient releases to surface waters. Exercise caution when using time-release fertilizers within any waterway riparian zone. (Schedule A.9.b.iii)
25. If an active treatment system (for example, electro-coagulation, flocculation, filtration, etc.) for sediment or other pollutant removal is employed, submit an operation and maintenance plan (including system schematic, location of system, location of inlet, location of discharge, discharge dispersion device design, and a sampling plan and frequency) before operating the treatment system. Obtain plan approval before operating the treatment system. Operate and maintain the treatment system according to manufacturer's specifications. (Schedule A.9.d)
26. Temporarily stabilize soils at the end of the shift before holidays and weekends, if needed. The registrant is responsible for ensuring that soils are stable during rain events at all times of the year. (Schedule A 7.b)
27. As needed based on weather conditions, at the end of each workday soil stockpiles must be stabilized or covered, or other BMPs must be implemented to prevent discharges to surface waters or conveyance systems leading to surface waters. (Schedule A 7.e.ii.(2))
28. Construction activities must avoid or minimize excavation and bare ground activities during wet weather. (Schedule A.7.a.i)
29. Sediment fence: remove trapped sediment before it reaches one third of the above ground fence height and before fence removal. (Schedule A.9.c.i)
30. Other sediment barriers (such as biobags): remove sediment before it reaches two inches depth above ground height and before BMP removal. (Schedule A.9.c.i)
31. Catch basins: clean before retention capacity has been reduced by fifty percent. Sediment basins and sediment traps: remove trapped sediments before design capacity has been reduced by fifty percent and at completion of project. (Schedule A.9.c.iii& iv)
32. Within 24 hours, significant sediment that has left the construction site, must be remediated. Investigate the cause of the sediment release and implement steps to prevent a recurrence of the discharge within the same 24 hours. Any in-stream clean-up of sediment shall be performed according to the Oregon Division of State Lands required timeframe. (Schedule A.9.b.i)
33. The intentional washing of sediment into storm sewers or drainage ways must not occur. Vacuuming or dry sweeping and material pickup must be used to cleanup released sediments. (Schedule A.9.b.ii)
34. The entire site must be temporarily stabilized using vegetation or a heavy mulch layer, temporary seeding, or other method should all construction activities cease for 30 days or more. (Schedule A.7.f.i)
35. Provide temporary stabilization for that portion of the site where construction activities cease for 14 days or more with a covering of blown straw and a tackifier, loose straw, or an adequate covering of compost mulch until work resumes on that portion of the site. (Schedule A.7.f.ii)
36. Do not remove temporary sediment control practices until permanent vegetation or other cover of exposed areas is established. Once construction is complete and the site is stabilized, all temporary erosion controls and retained soils must be removed and disposed of properly, unless doing so conflicts with local requirements. (Schedule A.8.c.iii(1) and D.3.c.ii and iii)

Rev. 12/15/15 By: Krista Ratliff

Table with columns for YEAR, MONTH, and various construction activities (CLEARING, EXCAVATION, GRADING, CONSTRUCTION, SEDIMENT CONTROLS, etc.) with X marks indicating completion status for years '20 to '21.

Table with columns for CONTROL MEASURE and PHASE 1 through PHASE 5, detailing implementation status for various measures like Silt Fencing, Construction Entrance, etc.

INSPECTION FREQUENCY FOR BMP

Table with columns for Site Condition and Minimum Frequency, detailing inspection requirements for active periods, site inactivity, and weather conditions.

BMP Rationale

A comprehensive list of available Best Management Practices (BMP) options based on DEQ's 1200-C Permit Application and ESCP Guidance Document has been reviewed to complete this Erosion and Sediment Control Plan. Some of the above listed BMPs were not chosen because they were determined to not effectively manage erosion prevention and sediment control for this project based on specific site conditions, including soil conditions, topographic constraints, accessibility to the site, and other related conditions. As the project progresses and there is a need to revise the ESCP, an Action Plan will be submitted.

SOIL TYPE(S): PER LINCOLN CO. SOIL SURVEY THE SITE SOILS INCLUDE "URBAN LAND-NELSCOTT COMPLEX, 0 TO 12 PERCENT SLOPES"
EROSION HAZARD: PER LINCOLN CO. SOIL SURVEY EROSION HAZARD IS "NOT RATED"
SITE AREA: .25 AC
DISTURBANCE AREA: .13 AC

SUPPLEMENTAL WESTECH NOTES:

- 1. Erosion control measures shall be maintained in such a manner as to ensure that sediment and sediment-laden water does not enter the drainage system, roadways, or violate applicable water quality standards.
2. The erosion control construction, maintenance, replacement and upgrading of the erosion control facilities is the responsibility of the Contractor until all construction is completed and approved, and permanent erosion control (i.e. vegetation/landscaping) is established on all disturbed areas.
3. All recommended erosion control procedures are dependent on construction methods, staging, site conditions, weather and scheduling. During the construction period, erosion control facilities shall be upgraded as necessary due to unexpected storm events and to ensure that sediment and sediment laden water does not leave the site.
4. The Contractor is responsible for control of sediment transport within project limits. If an installed erosion control system does not adequately contain sediment on site, then the erosion control measures shall be adjusted or supplemented by the Contractor as necessary to ensure that sediment laden water does not leave the site. Additional measures shall be provided as required to ensure that all paved areas are kept clean for the duration of the project. Additional interim measures will include, at a minimum, installation of silt fences in accordance with the details shown on the drawings. These measures shall be installed along all exposed embankments and cut slopes to prevent sediment transport.
5. All existing and newly constructed storm inlets and drains shall be protected until pavement surfaces are completed and/or vegetation is established.
6. Erosion control facilities and sediment fences on active sites shall be inspected by the Contractor at least daily during any period with measurable precipitation. Any required repairs or maintenance shall be completed immediately. The erosion control facilities on inactive sites shall be inspected and maintained by the Contractor a minimum of once a month or within 24 hours following the start of a storm event.
7. All catch basins and conveyance lines shall be cleaned prior to paving. The cleaning operation shall not flush sediment-laden water into the downstream system. The Contractor shall remove all accumulated sediment from all impacted catch basins and storm pipes prior to acceptance by the Owner.
8. The Contractor is solely responsible for protection of all adjacent property and downstream facilities from erosion and siltation during project construction. Any damage resulting from such erosion and siltation shall be corrected at the sole expense of the Contractor.
9. The Contractor shall provide site watering as necessary to prevent wind erosion of fine-grained soils.
10. Unless otherwise indicated on the drawings, all temporary erosion control facilities, including sediment fences, silt sacks, bio-bags, etc. shall be removed by the Contractor within 30 days after permanent landscaping/vegetation is established.
11. Sediment fences shall be constructed of continuous filter fabric to avoid use of joints. When joints are necessary, filter cloth shall be spliced together only at a support post, with a minimum 6-inch overlap, and both ends securely fastened to a post.
12. Sediment fence shall be installed per drawing details. Sediment fences shall have adequate support to contain all silt and sediment captured.
13. The standard strength filter fabric shall be fastened securely to stitched loops installed on the upslope side of the posts, and 6 inches of the fabric shall be extended into the trench. The fabric shall not extend more than 30 inches above the original ground surface. Filter fabric shall not be stapled to existing trees.
14. Bio-filter bags shall be clean 100 percent wood product waste. Bags shall be 18-inch x 18-inch x 30-inch, weigh approximately 45 lbs., and be contained in a bag made of 1/2-inch plastic mesh.
15. Sediment barriers shall be maintained until the up-slope area has been permanently stabilized. At no time shall more than 10-inches of sediment be allowed to accumulate behind sediment fences. No more than 2 inches of sediment shall be allowed to accumulate behind bio-filter bags. Sediment shall be removed prior to reaching the above stated depths. New sediment barriers shall be installed uphill as required to control sediment transport.
16. Stabilized construction entrances shall be installed at the beginning of construction and maintained for the duration of the project. Additional measures may be required to ensure that all paved areas are kept clean for the duration of the project.
17. The Contractor shall verify that all trucks are well sealed when transporting saturated soils from the site. Water dripage from trucks transporting saturated soils must be reduced to less than 1 gallon per hour prior to leaving the site.
18. The entrance shall be maintained in a condition that will prevent tracking or flow of mud onto the public right-of-way or approved access point. The entrance may require periodic top dressing as conditions demand, and repair and/or cleanout of any structures used to trap sediment.
19. All materials spilled, dropped, washed, or tracked from vehicles onto roadways or into storm drains must be removed immediately, and the Contractor shall provide protection of downstream inlets and catch basins to ensure sediment laden water does not enter the storm drain system.
20. Temporary grass cover measures must be fully established by October 15th, or other cover measures (ie. erosion control blankets with anchors, 3-inches minimum of straw mulch, 6 mil HDPE plastic sheet, etc.) shall be in place over all disturbed soil areas until April 30th. To establish an adequate grass stand for controlling erosion by October 15th, it is recommended that seeding and mulching occur by September 1st. Straw mulch, if used, shall not leave any bare ground visible through the straw.
21. Minimum wet weather slope protection. For slopes steeper than 3H:1V but less than 2H:1V, use Tensor/North American Green Type S150 erosion control blanket. For slopes 2H:1V or steeper, use Tensor/North American Green Type SC150 erosion control blanket. Use a minimum of 2-inches straw mulch or Tensor/North American Green Type S150 for slopes flatter than 3H:1V. Slope protection shall be placed on all disturbed areas immediately after completion of each section of construction activity, until the erosion control seeding has been established. As an option during temporary or seasonal work stoppages, a 6-mil HDPE plastic sheet may be placed on exposed slopes. The plastic sheet shall be provided with an anchor trench at the top and bottom of the slope, and shall be sandbagged on the slopes as required to prevent damage or displacement by wind.
22. Permanent erosion control vegetation on all embankments and disturbed areas shall be re-established as soon as construction is completed.
23. Soil preparation. Topsoil should be prepared according to landscape plans, if available, or recommendations of grass seed supplier. It is recommended that slopes be textured before seeding by rock walking (ie. driving a crawling tractor up and down the slopes to leave a pattern of cleat imprints parallel to slope contours) or other method to provide stable areas for seeds to rest.
24. When used, hydromulch shall be applied with grass seed at a rate of 2000 lbs. per acre between April 30 and June 10, or between September 1 and October 1. On slopes steeper than 10 percent, hydroseed and mulch shall be applied with a bonding agent (tackifier). Application rate and methodology to be in accordance with seed supplier recommendations.
25. When used in lieu of hydromulch, dry, loose, weed free straw used as mulch shall be applied at a rate of 4000 lbs. per acre (double the hydromulch application requirement). Anchor straw by working in by hand or with equipment (rollers, cleat trackers, etc.). Mulch shall be spread uniformly immediately following seeding.
26. When conditions are not favorable to germination and establishment of the grass seed, the Contractor shall irrigate the seeded and mulched areas as required to establish the grass cover.
27. Seeding. Recommended erosion control grass seed mix is as follows. Dwarf grass mix (low height, low maintenance) consisting of dwarf perennial ryegrass (80 % by weight), creeping red fescue (20 % by weight). Application rate shall be 100 lbs. per acre minimum.
28. Grass seed shall be fertilized at a rate of 10 lbs. per 1000 S.F with 16- 16-16 slow release type fertilizer. Development areas within 50 feet of water bodies and wetlands must use a non-phosphorous fertilizer.
29. Prior to starting construction contractor shall acquire the services of a DEQ Certified Erosion and Sediment Control Inspector and shall submit an "Action Plan" to DEQ identifying their names, contact information, training and experience as required in Schedule A.6.b.i-ii of the 1200-C Permit
30. Contractor shall submit "Notice of Termination" to DEQ to end the 1200-C permit coverage once all soil disturbance activities have been completed and final stabilization of exposed soils has occurred.

Professional seal and title block area containing EXCELLENCE INC. logo, WESTECH ENGINEERING, INC. logo, and drawing information: DRAWING C1.1, JOB NUMBER 3194.0000.0, DATE: JUNE 2019, and revision table.

SILT FENCE NOTES:

- BURY BOTTOM OF FILTER FABRIC 6" VERTICALLY BELOW FINISHED GRADE.
- TRENCH TO BE DUG WITH DITCH-WITCH, BY HAND OR OTHER METHOD AS REQUIRED TO MINIMIZE WIDTH.
- BACKFILL & COMPACT NATIVE SOIL IN TRENCH AFTER FENCE INSTALLATION.
- STITCHED LOOPS TO BE INSTALLED TO THE UPHILL SIDE OF THE FENCE.

MAINTENANCE NOTES:

- SEDIMENT BARRIERS SHALL BE MAINTAINED UNTIL UP-SLOPE AREA IS PERMANENTLY STABILIZED.
- AT NO TIME SHALL MORE THAN ONE FOOT OF SEDIMENT BE ALLOWED TO ACCUMULATE BEHIND SEDIMENT FENCES OR BIOFILTER BAGS.
- NEW SEDIMENT BARRIERS SHALL BE INSTALLED UPHILL AS REQUIRED TO CONTROL SEDIMENT TRANSPORT.

LAST REVISION DATE:	JO #
APRIL 2014	STANDARD
SEDIMENT BARRIERS	
(NTS)	
WESTECH ENG.	6110

BARRIER SPACING FOR GENERAL APPLICATION

SLOPE RATIO	MAXIMUM SPACING ON SLOPE BETWEEN WATTLES
10.0% OR FLATTER	50' O.C.
10.1% TO 20.0%	25' O.C.
20.1% TO 30.0%	10' O.C.
30.1% OR STEEPER	5' O.C.

NOTES:

- ALL MATERIAL SHALL CONFORM TO OSSC (ODOT/APWA) SPECIFICATIONS, CURRENT EDITION.
- SEDIMENT BARRIERS SHALL BE MAINTAINED UNTIL UP-SLOPE AREA IS PERMANENTLY STABILIZED.
- AT NO TIME SHALL SEDIMENT BE ALLOWED TO ACCUMULATE ABOVE THE TOP OF THE STRAW WATTLE.
- NEW SEDIMENT BARRIERS SHALL BE INSTALLED UPHILL AS REQUIRED TO CONTROL SEDIMENT TRANSPORT.

LAST REVISION DATE:	JO #
JUNE 2015	STANDARD
STRAW WATTLE SEDIMENT BARRIER	
(NTS)	
WESTECH ENG.	6120

MAINTENANCE NOTES:

- SEDIMENT BARRIERS SHALL BE MAINTAINED UNTIL UP-SLOPE AREA IS PERMANENTLY STABILIZED.
- AT NO TIME SHALL MORE THAN ONE FOOT OF SEDIMENT BE ALLOWED TO ACCUMULATE BEHIND SEDIMENT FENCES OR BIOFILTER BAGS.
- NEW SEDIMENT BARRIERS SHALL BE INSTALLED UPHILL AS REQUIRED TO CONTROL SEDIMENT TRANSPORT.

LAST REVISION DATE:	JO #
APRIL 2014	STANDARD
INLET SEDIMENT CONTROL	
(NTS)	
WESTECH ENG.	6130

MAINTENANCE NOTES:

- SEDIMENT BARRIERS SHALL BE MAINTAINED UNTIL UP-SLOPE AREA IS PERMANENTLY STABILIZED.
- AT NO TIME SHALL MORE THAN ONE FOOT OF SEDIMENT BE ALLOWED TO ACCUMULATE BEHIND BIOFILTER BAGS.
- NEW SEDIMENT BARRIERS SHALL BE INSTALLED UPHILL AS REQUIRED TO CONTROL SEDIMENT TRANSPORT.
- PT. 'A' SHALL BE 6" MIN. HIGHER THAN PT. 'B'.

LAST REVISION DATE:	JO #
APRIL 2014	STANDARD
DITCH AND SWALE EROSION PROTECTION	
(NTS)	
WESTECH ENG.	6140

NOTES:

- EMPTY SILT SACK AS NECESSARY.
- SILTSACK SEDIMENT CONTROL DEVICE AS MANUFACTURED BY ACF ENVIRONMENTAL AND SUPPLIED BY ACF WEST (503) 771-5115 OR APPROVED EQUAL.

LAST REVISION DATE:	JO #
OCT 2002	STANDARD
SILTSACK INLET DETAIL	
(NTS)	
WESTECH ENG.	6150

VERIFY SCALE	1"
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IF NOT ONE INCH ON SCALES ACCURACELY	
DSN.	JW
DRN.	JH
CKD.	JW
DATE:	JUNE 2019
NO.	1
DESCRIPTION	
REVISIONS	
BY	

REGISTERED PROFESSIONAL ENGINEER
76415PE
DIGITALLY SIGNED
WILLIAM J. WILLIAMS
OREGON
NOV. 12, 2008
REVIEWS: 6/20/2020

WESTECH ENGINEERING, INC.
CONSULTING ENGINEERS AND PLANNERS

3841 Fairview Industrial Dr. S.E., Suite 100, Salem, OR 97302
Phone: (503) 585-2474 Fax: (503) 585-3966
E-mail: westech@westech-eng.com




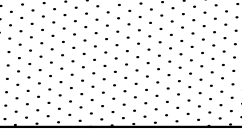
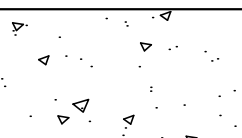


EXCELLENCE INC.

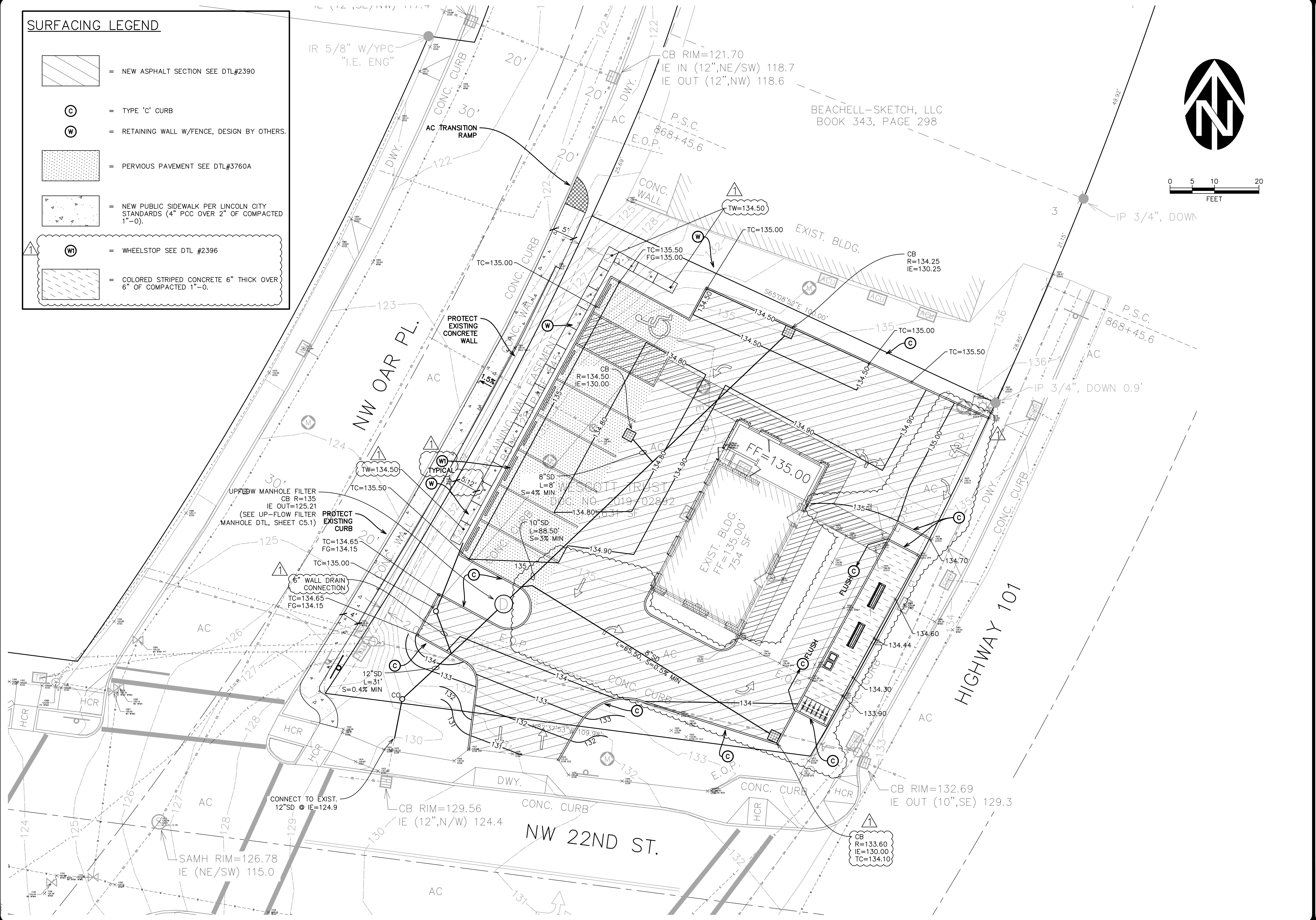
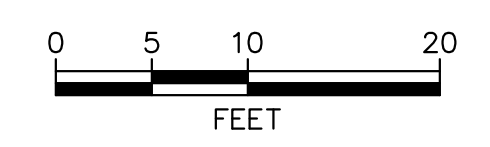
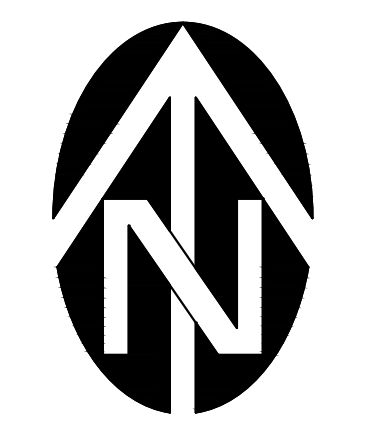
EROSION CONTROL DETAILS

DRAWING
C1.2

JOB NUMBER
3194.0000.0

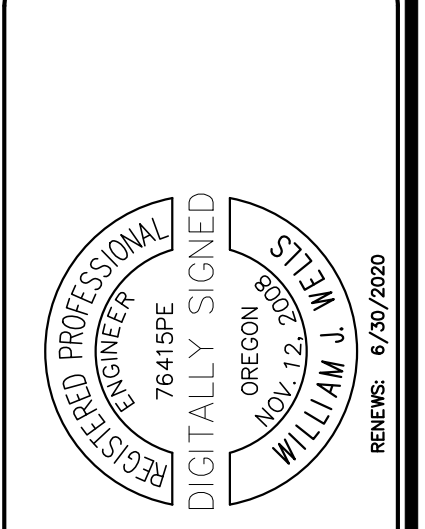
SURFACING LEGEND

-  = NEW ASPHALT SECTION SEE DTL#2390
-  = TYPE 'C' CURB
-  = RETAINING WALL W/FENCE, DESIGN BY OTHERS.
-  = PERVIOUS PAVEMENT SEE DTL#3760A
-  = NEW PUBLIC SIDEWALK PER LINCOLN CITY STANDARDS (4" PCC OVER 2" OF COMPACTED 1"-0).
-  = WHEELSTOP SEE DTL #2396
-  = COLORED STRIPED CONCRETE 6" THICK OVER 6" OF COMPACTED 1"-0.



NO.	DATE	PER CITY COMMENTS	DESCRIPTION	BY
1	6-3-2021			IH

VERIFY SCALE
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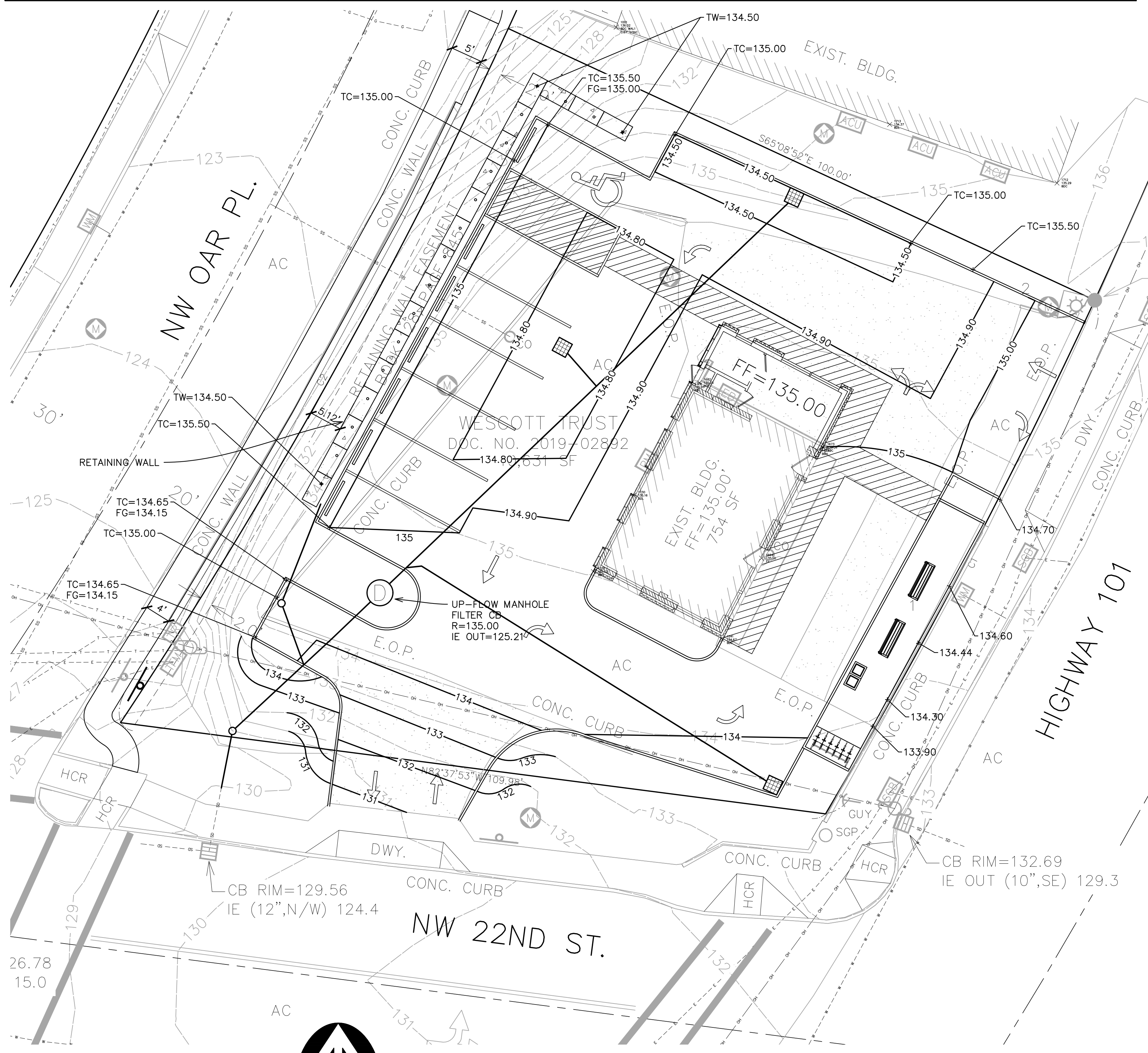
WESTTECH ENGINEERING, INC.
 CONSULTING ENGINEERS AND PLANNERS
 3841 Fairview Industrial Dr. S.E., Suite 100, Salem, OR 97302
 Phone: (503) 565-2474 Fax: (503) 565-3966
 E-mail: westtech@westtech-eng.com

EXCELLENCE INC.
GRADING, DRAINAGE, AND SURFACING PLAN
 DRAWING **C2.0**
 JOB NUMBER **3194.0000.0**

6/3/2021 2:09:14 PM
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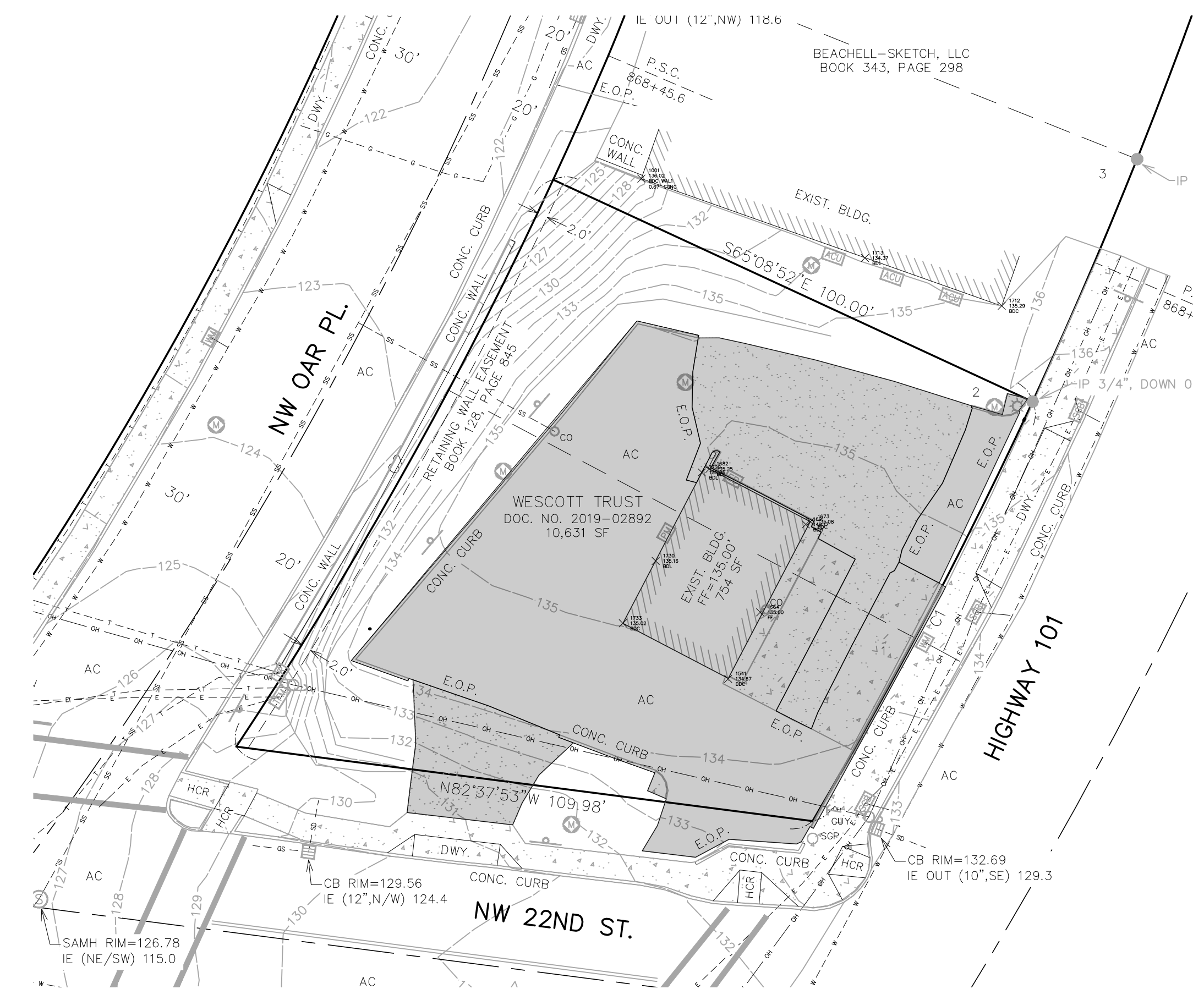
UP-FLOW WATER QUALITY FILTER SIZING

STORM EVENT	EXISTING PEAK Q (CFS)	POST-DEVELOPMENT PEAK Q (CFS)	EXISTING RUNOFF VOLUME (ACRE-FT)	POST-DEVELOPMENT RUNOFF VOLUME (ACRE-FT)	UP-FLOW TREATMENT CAPACITY (CFS)	NO. UP-FLOW FILTER MODULE
WQ (1/2 2 YR)	0.08	0.08	0.028	0.025	0.112	2
OVERFLOW 100 YR	0.32	0.29	0.107	0.097	-	-



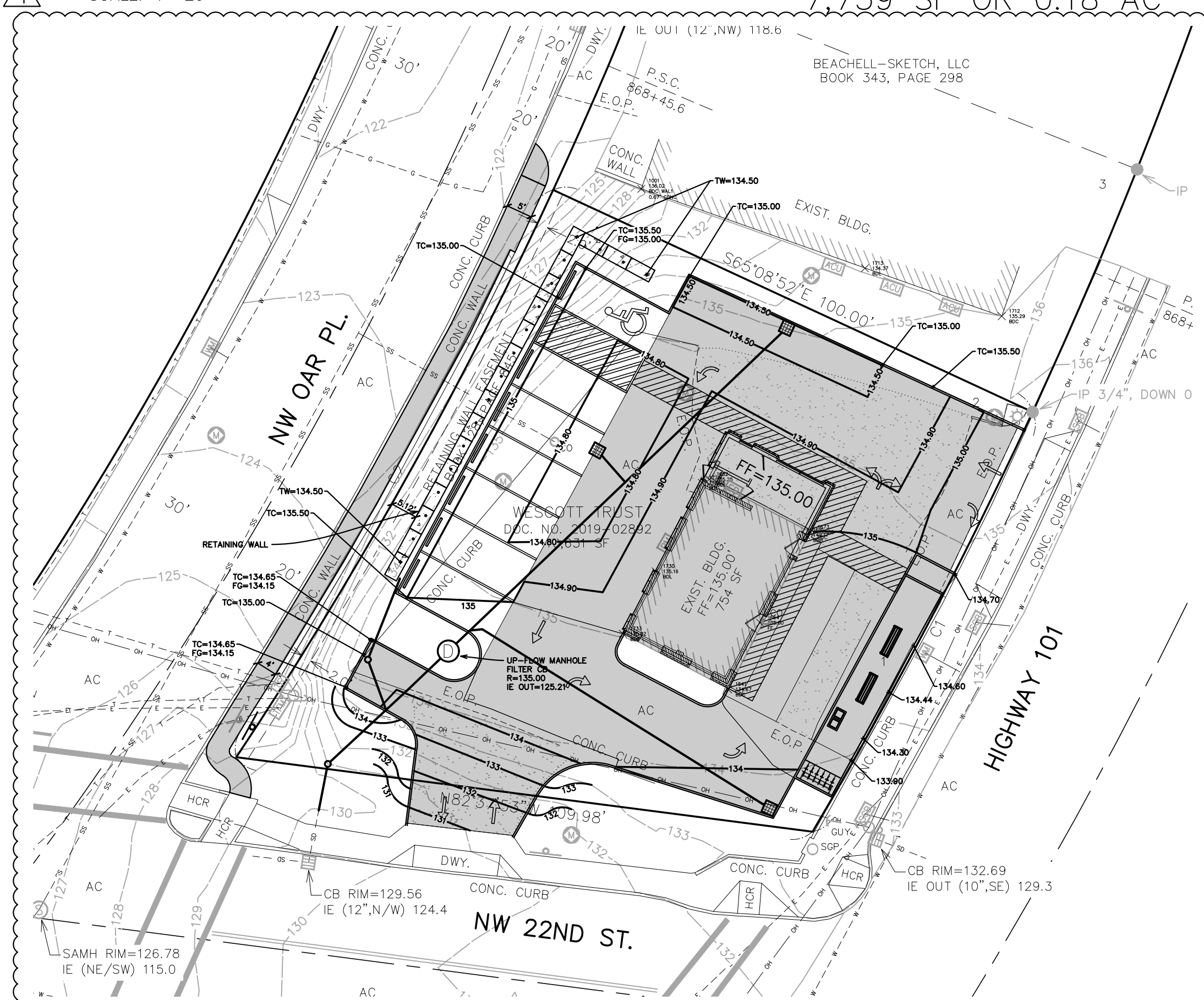
STORMWATER BASIN MAP
SCALE: 1"=10'

TOTAL AREA	=	11,644 SF
IMPERVIOUS	=	7,002 SF
PERVIOUS	=	4,642 SF



EXISTING IMPERVIOUS AREA
SCALE: 1"=20'

■ = IMPERVIOUS
7,739 SF OR 0.18 AC



PROPOSED IMPERVIOUS AREA
SCALE: 1"=20'

■ = IMPERVIOUS
7,448 SF OR 0.17 AC

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NO.	DATE	PER CITY COMMENTS	BY
1	6-3-2021		JH

VERIFY SCALE
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IF NOT ONE INCH ON SCALES ACCURACLY

REGISTERED PROFESSIONAL ENGINEER
76415PE
DIGITALLY SIGNED
WILLIAM J. STEINBOCK
OREGON
NOV. 12, 2010

WESTTECH ENGINEERING, INC.
CONSULTING ENGINEERS AND PLANNERS

WE

3841 Fairview Industrial Dr. S.E., Suite 100, Salem, OR 97302
Phone: (503) 585-2474 Fax: (503) 585-3966
E-mail: westtech@westtech-eng.com

EXISTING IMPERVIOUS AREA

DRAWING
C3.0

JOB NUMBER
3194.0000.0

GENERAL NOTES:

- Contractor shall procure, and conform to all construction permits required by Lincoln City.
- The following statement and wording is mandated by OAR 952-001-0020
ATTENTION: Oregon law requires you to follow rules adopted by the Oregon Utility Notification Center. Those rules are set forth in OAR 952-001-0010 through OAR 952-001-0090. You may obtain copies of the rules by calling the center. (Note: the telephone number for the Oregon Utility Notification Center is (503) 232-1987).
- Contractor to notify City, ODOT and all utility companies a minimum of 48 business hours (2 business days) prior to start of construction, and comply with all other notification requirements of agencies with jurisdiction over the work.
- Contractor shall procure a right-of-entry permit from ODOT State Highway Division for all work within the State right-of-way and conform to all conditions of the permit.
- A copy of final approved construction drawings and any required permits shall be kept on-site at all times, for review by inspectors upon request.
- Contractor shall provide all bonds and insurance required by public and/or private agencies having jurisdiction. Where required by public and/or private agencies having jurisdiction, the Contractor shall submit a suitable maintenance bond prior to final payment.
- All materials and workmanship for facilities in street right-of-way or easements shall conform to approving agencies' construction specifications wherein each has jurisdiction, including but not limited to the City, County, Oregon Health Division (OHD) and the Oregon Department of Environmental Quality (DEQ).
- Unless otherwise approved by the Public Works Director, construction of all public facilities shall be done between 7:00 a.m. and 6:00 p.m., Monday through Saturday.
- The Contractor shall perform all work necessary to complete the project in accordance with the approved construction drawings including such incidents as may be necessary to meet applicable agency requirements and provide a completed project.
- Any inspection by the City, County or other agencies shall not, in any way, relieve the Contractor from any obligation to perform the work in strict compliance with the contract documents, applicable codes, and agency requirements.
- Contractor shall maintain one complete set of approved drawings on the construction site at all times whereon he will record all approved deviations in construction from the approved drawings, as well as the station locations and depths of all existing utilities encountered. These field record drawings shall be kept up to date at all times and shall be available for inspection by the City or Owner's Representative upon request. Failure to conform to this requirement may result in delay in payment and/or final acceptance of the project.
- Upon completion of construction of all new facilities, Contractor shall submit a clean set of field record drawings containing all as-built information to the Engineer. All information shown on the Contractor's field record drawings shall be subject to verification. If significant errors or deviations are noted, an as-built survey prepared and stamped by a registered professional Land Surveyor shall be completed at the Contractor's expense.
- The contractor shall retain and pay for the services of a registered Civil Engineer and/or Land Surveyor licensed in the State of Oregon to establish construction control and perform initial construction surveys to establish the lines and grades of improvements as indicated on the drawings. Staking for buildings, structures, curbs, gravity drainage pipes/structures and other critical improvements shall be completed using equipment accurate to 0.04 feet horizontally and 0.02 feet vertically, or better. Use of GPS equipment for final construction staking of these critical improvements is prohibited. The registered professional surveyor shall provide the design engineer with copies of all grade sheets for construction staking performed for the project.

TRAFFIC CONTROL

- Contractor shall erect and maintain barricades, warning signs, traffic cones (and all other traffic control devices required) per City, County and ODOT requirements in accordance with the current MUTCD (including Oregon amendments). Access to driveways shall be maintained at all times. All traffic control measures shall be approved and in place prior to any construction activity. Prior to any work in the existing public right-of-way, Contractor shall submit final traffic control plan to the City, County and ODOT for review and issuance of a Lane Closure or Work in Right-of-Way Permit.

TESTING AND INSPECTION:

- The Contractor shall be responsible to ensure that all required or necessary inspections are completed by authorized inspectors prior to proceeding with subsequent work which covers or that is dependent on the work to be inspected. Failure to obtain necessary inspection(s) and approval(s) shall result in the Contractor being fully responsible for all problems and/or corrective measures arising from uninspected work.
- Unless otherwise specified, the attached "Required Testing and Frequency" table outlines the minimum testing schedule for the project. This testing schedule is not complete, and does not relieve the Contractor of the responsibility of obtaining all necessary inspections or observations for all work performed, regardless of who is responsible for payment. Cost for retesting shall be borne by the Contractor.

EXISTING UTILITIES & FACILITIES:

- The location and descriptions of existing utilities shown on the drawings are compiled from available records and/or field surveys. The Engineer or utility companies do not guarantee the accuracy or the completeness of such records. Contractor shall field verify locations and sizes of all existing utilities prior to construction.
- The Contractor shall retain a surveyor to research, locate and mark all existing property and street monuments within or adjacent to the work areas prior to construction. Any survey monuments that will be disturbed during construction of the project shall be referenced (prior to construction) and replaced (following construction) by a Registered Land Surveyor at the Contractor's expense. The monuments shall be replaced within a maximum of 90 days, and the County Surveyor shall be notified in writing and/or a survey document recorded as required by ORS 209.140, ORS 209.150 and/or ORS 209.155 as applicable.
- Contractor shall field verify location and depth of all existing utilities where new facilities cross. All utility crossings marked or shown on the drawings shall be potholed using hand tools or other non-invasive methods prior to excavating or boring. Contractor shall be responsible for exposing potential utility and other conflicts for enough ahead of construction to determine necessary grade, alignment or depth modifications without delaying the work or requiring otherwise unnecessary materials, fittings or structures. If grade, alignment or depth modification is necessary, Contractor shall notify the Design Engineer, and the Design Engineer or the Owner's Representative shall obtain approval from the City prior to construction.
- All facilities shall be maintained in-place by the Contractor unless otherwise shown or directed. Contractor shall take all precautions necessary to support, maintain, or otherwise protect existing utilities and other facilities at all times during construction. Contractor to leave existing facilities in an equal or better-than-original condition and to the satisfaction of the City and Owner's Representative.

- The Contractor shall be responsible for managing construction activities to ensure that public streets and right-of-ways are kept clean of mud, dust or debris. Dust abatement shall be maintained by adequate watering of the site by the Contractor.

GRADING, PAVING & DRAINAGE:

- Unless otherwise noted, all grading, rocking and paving to conform to OSSC (ODOT/APWA), 2018 edition.
- Clear and grub within work limits all surface vegetation, trees, stumps, brush, roots, etc. Do not damage or remove trees except as shown or noted on the drawings, or as approved by the Owner's Representative. Protect all roots two inches in diameter or larger on trees which are not to be removed.
- Strip work limits, removing all organic matter, which cannot be compacted into a stable mass. All trees, brush, and debris associated with clearing, stripping or grading shall be removed and disposed of off-site.
- Except as otherwise allowed by the specification, drawing details or notes, immediately following stripping and grading operations, compact subgrade to 95% of the maximum dry density per AASHTO T-180 test method (Modified Proctor). Subgrade must be inspected and approved by the Owner's authorized representative before placing embankments, engineered fills or fine grading for base rock.
- Engineered fills shall be constructed and compacted in 6" lifts over approved subgrade. All fills within public right-of-ways and easements shall be engineered, with each lift compacted to 95% of the maximum dry density per AASHTO T-180 test method (Modified Proctor).
- All fills outside of public right-of-ways which are within potential building envelopes shall be engineered and comply with the Oregon Structural Specialty Code, with each lift compacted to 90% of the maximum dry density per AASHTO T-180 test method (Modified Proctor). Fills outside of building envelopes which are over 12-inches in depth shall also be engineered and compacted.
- Granular baserock shall conform to the requirements of OSSC (ODOT/APWA) 02630.10 (Dense Graded Base Aggregate), with no more than 10% passing the #40 sieve and no more than 5% passing the #200 sieve.
- Compact granular baserock to 95% of the maximum dry density per AASHTO T-180 test method (Modified Proctor). Written baserock compaction test results from an independent testing laboratory must be received by the Owner's authorized representative before placing AC pavement.
- AC pavement shall conform to OSSC (ODOT/APWA) 00744 (hot mixed Asphalt Concrete Pavement -ACP) for standard duty mix. Unless otherwise specified or shown on the drawings, base lifts shall be 3/4" dense graded mix, while wearing courses shall be 1/2" dense graded mix. Unless otherwise specified or shown on the drawings, AC pavement for parking lots and streets shall be Level 2 mix (superpave 65 gyrations) per OSSC (ODOT/APWA) 00744.13.
- AC Pavement shall be compacted to a minimum of 91% of maximum density as determined by the Rice standard method. Written AC pavement compaction test results from an independent testing laboratory must be received by the Owner's authorized representative before final payment.
- Pavement surface shall be a smooth, well-sealed, tight mat without depressions or bair baths. Bony or open graded pavement surfaces shall be repaired to the satisfaction of the Owner's Representative, prior to final acceptance of the work.
- ACP mixtures shall be placed only when the surface is dry and weather conditions are such that proper handling, finishing and compaction can be accomplished. In no case shall bituminous mixtures be placed when the surface temperature is below the minimum established under 2015 OSSC (ODOT/APWA) 00744.40 (AC - Season and Temperature Limitations) or the project specifications, whichever is more stringent.
- Contractor shall protect new pavement against traffic as required, until it has cooled sufficiently to avoid tracking.
- Unless otherwise shown on the drawings or details, straight grades shall be run between all finish grade elevations and/or finish contour lines shown (exception: where grades shown cross sidewalks, slopes shall be adjusted to ensure that maximum allowable sidewalk cross slopes are not exceeded).
- Finish pavement grades at transition to existing pavement shall match existing pavement grades or be feathered past joints with existing pavement as required to provide a smooth, free draining surface.
- All existing or constructed manholes, cleanouts, monument boxes, gas valves, water valves and similar structures shall be adjusted to match finish grade of the pavement, sidewalk, landscaped area or median strip wherein they lie. Verify that all valve boxes and risers are clean and centered over the operating nut.
- All street signs, traffic control signs, curb & pavement painting or striping, and/or reflectors shall be installed (in conformance with City and MUTCD standards) prior to requesting final inspection by the City.
- Pavement Markings. All stop bars, crosswalk striping and other roadway marking and emblems shall be pre-formed thermoplastic (Premark BD by Ennis-Flint). Installation shall be by methods and by a contractor approved by the City.
- Unless otherwise shown on the drawings, no cut or fill slopes shall be constructed steeper than 2H:1V.
- All planter areas, shall be backfilled with approved topsoil minimum 8" thick. Stripping materials shall not be used for planter backfill.
- Contractor shall seed and mulch (uniformly by hand or hydroseed) all exposed slopes and disturbed areas which are not scheduled to be landscaped, including trench restoration areas.

CURBS & SIDEWALKS:

- Unless otherwise shown or indicated on the drawings, 6-inches nominal curb exposure used for design of all parking lot and street grades.
- Construction of all curbs & sidewalks shall conform to the applicable requirements of OSSC (ODOT/APWA) Section 00759, Miscellaneous Portland Cement Concrete Structures, including placement, curing, finishing and the repairing of minor defects. Major defects (as determined by the Owner's Representative) will require removal and replacement of the defective portions as directed.
- Where new curbing connects to existing curbing or is installed along existing streets or pavement, the gutter grade shall match the existing street grades so as to allow drainage from the street to the gutter, as well as through any transitions or connections between old & new curbs. The Contractor shall notify the Owner's Representative in writing of any grade discrepancies or problems prior to curb placement.
- Unless otherwise approved in writing by the Owner's Representative and the agency with jurisdiction, monolithic curb & sidewalk shall not be placed in the public right-of-way (ie. curb & sidewalk concrete shall be placed separately).
- Curb & sidewalk concrete shall be placed only during periods when it will not be damaged by rain (protect unhardened concrete from precipitation). Concrete shall not be placed on frozen baserock. Do not begin concrete placement until temperature in the shade is a minimum of 35F and rising, and stop placement if air temperature falls below 35F. Protect concrete from freezing for a minimum of

5 days after placement per OSSC (ODOT/APWA) 0000440.40.d & 00756.40 or the project specifications, whichever is more stringent.

48. Contractions joints shall be installed directly over any pipes that cross under the sidewalk, to control cracking. In general, cracks in new curbs or sidewalks (at locations other than contraction joints) are not acceptable, and cracked panels shall be removed & replaced unless otherwise approved by the City and the design engineer.
49. Where trench excavation requires removal of PCC curbs and/or sidewalks, the curbs and/or sidewalks shall be sawcut and removed at a joint, unless otherwise authorized in writing by the City. The sawcut lines shown on the drawings are schematic and not intended to show the exact alignment of such cuts.
50. Unless otherwise shown on the drawings, areas along curbs and sidewalks shall be backfilled with approved topsoil (typical 6" thick), as well as being seeded and mulched (or hydroseeded).

PIPED UTILITIES:

51. All tapping of existing municipal sanitary sewer, storm drain mains, and manholes must be done by Contractor forces.
52. The Contractor shall have appropriate equipment on site to produce a firm, smooth, undisturbed subgrade at the trench bottom, true to grade. The bottom of the trench excavation shall be smooth, free of loose materials or tooth grooves for the entire width of the trench prior to placing the granular bedding material.
53. All pipes shall be bedded with minimum 6-inches of 3/4"-0 crushed rock bedding and backfilled with compacted 3/4"-0 crushed rock in the pipe zone (crushed rock shall extend a minimum of 12-inches over the top of the pipe in all cases). Unless CLSM, CDF or other backfill is shown or noted on the drawings, crushed rock trench backfill shall be used unless all improved areas, including pavement, sidewalks, foundation slabs, buildings, etc.
54. Granular trench bedding and backfill shall conform to the requirements of OSSC (ODOT/APWA) 02630.10 (Dense Graded Base Aggregate), 3/4"-0. Unless otherwise shown on the drawings, compact granular backfill to 92% of the maximum dry density per AASHTO T-180 test method (Modified Proctor).
55. If trenches are over-excavated for any reason, over-excavation shall be filled to the design trench subgrade (ie. to the bottom of the 6" thick pipe bedding layer) with compacted, well-graded granular backfill as specified (the use of open graded rock for trench foundation stabilization is prohibited unless it is completely encapsulated in geotextile fabric & approved in writing by the AHJ).
56. All water, sanitary and storm sewer piping shall have an electrically conductive insulated 12 gauge solid core copper tracer wire the full length of the installed pipe using blue wire for water and green wire for storm and sanitary piping. Tracer wire shall be extended up into all valve boxes, catch basins, manholes and lateral cleanout boxes. Tracer wire penetrations into manholes shall be within 18 inches of the rim elevation and adjacent to manhole steps. The tracer wire shall be tied to the top manhole step or otherwise supported to allow retrieval from the outside of the manhole. All tracer wire splices shall be made with waterproof splices or waterproof/corrosion resistant wire nuts.
57. No trenches in sidewalks, roads, or driveways shall be left in an open condition overnight. All such trenches shall be closed before the end of each workday and normal traffic and pedestrian flows restored.
58. Before mandrel testing, TV inspection or final acceptance of gravity pipelines, all trench compaction shall be completed and all sewers and storm drains flushed & cleaned to remove all mud, debris & foreign material from the pipelines, manholes and/or catch basins.

STORM DRAIN SYSTEM:

59. Storm sewer pipe materials shall conform to the construction drawings and City requirements. Unless otherwise noted or shown on the drawings, storm sewer pipe materials with watertight joints shall conform to the attached "Storm Pipe Table". Contractor shall use uniform pipe material on each pipe run between structures unless otherwise directed or approved. Jointed HDPE pipe shall not be used for slopes exceeding ten percent (10%). All materials and workmanship for all private storm drains, including storm drains located within any building envelope, shall be installed in conformance with Oregon Plumbing Specialty Code requirements.
60. Contractor shall designate the pipe material actually installed on the field record drawings and provide this information for inclusion on the as-built drawings.
61. Catch basins and junction boxes shall be set square with buildings or with the edge of the parking lot or street wherein they lie. Storm drain inlet structures and paving shall be adjusted so water flows into the structure without ponding water.
62. Unless otherwise approved by the Engineer, all storm drain connections shall be by manufactured tees or saddles.
63. Unless otherwise shown on the drawings, all storm pipe inlets & outfalls shall be beveled flush to match the slope wherein they lie.
64. Sweep (deflect) storm sewer pipe into catch basins and manholes as required. Maximum joint deflection shall not exceed 5 degrees or manufacturers recommendations, whichever is less.
65. Unless otherwise shown or directed, install storm sewer pipe in accordance with manufacturer installation guidelines.
66. Storm Cleaning. After manhole channeling and prior to mandrel testing or final acceptance, flush and clean all sewers, and remove all foreign material from the mainlines, manholes and catch basins.
67. Mandrel Testing. Contractor shall conduct deflection test of flexible storm sewer pipes by pulling an approved mandrel through the completed pipeline following trench compaction. The diameter of the mandrel shall be 95% of the initial pipe diameter. Test shall be conducted not more than 30 days after the trench backfilling and compaction has been completed.
68. TV Inspection. Upon completion of all storm sewer construction, testing and repair, the Contractor shall conduct a color TV acceptance inspection of all mainlines in accordance with OSSC (ODOT/APWA) 445.74 to determine compliance with grade requirements of OSSC (ODOT/APWA) 445.40.b (no deviation greater than 1/32-inch per inch of pipe diameter [1/2-inch max for pipes >16-inch diameter], & no reverse sloping pipe inverts) and to verify pipelines are adequately cleaned. The TV inspection shall be conducted by an approved technical service which is equipped to make audio-visual recordings of the TV inspections on DVD. Unless otherwise required by the agency with jurisdiction, a standard 1-inch diameter ball shall be suspended in front of the camera during the inspection to determine the depth of any standing water. Sufficient water to reveal low areas or reverse grades shall be discharged into the pipe immediately prior to initiation of the TV inspection. The DVD and written report shall be delivered to the City.
69. Prior to acceptance, the Owner's Representative may lamp storm lines upstream & downstream of structures to verify that the pipes are clean and there is no grout or concrete in the mainlines, and that there are no observable bellies in the line. When necessary, sufficient water to reveal low areas shall be discharged into the pipe by the Contractor prior to any such inspection by the Owner's Representative or the City.

STORM PIPE TABLE	
Cover Depth	6" - 18" Diameter
Less than 2' Cover	Class 50 ductile iron pipe with bell and spigot joints and rubber gasket.
2' to 2-1/2' Cover	Pipe specified for lesser cover depths -or- Class 3, ASTM C-14 non-reinforced concrete pipe with bell and spigot joints & rubber gaskets, ASTM 150 Type II cement. -or- PVC pipe conforming to AWWA C900 DR 18 (6"-12") or AWWA C-905 (14"-18") with bell and spigot joints and rubber gasket
2-1/2' to 15' Cover	Pipe specified for lesser cover depths -or- PVC pipe conforming to ASTM D-3034 PVC SDR 35 (6"-15") or ASTM F-679 PVC solid wall SDR 35 (18") with bell and spigot joints and rubber gasket. -or- HDPE (high density polyethylene) pipe conforming to AASHTO M-252, (8"-10") or AASHTO M-294 (12"-18"). For slopes less than 6% the pipe shall be ADS N-12 IB ST, Hancor Sure-Lok F477, or approved equal. For slopes greater than 6% the pipe shall be ADS N-12 IB WT, Hancor Blue Seal, or approved equal with watertight pressure testable fittings, -except- jointed HDPE (high density polyethylene) pipe referenced above not permitted for depth to invert greater than 12 feet.
More than 15' Cover	See construction drawings.

REQUIRED TESTING AND FREQUENCY TABLE		Party Responsible for payment	
		Contractor	Others (see note 1)
Streets, Fire Lanes, Common Driveways, Parking Lots, Pads, Fills, etc.			
Subgrade	1 Test/4000 S.F./Lift (4 min), locations acceptable to approving agency (typically alternate sides of road or access aisles)	✓	See note 2 & note 3
Engineered Fills	1 Test/4000 S.F./Lift (4 min), locations acceptable to approving agency	✓	See note 2 & note 5
Baserock	1 Test/4000 S.F./Lift (4 min), locations acceptable to approving agency (typically alternate sides of road or access aisles)	✓	See note 2 & note 3
Asphalt	1 Test/6000 S.F./Lift (4 min), locations acceptable to AA (typ. alternate as above)	✓	See note 2
Piped Utilities, All			
Trench Backfill	1 Test/200 Foot Trench/Lift (4 min)	✓	See note 2
Trench AC Restoration	1 Test/300 Foot Trench (4 min)	✓	See note 2
Storm			
Mandrel	95% of actual inside diameter	✓	See note 4
TV Inspection	All. Lines must be cleaned prior to TV work	✓	
Concrete, Block, etc.			
	Slump, Air & Cylinders for structural & reinforced concrete, equipment slabs, curbs, sidewalks & PCC pavements. Unless otherwise specified, one set of cylinders per 100 cubic yards (or portion thereof) of each class of concrete placed per day. Slump & air tests required on same load as cylinders.	✓	See note 2
Note 1: "Others" refers to Owner's authorized Representative or Approving Agency as applicable. Contractor responsible for scheduling testing. All testing must be completed prior to performing subsequent work.			
Note 2: Testing must be performed by an approved independent testing laboratory.			
Note 3: In addition to in-place density testing, the subgrade and base rock shall be proof-rolled with a loaded 10 yard dump truck provided by the Contractor. Baserock proofroll shall take place immediately prior to (within 24 hours of) paving, and shall be witnessed by the Owner's authorized Representative or approving agency. Location and pattern of testing and proofroll to be as approved or directed by said Owner's authorized Representative or approving agency.			
Note 4: To be witnessed by the Owner's Representative or approving agency. The Contractor shall perform pretests prior to scheduling witnessed waterline or sanitary sewer pressure tests, or pipeline mandrel test.			
Note 5: The approved independent laboratory retained by the Contractor shall provide a certification (stamped by an engineer licensed in the State of Oregon) that the subgrade was prepared and all engineered fills were placed in accordance with the provisions of the construction drawings and the contract documents.			

NO.	DATE	DESCRIPTION	BY
1	JUN 2019		

VERIF SCALE
 THIS IS ONE INCH ON ORIGINAL DRAWING
 IF NOT ONE INCH ON SCALE, ACCURACLY

DSN: JW
 DRN: JH
 CKD: JW

DATE: JUNE 2019

REGISTERED PROFESSIONAL ENGINEER
 76415P
 DIGITALLY SIGNED
 WILLIAM J. STEVENS
 OREGON
 MY LIC. # 12-8003
 6/20/2020

WESTTECH ENGINEERING, INC.
 CONSULTING ENGINEERS AND PLANNERS

WE

3841 Fairview Industrial Dr. S.E., Suite 100, Salem, OR 97302
 Phone: (503) 585-2474 Fax: (503) 585-3966
 E-mail: westtech@westtech-eng.com

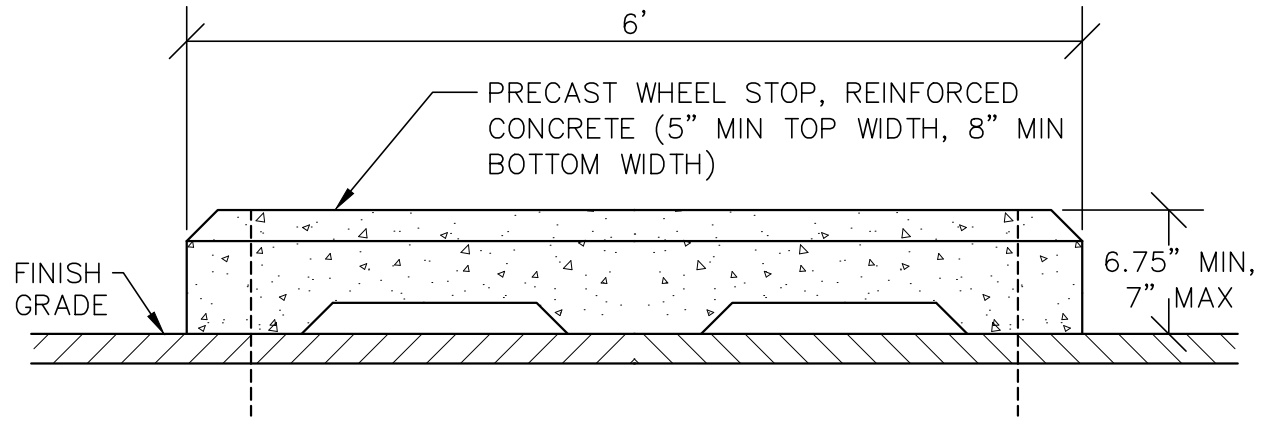
EXCELLENCE INC.

CONSTRUCTION NOTES &
 TABLE

DRAWING
 C4.0

JOB NUMBER
 3194.0000.0

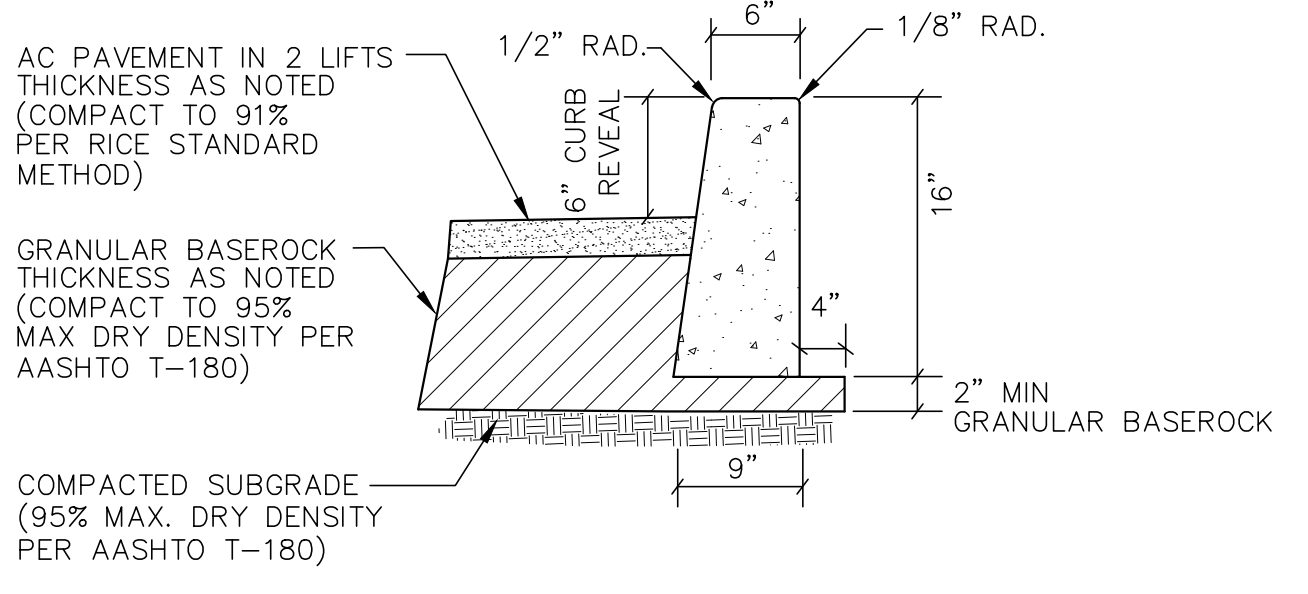
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SECTION
NTS

- NOTES:
- SEE DRAWINGS FOR LOCATION & NUMBER OF WHEEL STOPS, INCLUDING DIMENSION FROM CURB, EDGE OF PAVEMENT OR BUILDING AS APPLICABLE.
 - UNLESS OTHERWISE SPECIFIED OR SHOWN ON SITE PLAN, SET WHEEL STOPS 2 FEET FROM FACE OF CURB OR EDGE OF PAVEMENT, MEASURED FROM THE FACE OF THE WHEEL STOP (VEHICLE SIDE) TO FACE OF CURB (OR EDGE OF PAVEMENT). SET BACK FROM PROPERTY LINES PER CITY STANDARDS (3' MIN). MIN SETBACK FROM BUILDINGS AS SHOWN ON DWGS.
 - FOR USE ON HEAD-IN PARKING WITHOUT FULL HEIGHT CURBS, OR WHERE A SIDEWALK ALONG HEAD-IN PARKING IS LESS THAN 6 FEET WIDE.

LAST REVISION DATE:	JOB #
FEB 2013	STANDARD
PRECAST WHEELSTOP DETAIL	
(NTS)	
WESTECH ENG.	DETAIL NO. 2396

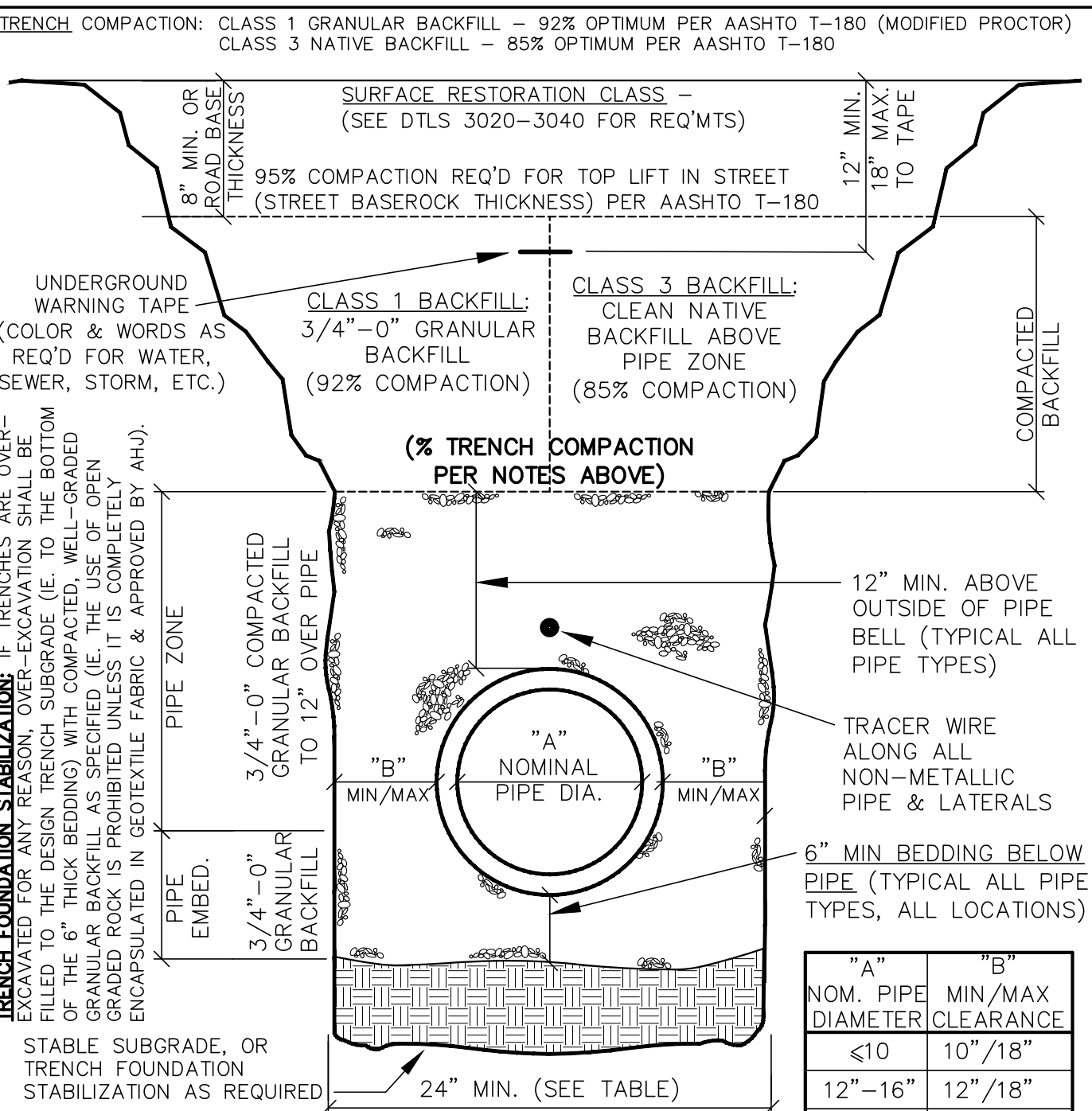


- PAVEMENT & BASEROCK SECTIONS:
- HEAVY DUTY PAVEMENT**
3" AC PAVEMENT IN 2 LIFTS (1" CLASS 'C' OVER 2" CLASS 'B')
12" OF 1"-0 GRANULAR BASEROCK

PAVEMENT/TYPE "C" CURB DETAIL
NTS

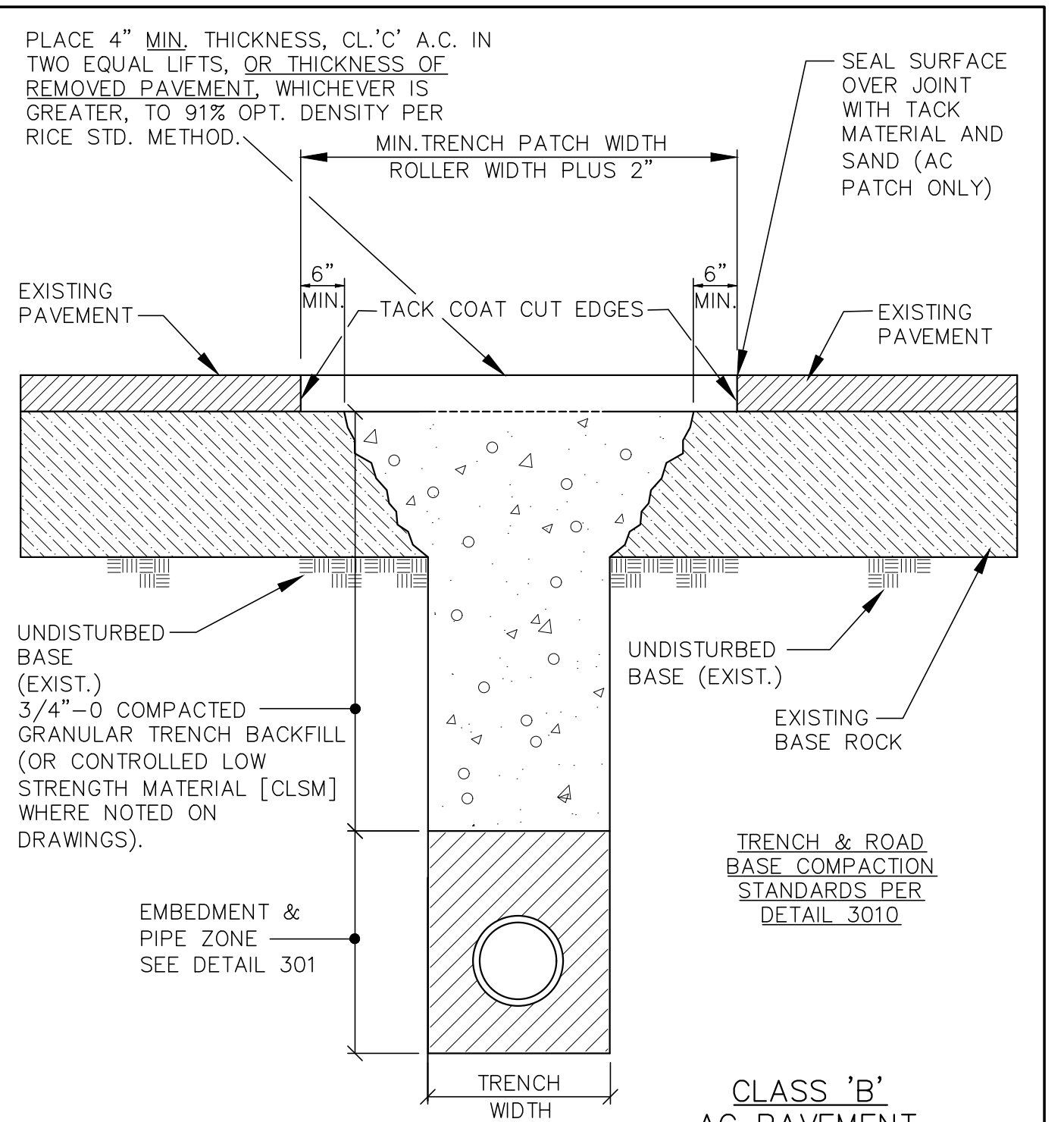
- NOTES:
- SEE GRADING PLAN FOR LOCATION OF LIGHT AND HEAVY DUTY PAVEMENT.
 - DESIGN SUBGRADES SHALL BE COMPACTED AND PROOF-ROLLED PRIOR TO PLACEMENT OF BASEROCK. IF SUBGRADE PASSES PROOF-ROLL BUT FAILS DENSITY TESTING, MIN. 4.5 OZ NON-WOVEN GEOTEXTILE FABRIC SHALL BE PLACED ON SUBGRADE PRIOR TO PLACEMENT OF BASEROCK. FAILURE OF PROOF-ROLL WILL REQUIRE OVEREXCAVATION.
 - IF SUBGRADE FAILS THE PROOF-ROLL, SUBGRADE SHALL BE OVEREXCAVATED TO UNDISTURBED SOIL AND BACKFILLED WITH BASEROCK OVER MIN. 8.0-OZ. NON-WOVEN FABRIC AS REQUIRED TO ALLOW COMPACTION OF UPPER (DESIGN) BASEROCK SECTION AND TO MAINTAIN STRUCTURAL INTEGRITY OF NATIVE SUBGRADE SOILS. TYPICAL MIN. OVEREXCAVATION REQUIRED IS 12-INCHES. NO RUBBER Tired EQUIPMENT ALLOWED ON SUBGRADE FOLLOWING OVEREXCAVATION.
 - SUBGRADE TO BE PROOFROLLED IMMEDIATELY PRIOR TO PLACING BASEROCK. BASEROCK TO BE PROOFROLLED IMMEDIATELY PRIOR TO PAVING.
 - CONTRACTION JOINTS SHALL BE PLACED AT 15' MIN. INTERVALS AND SHALL EXTEND AT LEAST 50% THROUGH THE CURB SECTION.
 - ALL CONCRETE SHALL BE 3300 PSI @ 28 DAYS.
 - CURBS TO CURE A MINIMUM OF 7 DAYS PRIOR TO PLACING FINAL BASEROCK AND PAVING. USE TYPE 1 OR 1-D CLEAR CURING COMPOUND.

LAST REVISION DATE:	JOB #
DEC 1999	X
PAVEMENT AND TYPE 'C' CURB DETAIL	
(NTS)	
WESTECH ENG.	DETAIL NO. 2390



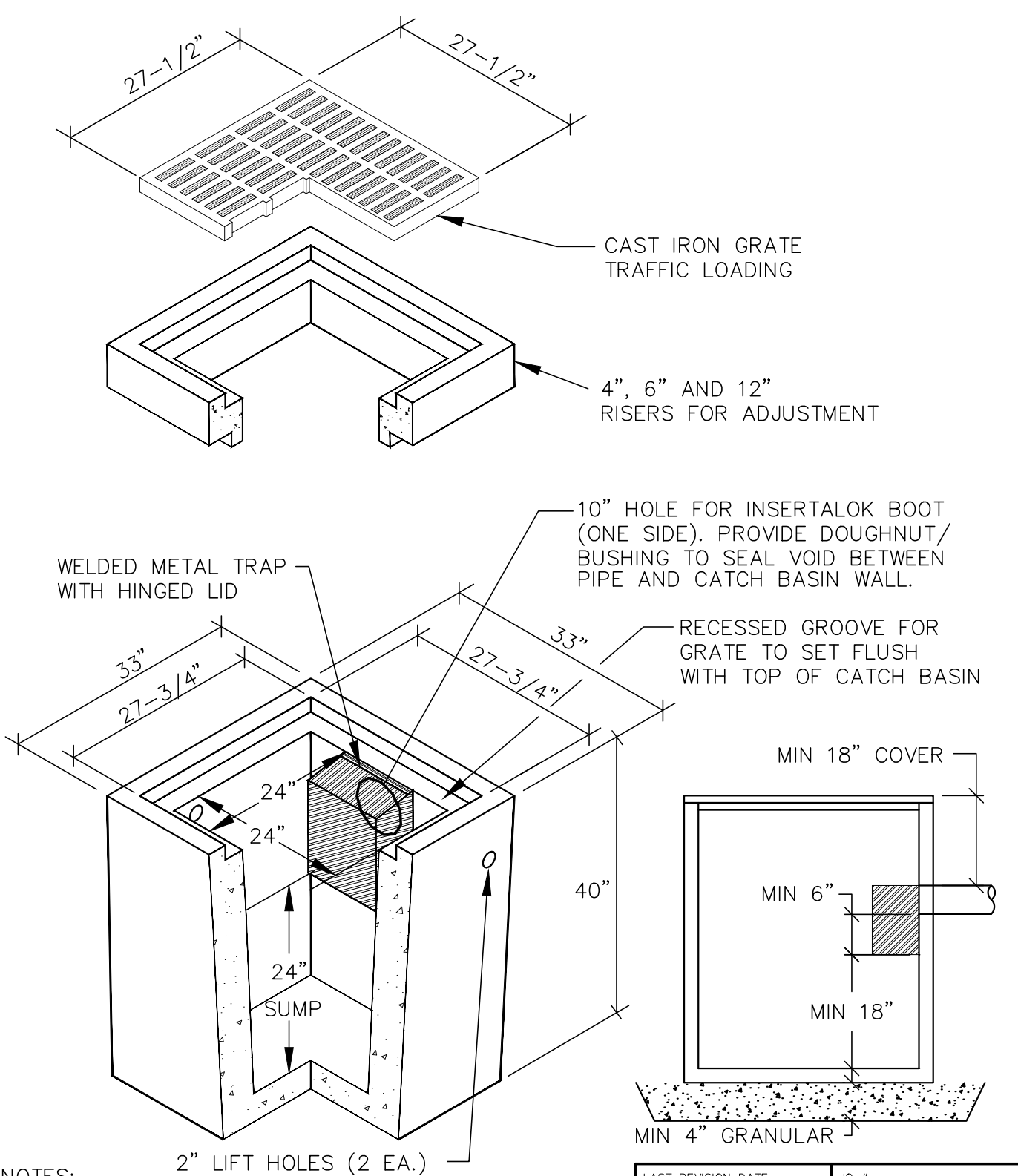
- NOTES:
- CLASS 1 REQ'D. UNDER ALL EXIST. OR FUTURE IMPROVED AREAS INCLUDING SIDEWALKS.
 - WHERE NEW PIPING IS IN SAME ALIGNMENT AS EXISTING PIPING, THE PIPE EMBEDMENT SHALL EXTEND TO A MIN. OF 6" BELOW THE NEW PIPING OR 6" BELOW EXISTING PIPING, WHICHEVER IS DEEPER.
 - FOR FLEXIBLE PIPE, BOTTOM OF TRENCH SHORING SHALL BE ABOVE PIPE SPRINGLINE PRIOR TO COMPACTING BACKFILL BELOW THE PIPE SPRINGLINE AND UNDER THE PIPE HAUNCHES.
 - MINIMUM CLEARANCES SHOWN ("B") ASSUMES STANDARD 6" WALL TRENCH BOXES SET ON TRENCH BOTTOM, AND REPRESENTS WIDTH REQUIRED TO CONSOLIDATE GRANULAR MATERIAL UNDER PIPE HAUNCHES (TO AVOID LOSS OF SIDE SUPPORT WHEN TRENCH BOX IS MOVED OR PULLED FORWARD). TRENCH WIDTH REDUCTION REQUIRES PRIOR APPROVAL BASED ON ACTUAL TRENCH SHORING PROPOSED.

LAST REVISION DATE:	JOB #
DEC 2018	STANDARD
TRENCH BACKFILL, BEDDING, AND PIPE ZONE	
(NTS)	
WESTECH ENG.	DETAIL NO. 3010



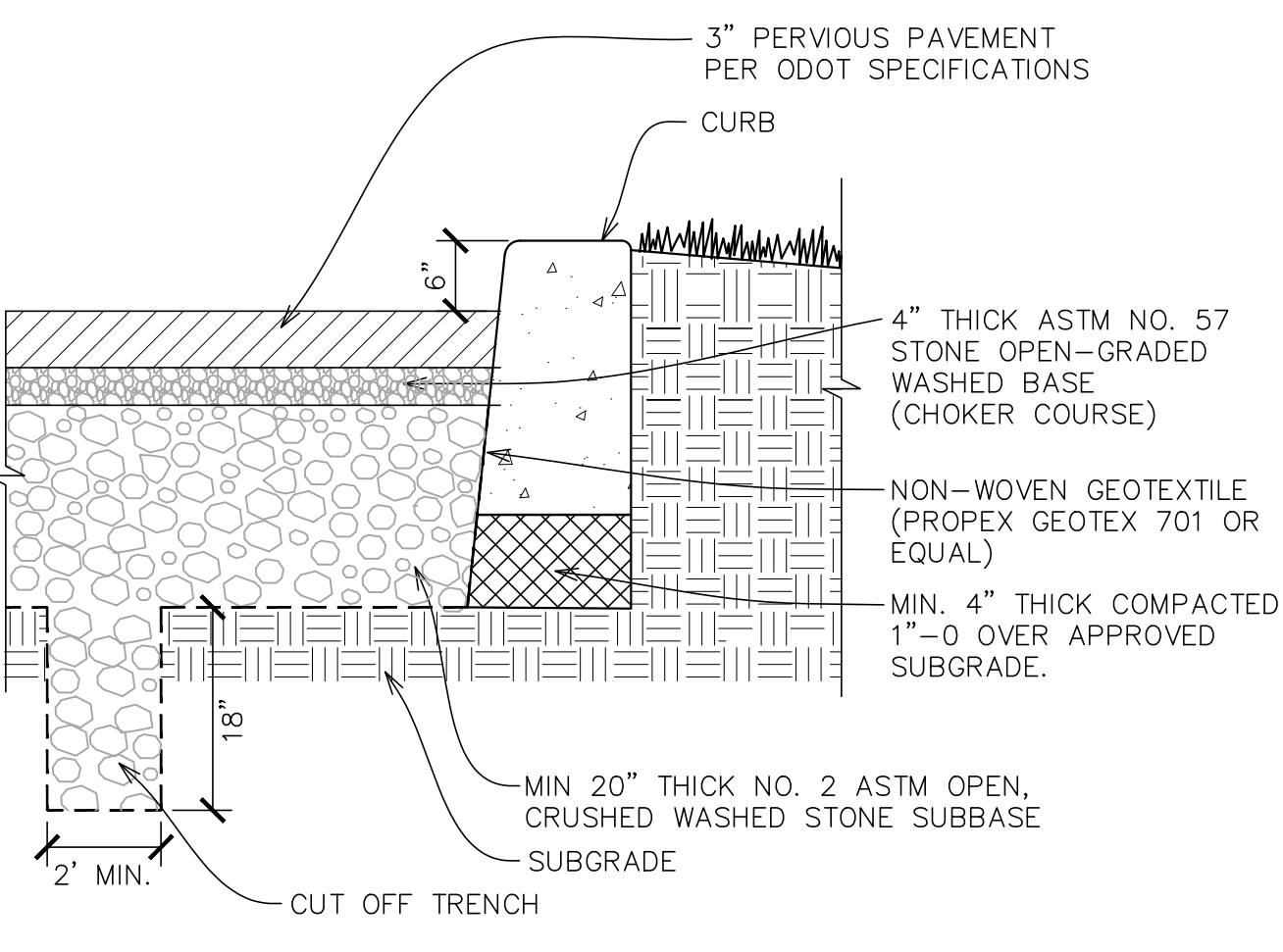
- NOTES:
- ALL EXISTING AC OR PCC PAVEMENT SHALL BE SAWCUT PRIOR TO REPAVING.
 - PCC CONCRETE PAVEMENT SHALL BE REPLACED WITH 3300 PSI PCC TO A MINIMUM THICKNESS OF 6" OR TO THE THICKNESS OF REMOVED CONCRETE, WHICHEVER IS GREATER.
 - FOR PAVED DRIVEWAYS (EXCEPT COMMERCIAL OR INDUSTRIAL) WITH LESS THAN 4" EXISTING AC, PAVEMENT THICKNESS MAY BE REDUCED TO 3" EACH SIDE.

LAST REVISION DATE:	JOB #
DEC 2015	STANDARD
STREET CUT SURFACE RESTORATION	
(NTS)	
WESTECH ENG.	DETAIL NO. 3020



- NOTES:
- SEE CONSTRUCTION DRAWINGS FOR PIPE SIZE, LOCATION AND INVERT ELEVATION.
 - CONCRETE SHALL BE 4000 PSI @ 28 DAYS.
 - REBAR SHALL CONFORM TO ASTM A615 GRADE 60.
 - REBAR SHALL BE MIN. #4 BARS @ 6" C.C.
 - SET CB SQUARE WITH BUILDINGS OR WITH EDGE OF PARKING LOT OR DRIVEWAY WHEREIN IT LIES.
 - ADJUST PAVING SO WATER FLOWS TO CB WITH NO PONDING.

LAST REVISION DATE:	JOB #
NOV 2018	STANDARD
PARKING LOT CATCH BASIN (PRECAST CONCRETE)	
(NTS)	
WESTECH ENG.	DETAIL NO. 3150



- NOTES:
- ALL ROCK SPECIFIED SHALL BE 100% FRACTURE FACE CRUSHED & WASHED.
 - DURING CONSTRUCTION OF PERMEABLE PAVEMENT SYSTEM, CONTRACTOR SHALL KEEP ALL SEDIMENT OUT OF BASE ROCK. ALL CONTAMINATED ROCK SHALL BE REMOVED AND REPLACED PRIOR TO INSTALLING THE NEXT LAYER.
 - CONTRACTOR SHALL MAINTAIN AND INSTALL EROSION CONTROL MEASURES AS NECESSARY TO KEEP SEDIMENT LADEN RUNOFF FROM ENTERING PERVIOUS PAVEMENT SYSTEM.
 - CONTRACTOR IS RESPONSIBLE FOR OBTAINING INSPECTION FROM OWNER'S REP. AFTER EACH LIFT. FAILURE TO DO SO COULD RESULT IN PRODUCT REJECTION.

LAST REVISION DATE:	JOB #
JUNE 2017	STANDARD
PERMEABLE PAVEMENT w/ FULL EXFILTRATION TO SOIL SUBGRADE	
(NTS)	
WESTECH ENG.	DETAIL NO. 3760A

NO.	DATE	PER CITY COMMENTS	DESCRIPTION	BY
1	6-3-2021			

VERIFY SCALE	1"
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IF NOT ONE INCH ON SCALES ACCURACLY	
DSN.	JW
DRN.	JH
CKD.	JW
DATE:	JUNE 2019

REGISTERED PROFESSIONAL ENGINEER
76415PE
DIGITALLY SIGNED
WILLIAM J. STEINBOCK
MAY 12, 2020 10:11 AM
REVISIONS: 6/20/2020

WESTECH ENGINEERING, INC.
CONSULTING ENGINEERS AND PLANNERS
3841 Fairview Industrial Dr. S.E., Suite 100, Salem, OR 97302
Phone: (503) 585-2474 Fax: (503) 585-3966
E-mail: westech@westech-eng.com

EXCELLENCE INC.

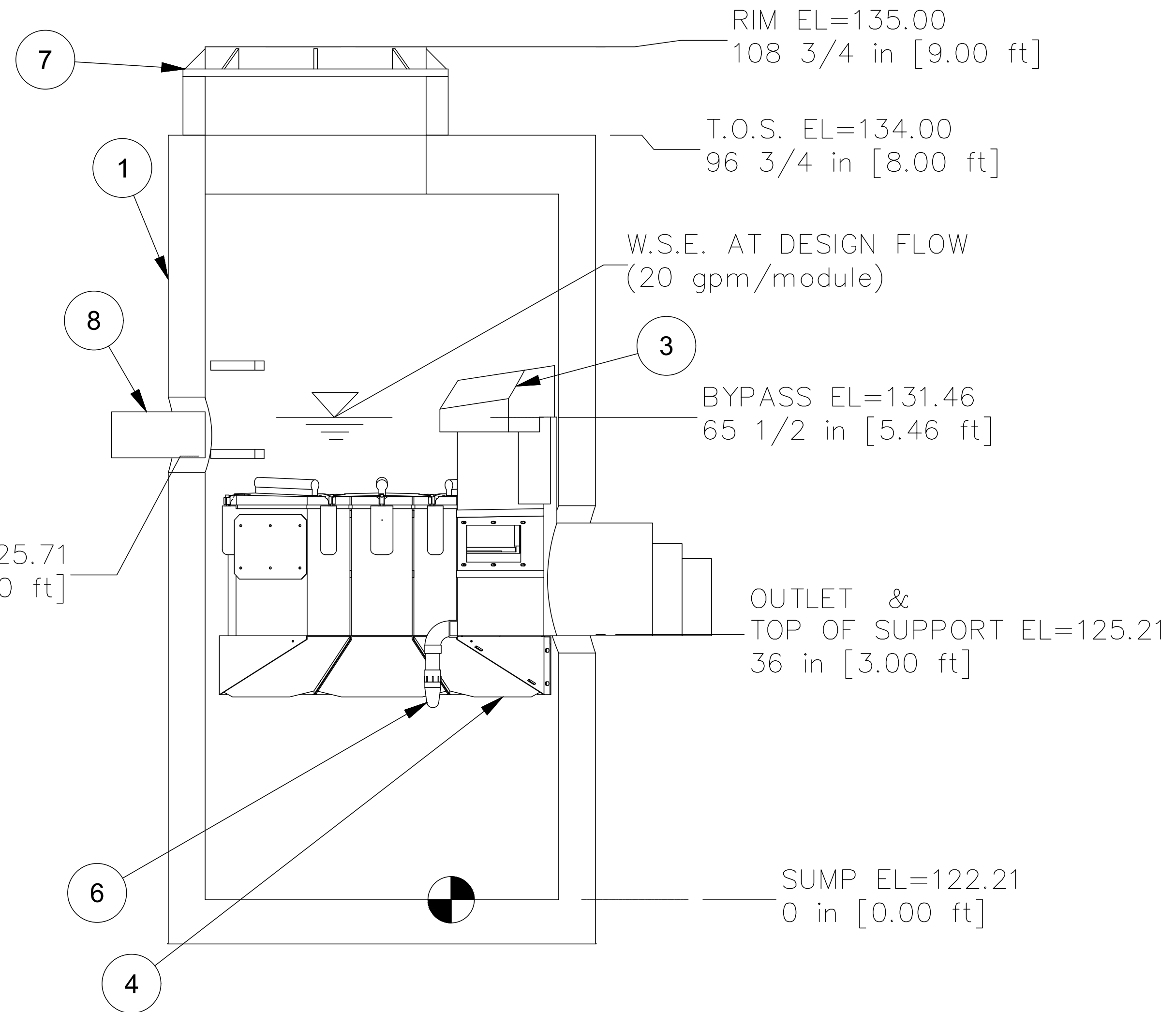
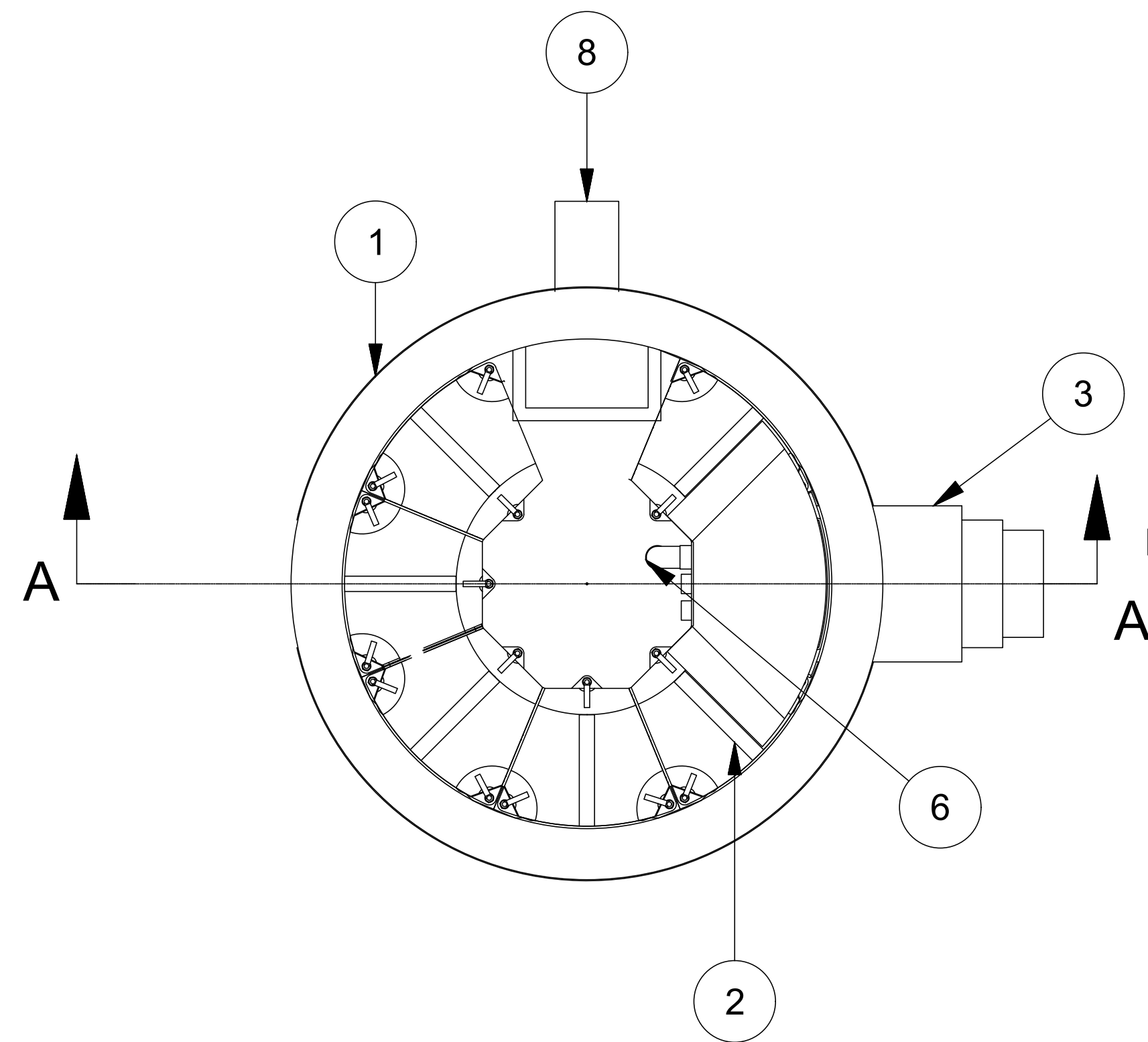
CONSTRUCTION DETAILS

DRAWING C5.0

JOB NUMBER 3194.0000.0

UP-FLO FILTER MANHOLE

BY HYDRO INTERNATIONAL®



SECTION A-A

EQUIPMENT PERFORMANCE

The stormwater filtration system shall adhere to the parameters listed below:
 Minimum Performance Criteria: 80% removal of Sil-Co-Sil 106 at the peak treatment flow.
 Peak Treatment Flow for 6 Filter Modules: 150 gpm (9.48 l/s), for 2 Filter Modules: 1.01 CFS
 Minimum Bypass Capacity: 1527 gpm (96.20 l/s)
 Minimum Sediment Storage Capacity: 16.6 cu. ft. (0.47 cu. m.)
 Minimum Oil Storage Capacity: 50 gal. (189 liters)
 Media Type: CPZ

OUTLET PIPE STUB SIZE INFORMATION

Outside Diameter: 10.5", 12.5" or 15.3" OD
 Concrete Penetration: Pipe Boot (by Hydro)
 Hook-up: Fernco-type coupling (by others)

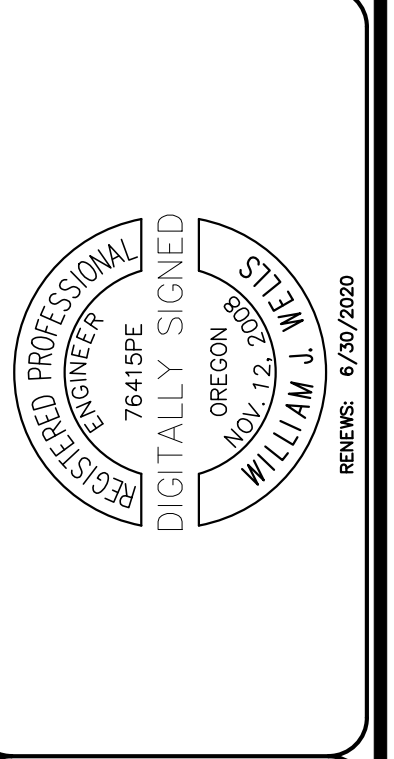
Parts List			
ITEM	QTY	SIZE	DESCRIPTION
1	1	48 in	CONCRETE MANHOLE
2	2		FILTER MODULE
3	1		OUTLET MODULE AND PIPE STUB
4	1		SINGLE SUPPORT BRACKET
5	1		OUTLET SUPPORT BRACKET
6	1		DRAINDOWN FILTER
7	1	30 in	FRAME AND COVER
8	1	10 in	INLET PIPE

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NO.	DATE	DESCRIPTION	BY
1			

VERIFY SCALE
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 IF NOT ONE INCH ON SCALES ACCURACLY

DSN. JW
 DRN. JH
 CKD. JW
 DATE: JUNE 2019



WESTTECH ENGINEERING, INC.
 CONSULTING ENGINEERS AND PLANNERS

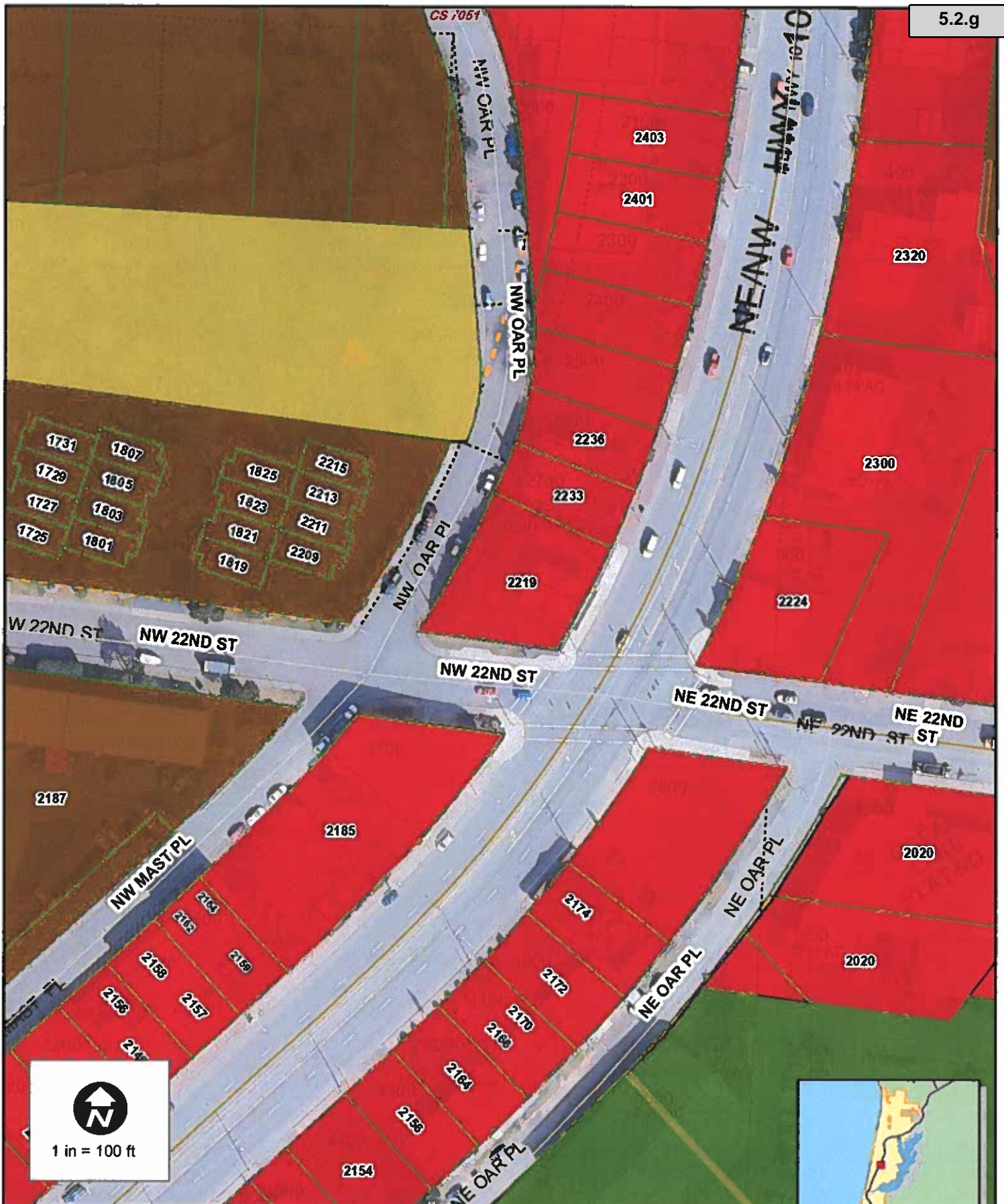
3841 Fairview Industrial Dr. S.E., Suite 100, Salem, OR 97302
 Phone: (503) 585-2474 Fax: (503) 585-3966
 E-mail: westtech@westtech-eng.com

EXCELLENCE INC.

CONSTRUCTION DETAILS

DRAWING
C5.1

JOB NUMBER
3194.0000.0



Vicinity Map

City of Lincoln City government use only. Use for any other purpose is entirely at the risk of the user. This product is for informational purposes and may not have been prepared for, or be suitable for legal, engineering, or surveying purposes. Users of this information should review or consult the primary data and information sources to ascertain the usability of the information.

Date: 10/13/2021



Location



Vicinity Map

City of Lincoln City government use only. Use for any other purpose is entirely at the risk of the user. This product is for informational purposes and may not have been prepared for, or be suitable for legal, engineering, or surveying purposes. Users of this information should review or consult the primary data and information sources to ascertain the usability of the information.

Date: 10/13/2021



Location

Planning Commission Communication

CUP 2021-01 Dock 3000 NE 26th Street

Meeting Date: October 19, 2021 Primary Staff Contact: AnneMarie Skinner
Department: Planning Commission E-Mail: ASkinner@lincolncity.org
Secondary Dept: Secondary Contacts:
Approval: Estimated Time:

Question:

Should the Planning Commission approve, approve with conditions, or deny the requested CUP 2021-01 conditional use permit for a new dock at 3000 NE 26th Street?

Staff Recommendation:

Staff recommends the Planning Commission approve with conditions CUP 2021-01, a request for a conditional use permit for a new dock at 3000 NE 26th Street.

Authority:

Lincoln City Municipal Code (LCMC) Chapter 17.76 Procedures
LCMC 17.77.060 Conditional Uses

Background:

See attached Staff Report

Financial Impact

None

Potential Motions:

1. Motion to approve CUP 2020-01 for a new dock at 3000 NE 26th Street with the conditions noted in the staff report; or
2. Motion to approve CUP 2020-01 for a new dock at 3000 NE 26th Street; or

3. Motion to deny CUP 2020-01 for a new dock at 3000 NE 26th Street (then list each criterion specifically not being met from Chapter 17.44 and/or Section 17.77.060)

See attached Staff Report

Attachments:

Staff Report CUP 2021-01 Dock (PDF)
3000 NE 26th Conditional Use Signed (PDF)
3000 NE 26th CU Dock Site Plan (PDF)
Dock cut sheet (PDF)
Project Narrative (PDF)
Zoning Map (PDF)
Natural Resources Map (PDF)

Staff Report

Planning Commission Hearing on October 19, 2021

Case File CUP 2021-01

Date: October 4, 2021

Case File: CUP 2021-01 New Dock

Applicant: Knottworks Construction
 Michael and Michelle Kammermeyer
 PO Box 197
 Otis, OR 97368
knottworks@gmail.com and laura.knottworks@gmail.com.

Property Owner: Michael Kammermeyer, Jr. and Michelle Kammermeyer
 3000 NE 26th Street,
 Lincoln City, OR 97367

Situs Address: 3000 NE 26th Street

Location: The subject property is located on the south side of NE 26th, approximately 84 feet east of NE West Devils Lake Road

Tax Map and Lot: 07-11-11-AC-00200

Comprehensive Plan Designation: Single-Family Residential District and Marine-Waterway (M-W)

Zoning District: Single-Family Residential District (R-1-7.5)

Site Size: 12,197 square feet or 0.28 acre

Proposal: Request for a conditional use permit for a new dock

Surrounding Land Uses and Zones: North: NE 26th Street, multi-unit housing complex; R-M
 South: canal waterway and undeveloped; R-1-5 and R-1-7.5
 East: vacant; R-1-7.5
 West: vacant; R-1-7.5



Public Notice: The Planning and Community Development Department mailed notice to the owners of all properties within 250 feet of the site on September 28, 2021. The *News Guard* published the public hearing notice on October 5, 2021.

Authority: Section 17.76.160 of the Lincoln City Municipal Code (LCMC) gives the Planning Commission the authority to approve, approve with conditions, disapprove, or revoke conditional use permits subject to the provisions of LCMC Chapter 17.77.

Applicable Substantive Criteria: LCMC Chapter 17.16 Single-Family Residential (R-1-7.5) Zone
LCMC Chapter 17.44 Marine Waterway (M-W) Zone
LCMC Section 17.77.060 Conditional Uses

BACKGROUND

The subject property (site) is addressed as 3000 NE 26th Street and contains a newly-constructed detached dwelling. The assessor's tax map shows that the eastern portion of the canal abutting the site is owned by Lincoln City, identified as Tax Map 07-11-11-AC-00399-00. The assessor's tax map shows that the western portion of the canal abutting the site is part of Tax Map 07-11-11-BD-00800, owned by Olivia L Holt Trustee. The southernmost 50-foot-wide strip of the site is mapped as a significant riparian area.

The site contains enough water on the site's southern end to accommodate a functional dock. The dock site plan included with the submitted application shows the proposed location of the dock as being completely self-contained on the site, with no encroachments into the adjacent canal that is owned by others. The southeastern corner of the proposed dock abuts the public portion of the canal, while not encroaching into it. This allows for the dock placement on private property (the site) with the ability to place kayaks into the portion of the canal that is for the public.

The location of the dock is within the site's significant riparian area. The applicant acknowledges that approval of a natural resources development review will be required prior to commencing construction of the dock. Once conditional use approval has been obtained for the dock, then an application will be submitted to Lincoln City for the required natural resources development review. Pursuant to LCMC 17.46.040.B.3.b (iv) a dock is allowed in a natural resource area provided all city code requirements are met.

ANALYSIS

The project must meet the applicable requirements of the M-W Zone (Chapter 17.44). A dock is allowed with conditional use approval, so the review process must also comply with Section 17.77.060, Conditional Uses. The specific findings made by the Planning Commission in granting a conditional use permit must be factual and supported by substantial evidence. The burden of producing substantial evidence to support the requisite findings is on the applicant seeking the approval of the conditional use.

COMMENTS

No comments were received during the 14-day comment period.

17.16.040

17.16.040, Conditional Uses, L. Docks, subject to the provisions of LCMC 17.44.040.

Finding: Section 17.16.040.L allows docks in the R-1 zones, subject to an approved conditional use permit application and compliance with the provisions of Section 17.44.040. This application is the official request for approval of the proposed dock conditional use. Compliance with Section 17.44.040 is addressed in this report.

17.44 MARINE WATERWAY (M-W) ZONE

17.44.040 Standards for conditional uses

In the M-W zone, the applicable provisions of this section shall apply. The standards for conditional uses shall be as follows:

A. Standards for Uses Involving Filling, Dredging, Drainage, Disposal of Dredging Spoils, and Similar Activities or Construction of Wharfs, Bulkheads or Similar Devices.

Finding: This application and request does not include filling, dredging, drainage, or disposal of dredging spoils; therefore, these standards are not applicable.

B. Standards for Uses Involving Construction, Addition or Reconstruction of a Pier, Dock, Boat House or Similar Facility.

Finding: According to the applicant, the owners of the site would like to construct a dock on their property. The proposed dock would be new construction. The site does not contain any existing piers, docks, boat houses, or similar facilities.

- 1. Evidence shall be provided that the applicant has complied with, or fully intends to comply with, all standards of the Department of Environmental Quality, the Division of State Lands, and all other agencies having interests or ordinances applicable to the property in question.*

Finding: The property owners have acknowledged, as part of this application submission, that all standards of the Department of Environmental Quality, the Division of State Lands, and all other agencies having interests shall be followed in the construction of the dock. After conditional use permit approval has been received, the Department of Environmental Quality and the Division of State Lands will be contacted for required permits, if any. Lincoln City's required natural resources development review will be completed after conditional use approval as well.

- 2. The facility or any use related to it shall not allow any water pollution to occur to any nearby tidelands, marshlands, rivers, streams or other waterways used for the raising, production or preservation of marine life or other natural resources.*

Finding: According to the applicant, the property owners are only constructing a dock. No activities are proposed that will involve any water pollution to occur. Dock construction and materials will adhere to all requirements to avoid water pollution. The property owners will place chairs on the dock for viewing enjoyment of the water and will use the dock to place kayaks in the water. These activities do not create any water pollution.

3. *The facility shall not substantially alter the course of any channel or the natural movement of any waters or result in increased flood hazards, or the formation of appreciable bottom or sludge deposits deleterious to marine life, and shall meet all of the following requirements:*

a. No dock, pier or similar facility shall extend into any watercourse more than 25 feet from ordinary low water line nor 50 feet from ordinary high water line, unless it can be shown that such extension is necessary and will not increase flood hazards or create other problems such as the deterioration or destruction of marine life or wildlife habitat as a result of the extension.

b. No dock, pier or similar facility shall extend into the navigable channel any distance greater than required for safe moorage and shall be designed so as to minimize potential flood hazard and loss of navigable waterway area.

c. No dock, pier or similar facility shall extend into any watercourse more than five percent of the width thereof as measured perpendicular from the mean low water line on one side of the watercourse to the mean low water line on the opposite side.

Finding: At 48 square feet, as described by the applicant, the dock has been designed to be as small as possible to minimize flood hazard and to avoid any altering of the course of the canal or its water movement. It is completely contained on the site.

According to the applicant, the dock will be completely contained on the site, and will not extend into the privately or publicly owned canal. The dimension of the dock from the southern edge of the site's water line to the site's southern property line is only 4 feet in depth.

The dock is completely contained on the site, and does not extend into the privately or publicly owned canal. There will be no loss of navigable waterway area due to placement of this dock. The dock has been designed to be as small as possible to minimize potential flood hazard, and the location of the dock is at a lower elevation than the house.

3. *No plumbing facilities for the handling of domestic or industrial waste shall be part of the facility unless approved by the city manager or design.*

Finding: The applicant proposes no plumbing facilities for the dock.

5. *Application for a permit for a pier, dock, bulkhead, boat house, or similar facility shall include:*

a. The source of the applicant's right to construct the facility;

b. The purpose of the facility;

c. The legal description of the area where the facility will be located;

d. A map and drawings, showing the plan for construction of the facility. Such plan shall include a vicinity map drawn to scale showing location and design of similar facilities and other development within 250 feet of the parcel upon which the improvement is proposed;

e. The time when the project is scheduled to begin and to be completed.

Findings: The site is in the R-1-7.5 zone, and a dock is allowed with a conditional use approval.

As stated by the applicant, the purpose of the proposed dock is to afford the owners of the site an area to sit directly at the water's edge and enjoy the view, as well as an area in which to place kayaks into the public portion of the canal and kayak to the east.

The legal description of the site is Parcel 1 of Partition Plat 2020-15.

The application materials included a cut sheet showing the construction of the dock. The submitted site plan shows the structures within 250 feet of the site as a dock and boathouse approximately 150 feet to the east of the site and a duplex to the west of the site.

Construction of the dock will commence upon approval of the conditional use application and the natural resources development review, but not sooner than July 1, 2022, and cease on September 15, 2022. If the project is not completed by September 15, 2022, then it will be completed between July 1, 2023 and September 15, 2023.

6. Plans for a pier, dock, boat house, or similar facility shall meet the following requirements:

- a. In new subdivisions considered by the planning commission following the adoption of the ordinance codified in this title, docks having less than 10 moorage spaces will be approved only in the instance that no other public or private means of launching or moorage is available or can be developed within 1,000 feet of the site in question.*
- b. Facilities being proposed in areas where it is likely that additional similar structures will be desired shall be designed to be combined into joint facilities whenever possible.*
- c. The design of moorages must provide sheer logs or similar devices for fending debris. Such improvements need not be maintained during periods where there is no danger of flood water.*
- d. Docks shall have the long dimension running parallel to the channel unless future development will result in pier construction or moorages being connected, necessitating facility design perpendicular to the channel. The width of those portions of such facilities shall be the minimum dimension required to provide safe access and moorage.*
- e. One dock shall not be closer to another dock than the length of the shorter structure or 25 feet, whichever distance is greater.*
- f. Additionally, docks allowed with conditional use approval in the GC, TVC, OS, P, RM, RC, and R1 zoning districts, must meet the following requirements and standards:*
 - i. Total area of dock on water must not exceed 144 square feet with a width not exceeding six feet and a length not exceed 24 feet.*
 - ii. No part of the dock can be covered or enclosed, such as but not limited to boat houses, sheds, fish cleaning stations, kayaks, canoes, hot tubs, and/or benches.*

- iii. *Docks must have at least 50 percent of the float surface composed of grating containing at least 60 percent open space surface.*
- iv. *The ramp/gangway of the dock must be 100 percent grated to allow light to pass through. Ramp width must not exceed five feet. The ramp square footage is not included in the total dock area square footage.*
- v. *Grated surfaces on the docks must not be used for storage (e.g., boats, benches, kayaks, fish cleaning stations, etc.) or other purposes that will reduce natural light penetration through the dock.*
- vi. *Treated wood is not allowed as a construction material for docks or ramps. Treated wood is lumber, pilings, and other wood products preserved with alkaline copper quaternary (ACQ), ammoniacal copper arsenate (ACA), ammoniacal copper zinc arsenate (ACZA), copper naphthenate, chromated copper arsenate (CCA), pentachlorophenol, or creosote.*
- vii. *Oregon law requires encapsulation of expanded polystyrene foam flotation used in state waters. Encapsulation methods and materials must be approved by the Oregon State Marine Board prior to installation of foam flotation.*
- viii. *All pilings must be fitted with devices to prevent perching by piscivorous birds.*
- ix. *All work must be completed in accordance with the Oregon Department of Fish and Wildlife Guidelines for Timing of In-Water Work to Protect Fish and Wildlife Resources.*
- x. *There must be minimal disturbance to any buried, submerged, or floating woody debris removal during construction.*
- xi. *For piling removal, dislodge the piling with a vibratory hammer, when possible, to avoid a pile break by twisting or bending. A floating surface boom may be necessary to capture floating surface debris.*

Finding: The applicant provided the following comments in response to each of the requirements as follows:

This site is not part of a new subdivision. It is not anticipated that either property to the east or west of the site will be constructing a dock. No moorages are proposed.

The proposed dock's length dimension of 14 feet runs parallel with the channel as required. This requirement is met. A review of the submitted site plan shows there are no docks within 25 feet of the proposed dock.

The dock site plan shows that the proposed dock will be 48 square feet with a length of 12 feet, and a width of 4 feet. The entirety of the dock will be uncovered. No enclosures are proposed.

As described by the applicant, the review of the submitted cut sheet shows that the entire surface is composed of grating. No ramp, gangway, or walkway is proposed.

No storage is proposed on the dock's grated surfaces.

No treated wood is proposed in the construction of the dock.

The proposed dock is an FWM Floating Dock - featuring a heavy-duty modular aluminum frame, 13" freeboard, and commercial-grade flotation capacity. Flotation drums are used for flotation.

No expanded polystyrene foam flotation is proposed in the dock.

No pilings are necessary for this dock. The dock uses flotation drums, as shown in the submitted cut sheet.

The Oregon Guidelines for Timing of In-Water Work to Protect Fish and Wildlife Resources provides construction dates for various offices throughout Oregon. The document lists a Newport office (the closest office listed to Lincoln City) which gives a construction window of July 1 through September 15 for coastal lake tributaries. The canal is a tributary of Devils Lake, so the construction window for the proposed dock is July 1 through September 15. The applicant intends to begin construction on or after July 1, 2022, pending conditional use approval, with completion by September 15, 2022. If the dock construction isn't complete by September 15, 2022, then construction will cease until July 1, 2023.

A site inspection shows that the proposed location for the dock does not contain any visible buried, submerged, or floating woody debris. Should any buried woody debris be encountered during construction, it will remain minimally disturbed to the greatest extent possible.

There will be no piling removal since no pilings exist.

17.77.060 Conditional Uses

In order to grant any conditional use, the planning commission must find, based upon evidence, both factual and supportive, provided by the applicant, that:

1. The proposal is in compliance with the comprehensive plan;

Finding: The project proposes installation of a dock on the site. The 4'x12' dock meets the requirements of LCMC Chapter 17.44 as detailed earlier in this report. No setbacks, common spaces, retaining walls, parking and loading areas, or landscaping are required for a dock.

Staff concludes, therefore, that once approved as a conditional use and operating in conformance with all conditions, that the proposal complies with the comprehensive plan. This criterion is satisfied.

2. The site for the proposed use is adequate in size and shape to accommodate the use and all yards, spaces, wall and fences, parking, loading, landscaping and other features required by this title;

Finding: The project proposes installation of a dock on the site. The 4'x12' dock meets the requirements of LCMC Chapter 17.44 as detailed earlier in this report. No setbacks, common spaces, retaining walls, parking and loading areas, or landscaping are required for a dock.

With application of the code requirements, this criterion is satisfied.

3. The site for the proposed use relates to streets and highways adequate in width and degree of improvement to handle the quantity and kind of vehicular traffic that would be generated by the proposed use;

Finding: The project proposes installation of a private dock at a private residence. The project will not generate any traffic over and above the traffic from the existing detached dwelling. This criterion is satisfied.

- 4. *The proposed use will have minimal adverse impact upon adjoining properties and the improvements thereon. In making this determination, the commission shall consider, but not be limited to, the proposed location of the improvements on the site, vehicular egress/ingress and internal circulation, pedestrian access, setbacks, height and bulk of buildings, walls and fences, landscaping, screening, exterior lighting and signing;***

Finding: The placement of the dock on the site meets all the dock siting requirements of LCMC Chapter 17.44 as detailed earlier in this report. No buildings, retaining walls, fences, landscaping, screening, exterior lighting, or signage are proposed or required for the dock. No pedestrian access is required to the private dock on a private residence site. Vehicular egress/ingress and internal circulation is not needed for a private dock on a private residence site. The site itself is already existing and improved with a driveway and detached dwelling with associated required residential landscaping. Staff concludes that this criterion is satisfied.

- 5. *In areas designated as requiring preservation of historic, scenic or cultural attributes, proposed structures will be of a design complementary to the surrounding area.***

Finding: The site does not have any historic, scenic, or cultural attributes, so this requirement is not applicable.

RECOMMENDATION

Staff recommends that the Planning Commission hold a public hearing on the proposed conditional use request, take public testimony, close the public hearing, deliberate, and determine whether the request complies with the criteria for granting approval of the request, complies with the criteria with conditions, or doesn't comply with the criteria.

Should the Planning Commission determine the request complies with the criteria with conditions, staff suggests the following conditions at a minimum:

1. Prior to construction of the dock, the applicant shall submit an application for the required Natural Resources Development Review as identified in LCMC 17.46.050. Approval shall be obtained prior to dock construction.
2. The applicant shall obtain all necessary permits and comply with associated conditions of permit approvals, including but not limited to: Oregon Department of State Lands (DSL), and any required environmental assessment reports as identified during the permitting process by any applicable agency.
3. Approval is for the design, size, shape, and location identified in the submitted application materials. If the design, size, shape, or location must change as a result of DSL (or other applicable agency) review, or for any other reason, the applicant must submit those changes to the Lincoln City Planning and Community Development Department for review through the appropriate review process as determined by the significance of the changes.
4. The applicant shall provide evidence prior to initiating construction that the project has been reviewed and approved by DSL or evidence that such review and approval is not necessary.
5. All required permits shall be obtained prior to staging or work activity and an electronic copy of each shall be emailed to the Lincoln City Planning and Community Development Department for inclusion in the project folder.

Prepared by: David Mattison, Senior Planner
Reviewed by: Anne Marie Skinner, Director

OFFICE USE ONLY Date Filed: _____ Amount/Fee: _____ Receipt No: _____ Received By: _____ 30 Days: _____ Deemed Incomplete: _____ Deemed Complete: _____ 120 Day Deadline: _____	 LAND USE APPLICATION	OFFICE USE ONLY Stamp Date Received _____ File No: _____
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APPLICATION TYPE

<input type="checkbox"/> ANNEXATION	<input type="checkbox"/> LOT LINE ADJUSTMENT	<input type="checkbox"/> SITE PLAN REVIEW
<input type="checkbox"/> APPEAL OF PLANNING COMMISSION DECISION	<input type="checkbox"/> MINOR PARTITION	<input type="checkbox"/> SUBDIVISION
<input type="checkbox"/> APPEAL OF PLANNING DEPARTMENT DECISION	<input type="checkbox"/> NATURAL RESOURCE DEVELOPMENT REVIEW	<input type="checkbox"/> URBAN GROWTH BOUNDARY AMENDMENT
<input type="checkbox"/> COMPREHENSIVE PLAN & ZONING MAP AMENDMENT	<input type="checkbox"/> NATURAL RESOURCE DEVELOPMENT VARIANCE	<input type="checkbox"/> VACATION
<input type="checkbox"/> COMPREHENSIVE PLAN &/OR ZONING ORDINANCE TEXT AMENDMENT	<input type="checkbox"/> PLANNED UNIT DEVELOPMENT PRELIMINARY MASTER PLAN	<input type="checkbox"/> VARIANCE
<input checked="" type="checkbox"/> CONDITIONAL USE PERMIT	<input type="checkbox"/> PLANNED UNIT DEVELOPMENT FINAL MASTER PLAN	<input type="checkbox"/> OTHER _____

NAME OF APPLICANT: Knottworks Construction and Michael and Michelle Kammermeyer

MAILING ADDRESS: PO BOX 197

CITY: OTIS **STATE:** OR **ZIP:** 97368

DAYTIME PHONE: 541.992.4405 **Signature:** *Michael Kammermeyer Michelle Kammermeyer*

EMAIL (OPTIONAL): knottworks@gmail.com and laura.knottworks@gmail.com

OWNER OF RECORD (if other than applicant)

NAME: Michael Kammermeyer, Jr. and Michelle Kammermeyer

MAILING ADDRESS: 3000 NE 26TH ST

CITY: LINCOLN CITY **STATE:** OR **ZIP:** 97367

DAYTIME PHONE: _____ **Signature:** *Michael Kammermeyer Michelle Kammermeyer*

EMAIL (OPTIONAL): _____

APPLICANT AND/OR OWNER'S INTEREST IN PROPERTY SUBJECT TO REQUEST:

AGENT, FEE OWNER, CONTRACT PURCHASER, OPTION HOLDER, LESSEE, OTHER

PROJECT LOCATION: SOUTH SIDE OF NE 26TH ST, APPROX. 84 FEET EAST OF NE WEST DEVILS LAKE RD

PROJECT ADDRESS: 3000 NE 26TH ST

ASSESSOR'S MAP NO.: 07-11-11-AC **TAX LOT(S):** 00200

AREA: 0.28 **(acres)** **SQ. FT.:** 12,196.8 **ZONING:** R-1-7.5

**CONDITIONAL USE PERMIT
EVIDENCE TO SUPPORT OF FINDINGS OF FACT**

Directions: Please provide factual and supportive evidence that your application meets the Findings of Fact listed below. It is the applicant's responsibility to demonstrate that the conditional use permit will comply with these Findings of Fact. Additional sheets may be attached if necessary.

- 1. The proposal is in compliance with the Comprehensive Plan.

See attached narrative.

- 2. The site for the proposed use is adequate in size and shape to accommodate said use and all yards, spaces, walls and fences, parking, loading, landscaping and other features required by the Zoning Ordinance.

See attached narrative.

- 3. The site for the proposed use relates to streets and highways adequate in width and degree of improvement to handle the quantity and kind of vehicular traffic that would be generated by the proposed use.

See attached narrative.

- 4. The proposed use will have minimal adverse impact upon adjoining properties and the improvements thereon. In making this determination, the Planning Commission shall consider, but not be limited to, the proposed location of the improvements on the site, vehicular egress/ingress and internal circulation, pedestrian access, setbacks, height and bulk of buildings, walls, and fences, landscaping, screening, exterior lighting and signing.

See attached narrative.


- 5. In areas designated as requiring preservation of historic, scenic or cultural attributes, proposed structures will be of a design complimentary to the surrounding area.

See attached narrative.

APPLICANT'S VERIFICATION

I (We) hereby declare under penalty of perjury under the laws of the State of Oregon that the information in this application and its attachments is true, complete, and correct.

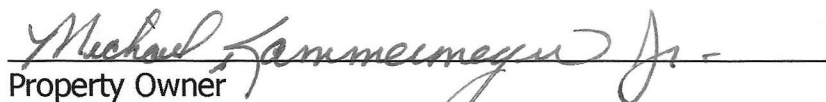
SIGNATURES



 Applicant

9/3/2021

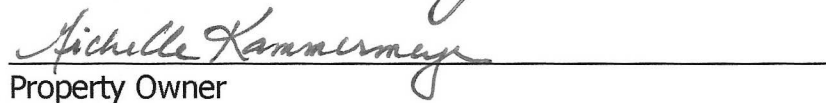
 Date



 Property Owner

9/3/21

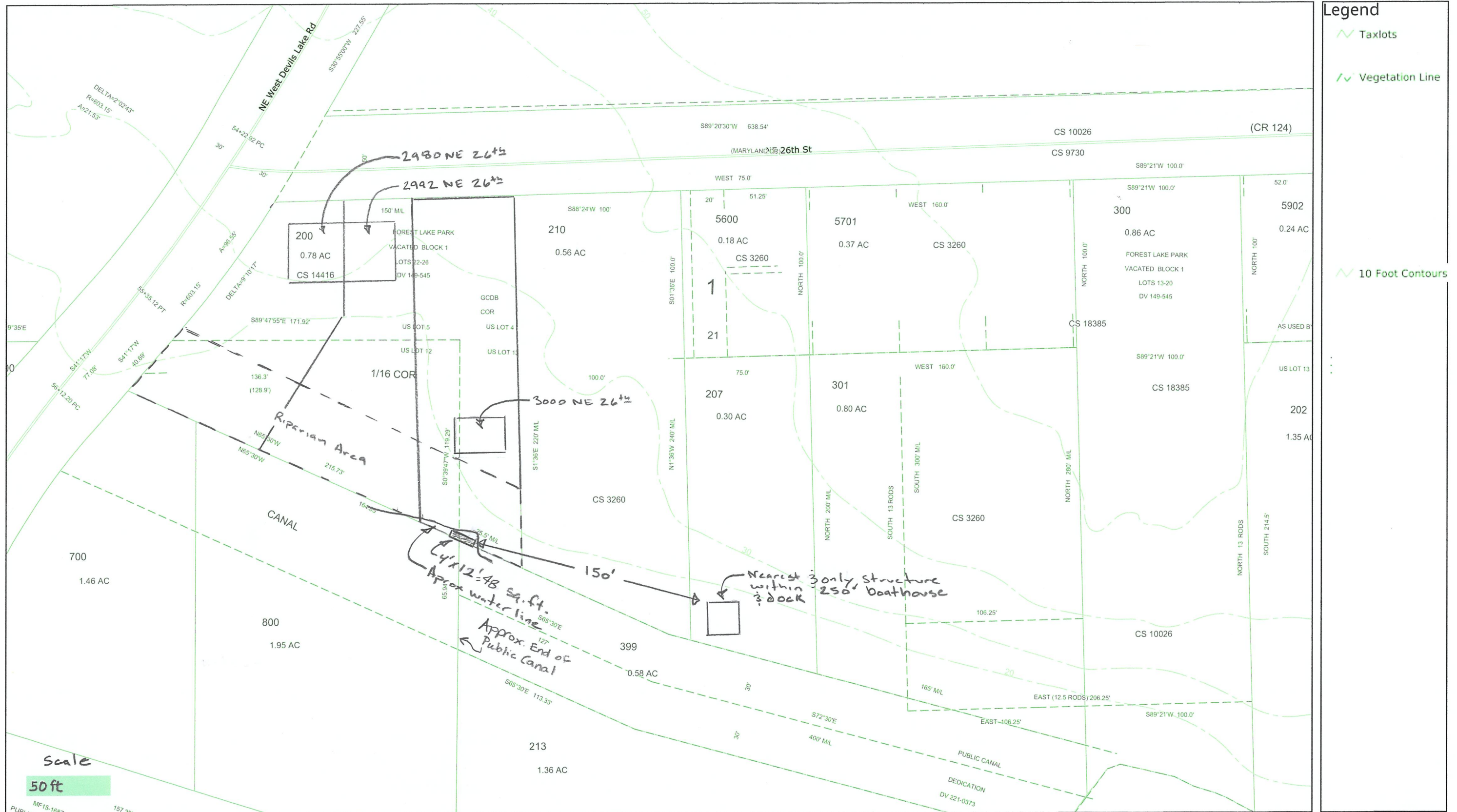
 Date



 Property Owner

9-3-21

 Date



Printed on 5 / 27 / 2021

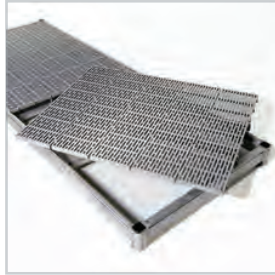
Lincoln County Government Use only. Use for any other purpose is entirely at the risk of the user. This product is for informational purposes and may not have been prepared for, or be suitable for legal, engineering, or surveying purposes. Users should review the primary information sources to ascertain their usability.



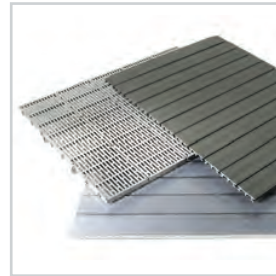
FWM | FLOATING DOCKS

Ideal for Deep or Fluctuating Water Levels

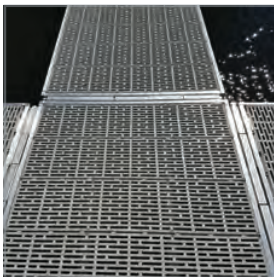
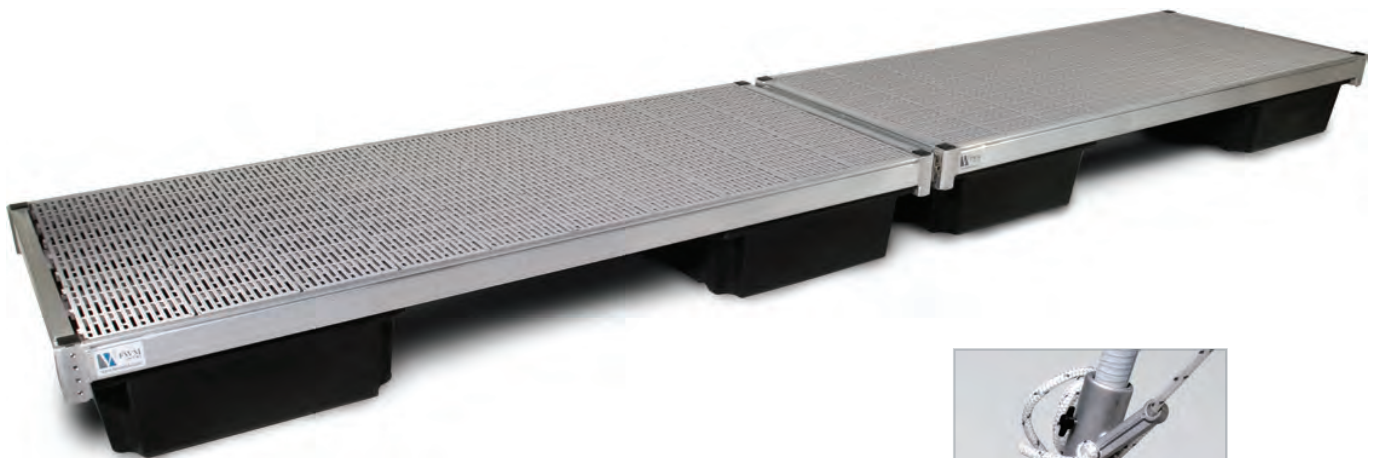
Our versatile FWM Floating Docks — featuring a heavy-duty modular aluminum frame, 13" freeboard height and commercial-grade flotation capacity — are ideal for projects of any size on practically any waterway.



4. Panelized Decking



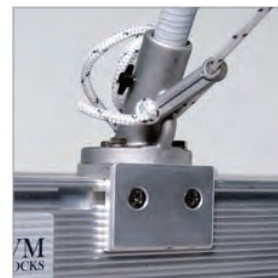
5. Decking Options



1. Piano Hinge Connection



2. Flotation Drum



3. Track System

Applicant Knottworks Construction and Michael and Michelle Kammermeyer
 PO Box 197
 Otis, OR 97368
 knottworks@gmail.com and laura.knottworks@gmail.com

Owner Michael Kammermeyer, Jr. and Michelle Kammermeyer
 3000 NE 26th Street
 Lincoln City, OR 97367

Site Information

Address: 3000 NE 26th Street
 Location: South side of NE 26th Street, approximately 84 feet east of NE West Devils Lake Road
 Map/Tax Lot: 07-11-11-AC-00200-00
 Area: 0.28 acre
 Zoning: Single-Family Residential (R-1-7.5)

Surrounding Zones

North NE 26th Street
 South Single-Family Residential (R-1-5)
 East Single-Family Residential (R-1-7.5)
 West Single-Family Residential (R-1-7.5)

Documents in Application Package

- Completed application form signed by applicant and property owners
- Narrative
- Dock Site Plan with Vicinity and Existing Conditions
- Dock Cut Sheet

Proposed Project

The subject property (site) contains a single-family dwelling that was constructed in 2020. The southern boundary of the site is the north line of a canal owned by others. The assessor's tax map shows that the eastern portion of the canal abutting the site is owned by Lincoln City, identified as Tax 07-11-11-AC-00399-00. The assessor's tax map shows that the western portion of the canal abutting the site is part of Tax 07-11-11-BD-00800, owned by Olivia L. Holt Trustee. The southernmost 50-foot-wide strip of the site is mapped as a significant riparian area.

The site contains enough water on the site's southern end to accommodate a functional dock. The dock site plan included with this application shows the proposed location of the dock as being completely self-contained on the site, with no encroachments into the adjacent canal that is owned by others. The southeastern corner of the proposed dock abuts the public portion of the canal, while not encroaching into it. This allows for the dock placement on private property (the site) with the ability to place kayaks into the portion of the canal that is for the public.

The location of the dock is within the site's significant riparian area. The applicant acknowledges that approval of a natural resources development review will be required prior to commencing construction of the dock. Once conditional use approval has been obtained for the dock, then an application will be submitted to Lincoln City for the required natural resources development review. Pursuant to Lincoln City Municipal Code (LCMC) 17.46.040.B.3.b(iv) a dock is allowed in a natural resource area provided all city code requirements are met.

LCMC TITLE 17

CHAPTER 17.16 SINGLE-FAMILY RESIDENTIAL (R-1) ZONE

17.16.040 Conditional Uses

L. Docks, subject to the provisions of LCMC 17.44.040

Response: LCMC 17.16.040.L allows docks in the R-1 zones, subject to an approved conditional use application and the provisions of LCMC 17.44.040. This application is the official request for approval of the dock conditional use. In the narrative below, compliance with LCMC 17.44.040 is addressed.

CHAPTER 17.44 MARINE WATERWAY (M-W) ZONE

17.44.040 Standards for conditional uses

In the M-W zone, or any other zone referencing these conditional use standards, the applicable provisions of this section shall apply. The standards for conditional uses shall be as follows:

A. *Standards for Uses Involving Filling, Dredging, Drainage, Disposal of Dredging Spoils, and Similar...*

Response: No filling, dredging, drainage, or disposal of dredging spoils is proposed.

B. *Standards for Uses Involving Construction, Addition or Reconstruction of a Pier, Dock, Boat House or Similar Facility*

Response: The owners of the site would like to construct a dock on their property. The proposed dock would be new construction. The site does not contain any existing piers, docks, boat houses, or similar facilities.

1. *Evidence shall be provided that the applicant has complied with, or fully intends to comply with, all standards of the Department of Environmental Quality, the Division of State Lands, and all other agencies having interests or ordinances applicable to the property in question.*

Response: The property owners hereby acknowledge as part of this application submission that all standards of the Department of Environmental Quality, the Division of State Lands, and all other agencies having interests shall be followed in the construction of the dock. After conditional use permit approval has been received, the Department of Environmental Quality and the Division of State Lands will be contacted for required permits, if any. Lincoln City's required natural resources development review will be completed after conditional use approval as well.

2. *The facility or any use related to it shall not allow any water pollution to occur to any nearby tidelands, marshlands, rivers, streams or other waterways used for the raising, production or preservation of marine life or other natural resources.*

Response: The property owners are only constructing a dock. No activities are proposed that will involve any water pollution to occur. Dock construction and materials will adhere to all requirements to avoid water pollution. The property owners will place chairs on the dock for viewing enjoyment of the water and will use the dock to place kayaks in the water. These activities do not create any water pollution.

3. *The facility shall not substantially alter the course of any channel or the natural movement of any waters or result in increased flood hazards, or the formation of appreciable bottom or sludge deposits deleterious to marine life, and shall meet all of the following requirements:*

Response: At 48 square feet, the dock has been designed to be as small as possible to minimize flood hazard and to avoid any altering of the course of the canal or its water movement. It is completely contained on the site.

- a. *No dock, pier or similar facility shall extend into any watercourse more than 25 feet from ordinary low water line nor 50 feet from ordinary high water line, unless it can be shown that such extension is necessary and will...*

Response: The dock will be completely contained on the site, and does not extend into the privately or publicly owned canal. The dimension of the dock from the southern edge of the site's water line to the site's southern property line is only 4 feet in depth.

- b. *No dock, pier or similar facility shall extend into the navigable channel any distance greater than required for safe moorage and shall be designed so as to minimize potential flood hazard and loss of navigable waterway area.*

Response: The dock is completely contained on the site, and does not extend into the privately or publicly owned canal. There will be no loss of navigable waterway area due to placement of this dock. The dock has been designed to be as small as possible to minimize potential flood hazard, and the location of the dock is at a lower elevation than the house.

- c. *No pier, dock or similar facility shall extend into any watercourse more than five percent of the width thereof as measured perpendicular from the mean low water line on one side of the watercourse to the mean low water line on the opposite side.*

Response: The dock is completely contained on the site, and does not extend into the privately or publicly owned canal.

4. *No plumbing facilities for the handling of domestic or industrial waste shall be part of the facility unless approved by the city manager or design.*

Response: No plumbing facilities at all are proposed for the dock.

5. *Application for a permit for a pier, dock, bulkhead, boat house, or similar facility shall include:*
- a. *The source of the applicant's right to construct the facility;*

Response: The site is in an R-1 zone. A dock is allowed in an R-1 zone with conditional use approval, subject to the requirements of LCMC 17.44.040.

- b. *The purpose of the facility;*

Response: The purpose of the proposed dock is to afford the owners of the site an area to sit directly at the water's edge and enjoy the view, as well as an area in which to place kayaks into the public portion of the canal and kayak to the east.

- c. *The legal description of the area where the facility will be located;*

Response: The legal description of the site is Parcel 1 of Partition Plat 2020-15.

- d. *A map and drawings, showing the plan for construction of the facility. Such plan shall include a vicinity map drawn to scale showing location and design of similar facilities and other development within 250 feet of the parcel upon which the improvement is proposed;*

Response: A cut sheet is included in the submitted package showing the construction of the facility. The submitted site plan shows the structures within 250 feet of the site as a dock and boathouse approximately 150 feet to the east of the site and a duplex to the west of the site.

- e. *The time when the project is scheduled to begin and to be completed.*

Response: Construction of the dock will commence upon approval of the conditional use application and the natural resources development review, but not sooner than July 1, 2022, and cease on September 15, 2022. If the project is not completed by September 15, 2022, then it will be completed between July 1, 2023 and September 15, 2023.

6. *Plans for a pier, dock, boat house, or similar facility shall meet the following requirements:*
- a. *In new subdivisions...*

Response: The site is not part of a new subdivision.

- b. *Facilities being proposed in areas where it is likely that additional similar structures will be desired shall be designed to be combined into joint facilities whenever possible.*

Response: It is not anticipated that either property to the east or west of the site will be constructing a dock.

- c. *The design of moorages must provide sheer logs or similar devices for fending debris. Such improvements need not be maintained during periods where there is no danger of flood water.*

Response: No moorages are proposed.

- d. *Docks shall have the long dimension running parallel to the channel unless future development will result in pier construction or moorages being connected, necessitating facility design perpendicular to the channel. The width of those portions of such facilities shall be the minimum dimension required to provide safe access and moorage.*

Response: The proposed dock's length dimension of 14 feet runs parallel with the channel as required. This requirement is met.

- e. *One dock shall not be closer to another dock than the length of the shorter structure or 25 feet, whichever distance is greater.*

Response: A review of the submitted site plan shows there are no docks within 25 feet of the proposed dock.

- f. *Additionally, docks allowed with conditional use approval in the GC, TVC, OS, P, RM, RC, and R1 zoning districts, must meet the following requirements and standards:*
- i. *Total area of dock must not exceed 144 square feet with a width not exceeding six feet and a length not exceeding 24 feet.*

Response: A review of the dock site plan shows that the proposed dock will be 48 square feet with a length of 12 feet, and a width of 4 feet.

- ii. *No part of the dock can be covered or enclosed, such as but not limited to boat houses, sheds, fish cleaning stations, kayaks, canoes, hot tubs, and/or benches.*

Response: The entirety of the dock will be uncovered. No enclosures are proposed.

- iii. *Docks must have at least 50 percent of the float surface composed of grating containing at least 60 percent open space surface.*

Response: A review of the submitted cut sheet shows that the entire surface is composed of grating.

- iv. *The ramp/gangway of the dock must be 100 percent grated to allow light to pass through. Ramp width must not exceed five feet. The ramp square footage is not included in the total dock area square footage.*

Response: No ramp, gangway, or walkway is proposed.

- v. *Grated surfaces on the docks must not be used for storage (e.g., boats, benches, kayaks, fish cleaning stations, etc.) or other purposes that will reduce natural light penetration through the dock.*

Response: No storage is proposed on the dock's grated surfaces.

- vi. *Treated wood is not allowed as a construction material for docks or ramps. Treated wood is lumber, pilings, and other wood products preserved with alkaline copper quaternary (ACQ), ammoniacal copper arsenate (ACA), ammoniacal zopper zinc arsenate (ACZA), copper naphthenate, chromated copper arsenate (CCA), pentachlorophenol, or creosote.*

Response: No treated wood is proposed in the construction of the dock. The proposed dock is an FWM Floating Dock - featuring a heavy-duty modular aluminum frame, 13" freeboard, and commercial-grade flotation capacity. Flotation drums are used for flotation.

- vii. *Oregon law requires encapsulation of expanded polystyrene foam flotation used in state waters. Encapsulation methods and materials must be approved by the Oregon State Marine Board prior to installation of foam flotation.*

Response: No expanded polystyrene foam flotation is proposed in the dock.

- viii. *All pilings must be fitted with devices to prevent perching by piscivorous birds*

Response: No pilings are necessary for this dock. The dock uses flotation drums, as shown in the submitted cut sheet.

- ix. *All work must be completed in accordance with the Oregon Department of Fish and Wildlife Guidelines for Timing of In-Water Work to Protect Fish and Wildlife Resources.*

Response: The Oregon Guidelines for Timing of In-Water Work to Protect Fish and Wildlife Resources provides construction dates for various offices throughout Oregon. The document lists a Newport office (the closest office listed to Lincoln City) which gives a construction window of July 1 through September 15 for coastal lake tributaries. The canal is a tributary of Devils Lake, so the construction window for the proposed dock is July 1 through September 15. The applicant intends to begin construction on or after July 1, 2022, pending conditional use approval, with completion by September 15, 2022. If the dock construction isn't complete by September 15, 2022, then construction will cease until July 1, 2023.

- x. *There must be minimum disturbance to any buried, submerged, or floating woody debris removal during construction.*

Response: A site inspection shows that the proposed location for the dock does not contain any visible buried, submerged, or floating woody debris. Should any buried woody debris be encountered during construction, it will remain minimally disturbed to the greatest extent possible.

- xi. *For piling removal, dislodge the piling with...*

Response: There will be no piling removal since no pilings exist.

CHAPTER 17.77 APPLICATIONS

17.77.060 Conditional use permit

- A. *Purpose.*
- B. *Procedure. Conditional use permit requests are subject to the Type III procedure, as described in LCMC 17.76.050.*
- C. *Submittal Requirements.*
 1. *Type III application submittal requirements are set forth in LCMC 17.76.050. Specific submittal requirements are provided on application forms and checklists as authorized on LCMC 17.76.110.*
 2. *At a minimum, an application for a conditional use shall include the following:*
 - a. *An application form signed by the applicant and every current property owner of record as shown on the most current recorded deed;*

Response: The completed application form for the conditional use permit and the master land use application form have been completed and signed by both the applicants and property owners.

b. *Full payment of the application fee, based on the fee schedule in effect on the date of submittal;*

Response: The full application fee has been paid as part of this submission.

c. *Existing conditions plan;*

Response: Existing conditions are included on the site plan,

d. *Site plan;*

Response: The required site plan is included as part of the submission.

e. *Landscape plan;*

Response: There are no landscaping requirements for a dock.

f. *Building elevations (for new construction);*

Response: The cut sheet for the proposed dock is included in the submission. No new construction of buildings is proposed.

g. *Phasing plan (if phasing is proposed);*

Response: No phasing is proposed.

h. *Traffic impact study pursuant to LCMC 17.52.300; and*

Response: Installation of a dock at a single-family residence does not meet the threshold for a traffic impact study.

i. *Narrative addressing compliance with each approval criterion and applicable standard.*

Response: This document is the narrative that addresses compliance with each approval criterion and applicable standard.

D. *Approval Criteria. In order to grant any conditional use, the planning commission must find, based upon evidence, both factual and supportive, provided by the applicant, that:*

1. *The proposal is in compliance with the comprehensive plan;*
2. *The site for the proposed use is adequate in size and shape to accommodate the use and all required setbacks, common spaces, retaining walls, parking and loading areas, landscaping, and other features required by this title;*

Response: The project proposes installation of a dock on the site. The 4'x12' dock meets the requirements of LCMC Chapter 17.44 as detailed earlier in this narrative. No setbacks, common spaces, retaining walls, parking and loading areas, or landscaping are required for a dock.

3. *The site for the proposed use relates to streets and highways adequate in width and degree of improvement to handle the quantity and kind of vehicular traffic that would be generated by the proposed use;*

Response: The project proposes installation of a private dock at a private residence. The project will not generate any traffic over and above the traffic from the existing single-family dwelling.

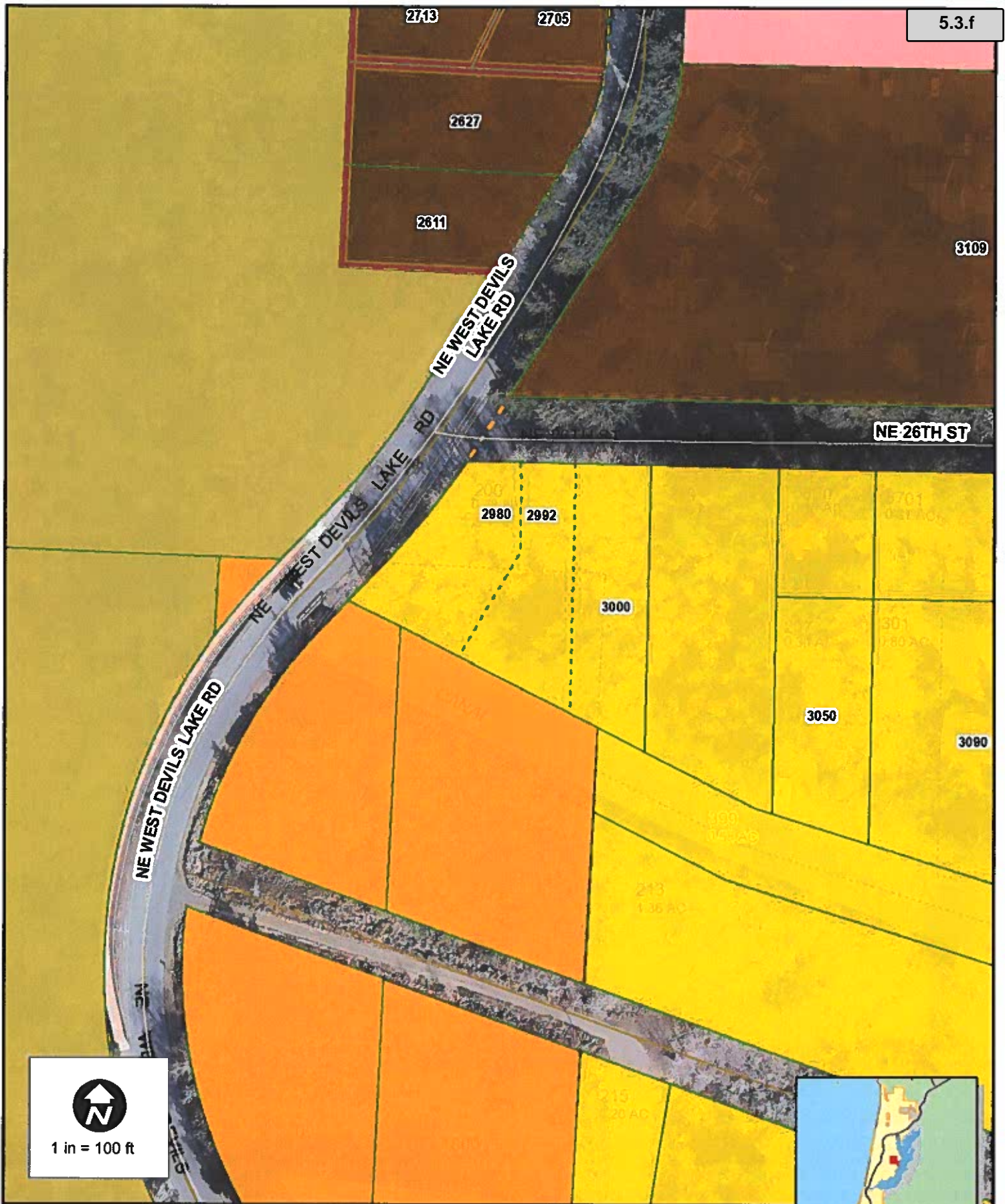
4. *The proposed use will have minimal adverse impact upon adjoining properties and the improvements thereon. In making this determination, the review authority shall consider, but not be limited to, the*

proposed location of the improvements on the site, vehicular egress/ingress and internal circulation, pedestrian access, setbacks, height and mass of buildings, retaining walls, fences, landscaping, screening, exterior lighting, and signage;

Response: The placement of the dock on the site meets all the dock siting requirements of LCMC Chapter 17.44 as detailed earlier in this narrative. No buildings, retaining walls, fences, landscaping, screening, exterior lighting, or signage are proposed or required for the dock. No pedestrian access is required to the private dock on a private residence site. Vehicular egress/ingress and internal circulation is not needed for a private dock on a private residence site. The site itself is already existing and improved with a driveway and detached single-family dwelling with associated required residential landscaping.

5. *In areas designated as requiring preservation of historic, scenic or cultural attributes, proposed structures will be of a design complementary to the surrounding area.*

Response: The site does not have any historic, scenic, or cultural attributes, so this requirement is not applicable.



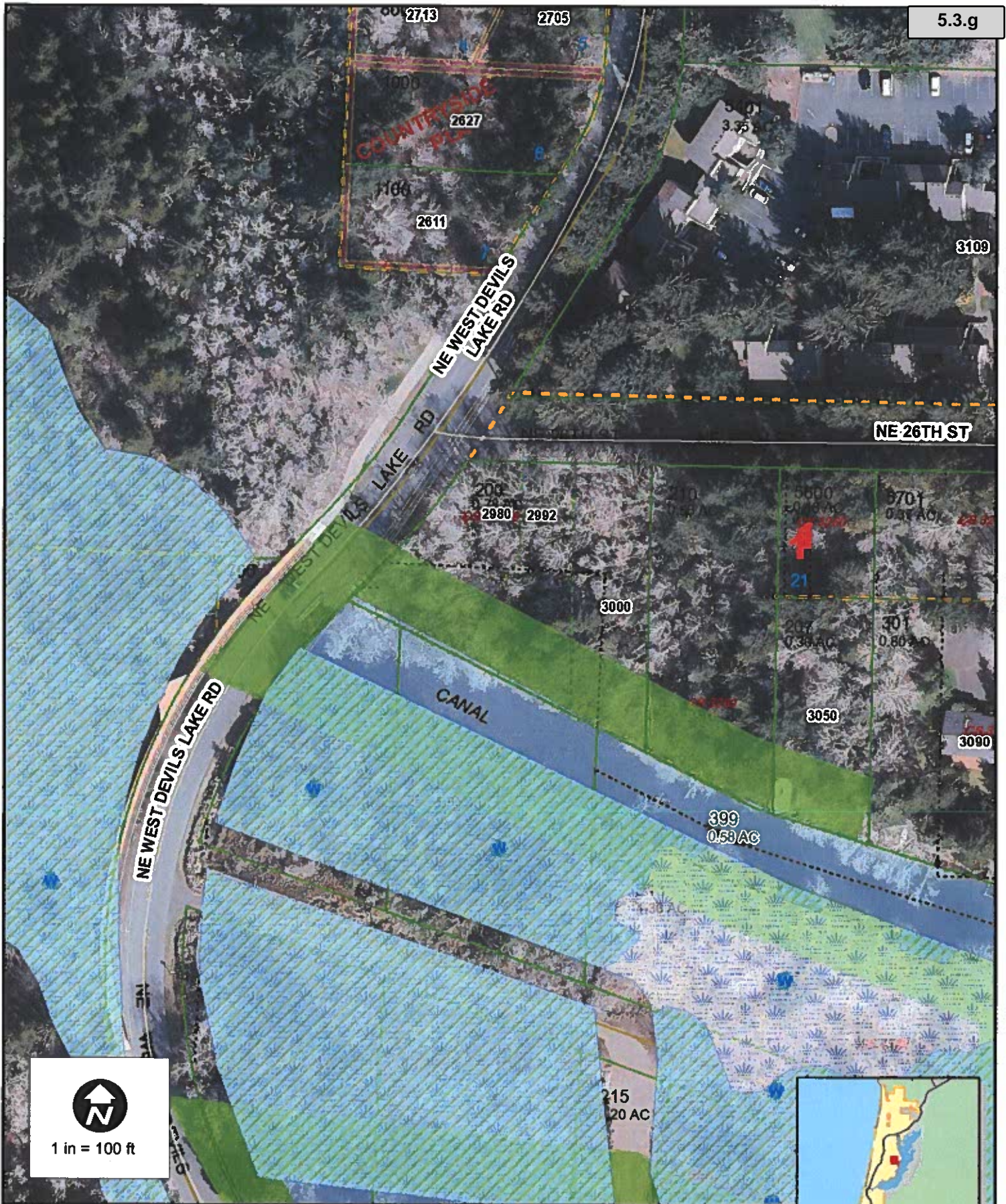
Location



Vicinity Map

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Date: 10/13/2021



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Location